



UNION PARK DISTRICT COUNCIL

161 Snelling Avenue North, Saint Paul, MN 55104
651.645.6887 | info@unionparkdc.org | www.unionparkdc.org
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November 6, 2017

Tia Anderson
Department of Safety and Inspections
City of Saint Paul
375 Jackson Street
Saint Paul, MN 55101

Dear Tia:

The Union Park District Council Committee on Land Use and Economic Development convened a special meeting on October 30 to discuss the site plan for the proposed development at 1973/1977 Marshall Avenue. Property owner Jon Schwartzman and architect Paul Holmes presented on the project.

I have summarized the primary concerns and requests set forth by Committee and community members at the meeting. Thank you for the opportunity to share these issues with the site plan review team.

1. Building height

A primary request is to lower the overall height of the proposed five-story structure by one—or preferably two—stories. The prevailing reasons for this include the facts that:

- All of the multifamily residences in the area are a maximum of three stories; this proposed structure is much taller than other nearby buildings.
- The west elevation of the building as proposed is essentially six stories, exceeding the maximum allowable number of stories.
- There is concern that the building as submitted in the plans exceeds the maximum allowable height of 50 feet, both in building design and through the establishment of a new grade.
- In the City's Comprehensive Plan, Marshall Avenue is a medium-density residential corridor, which calls for a maximum density of 30 units per acre; the density of the proposed project is reportedly about 49 units per acre.

2. Balconies and window issues

The architect has acknowledged that the tenants of this property will be primarily—if not exclusively—college students. Accordingly, there is general consensus that the balconies should be removed from the plans for the following reasons:

- Noise impacts are of primary concern to the community. Excessive late-night noise from balcony use, exacerbated by their height, is anticipated.
- Balconies pose a safety issues as well as a greater likelihood for nuisance, such as thrown objects and urination off of them; neighbors in the area have experienced similar nuisances created by student rentals in the past.
- With the limited amount of storage provided within the building, there is concern that the balconies will become cluttered and unsightly.

The developer is also urged to replace the proposed vinyl windows with higher-quality energy-efficient non-opening windows to contain sound within the building.

3. Traffic and parking

With the number of tenants (not to mention visitors) at this property likely providing a net of at least 54 additional vehicles, a traffic study and traffic management plan are called for, focused on the Marshall and Moore intersection. With Four Seasons A+ Elementary and St. Mark's schools in the immediate vicinity, and frequent church traffic on weekends, there is a need to consider a marked crosswalk, pedestrian refuge median, rectangular rapid flashing beacon, and other types of traffic control at that intersection to ensure pedestrian safety and adequate left-hand turning movements.

There are also significant parking concerns. With 64 residents and 32 parking spaces provided, pressure on side-streets will be significant. Specifically, with respect to parking:

- There are concerns that if tenants must pay extra for off-street parking, that they will elect not to use it. Thus, the developer is urged to attach parking costs to the units.
- There are concerns about the adequacy of proposed off-street parking. For example, questions have been raised as to the legality of the separate garage structures (situated in the side yard, and violating set-back requirements) and the feasibility of the parallel spot in the main garage.
- Better bicycle parking options are also requested: outdoor bicycle parking is not secure in this area, and bicycle parking over vehicles in garage does not seem desirable.

4. Exterior appearance and finishes

There is strong sentiment that the proposed building is entirely out of character with the neighborhood. Primary requests include the following:

- The building's design should better match the historic nature of the neighborhood, with an early 20th century classic appearance.
- Recessed or set-back upper stories would reduce the monolithic appearance and break up the exterior façade; the EastRiver Apartments at 2320 Marshall was identified as an example of this.
- A primarily, if not exclusively, brick exterior would better comport with the neighborhood character; the Vintage development at 1555 Selby Avenue was identified as an example of this.
- Recessed windows with thresholds and reveals would give building design more relief.
- Higher quality and refinement of finishes overall is desired; there is a sense that student tenants will treat the property with more care if it is higher quality.

5. Other issues and concerns

Additional requests and concerns with consensus support include the following:

- A shadow analysis should be completed, and shade impacts on surrounding residences should be mitigated to the extent possible to preserve the ability to capture solar and grow vegetation.
- Trash and recycling must be adequately addressed and managed with minimal impacts to surrounding residences.
- Situating the rental management office on-site is supported, but the owner has indicated that it will also serve as the leasing office for his seven other rental properties. There is concern that this commercial-type use will be burdensomely intense and not allowed within RM2 zoning.

- The property owner must institute a robust security plan with adequate lighting, cameras, and other security measures.
- The property owner must have strong lease conditions with expectations and regulations related to quiet hours, nuisance issues, visitors, and over-occupancy.
- The contractor must share a construction management plan for demolition and development, and weekly communication on the project with designated neighbors and the district council.

There have also been concerns expressed about recent maintenance-based complaints lodged against this developer with respect other student-rental properties he owns, concerns that he has rented to students in violation of the student housing overlay, and concerns about his selection of a newly-graduated college student as the on-site manager of this housing complex.

6. Union Park District Council's ten-year plan

While likely more pertinent for the Planning Commission public hearing than for site plan review, there are numerous provisions of Union Park's Ten-Year Community Plan that apply to support the above requests. In its plan, the community has committed to:

- Preserve the well-kept, traditional feel and scale of the neighborhood (LU2), and preserve and improve the character of the neighborhood (H2).
- Ensure that new development fits within the character and scale of adjacent neighborhoods (LU2.3).
- Support multi-unit mixed-use development *in mixed-use corridors* (specifically, Marshall Avenue *between Snelling and Hamline* per LU3.2) that can accommodate higher density levels, while minimizing impacts on adjacent lower density areas, and discourage multi-unit housing uses that are incompatible with single-family residential areas (H1.1).
- Integrate historic significance into Union Park's housing and land use decision-making processes, supporting preservation over demolition (HP1) and encourage the continued use and rehabilitation of existing structures to preserve the historic character of residential and commercial districts (LU2.2).

And, the Plan mandates that "new residential construction shall be consistent with the character of the surrounding homes, while minimizing impact on the neighborhood." (H2.2a).

This project is the first major redevelopment along this stretch of Marshall Avenue in 50 years. The project's architect acknowledged that "bringing this higher density student use" will have a significant impact on the neighborhood. The City Council recently unanimously supported an interim ordinance putting a hold on such projects so that development can be done thoughtfully, in a way that meets our City's increased housing demand while preserving important historical assets and neighborhood character when possible. We request that City staff approach the evaluation of this project with those principles in mind.

Sincerely,



Julie Reiter, Executive Director, Union Park District Council

From: Meg Arnosti <arnosti.meg@gmail.com>

Sent: Wednesday, December 27, 2017 11:54:18 PM

To: Englund, Cherie (CI-StPaul); Diatta, YaYa (CI-StPaul); Johnson, Tony (CI-StPaul); dan.edgerton@stantec.com; blindeke@gmail.com; cedrick.baker@gmail.com; adejoy@esndc.org; christopher.james.ochs@gmail.com; ecr@trios-llc.com

Subject: Oppose 1973 and 1977 Marshall Avenue development

Dear Mr. Edgerton and Members of the Zoning Committee:

I urge you to oppose the development at 1973/1977 Marshall Avenue for the following reasons.

Union Park's 10-Year Community Plan calls for preserving the traditional feel of the neighborhood, discouraging multi-unit housing that is incompatible with single-family houses, and integrating historic significance into housing decisions. The existing homes at 1973 and 1977 Marshall contribute to the historic and unique character of Marshall Avenue, but the proposed apartment building would be destructive to that character.

The City Council approved a one-year development moratorium along these six-blocks of Marshall Avenue specifically to give a chance to evaluate the destruction of this sort of historic housing. On that very day, the developer squeaked in under the wire and submitted his hasty proposal to try to avoid being subject to the moratorium.

The University of St. Thomas has promised for years to supply enough student housing for its students and has failed to do so. This results in pressure to destroy the beautiful residential neighborhood forever.

The west elevation of the building as proposed exceeds the maximum allowed height of 50 feet because it is elevated on a hill from the street.

Marshall Avenue is a medium-density residential corridor in the City's Comprehensive Plan. This allows a maximum density of 30 units per acre; the density of the proposed project is 49 units per acre.

Only 32 parking spaces are proposed for 64 residents, and tenants will have to pay extra for these spots. This means that parking will overflow into already-crowded residential streets.

This development would unreasonably burden the neighboring homes, especially 1969 Marshall Avenue, and also 1985 Marshall Avenue and 1972 and 1980 Iglehart Avenue, with stormwater runoff, car noise and fumes.

Please keep in mind that once the character of Marshall Avenue changes, the charm of one of our great streets will be lost forever. Thank you for your careful consideration of this matter.

Sincerely,
Margaret Malde-Arnosti
1722 Princeton Avenue
St. Paul, MN 55105

From: pcc322 <pcc322@centurylink.net>

Sent: Wednesday, December 27, 2017 9:21:53 PM

To: Englund, Cherie (CI-StPaul); Diatta, YaYa (CI-StPaul); Johnson, Tony (CI-StPaul)

Subject: Merriam Park development.

To the Zoning Committee:

I am an active member of the congregation of the Evangelical Formosan Church at 1982 Iglehart.

I am concerned about the plan to tear down the house at the NE corner of Marshal and Moore.

I arrive early on Sunday before most of the congregation, and at that early hour, I can't always find a convenient place to park. Those who arrive later may have to walk nearly a block.

We have a couple of handicapped members, and though there isn't any designated handicapped parking places, most of us try to leave a convenient spot or two.

I know from a brief encounter that the tight parking situation on Sunday mornings does irritate some of the neighbors, and I sympathize with them.

So... As it is there is barely enough parking in the neighborhood as it is. Adding more automobiles to the on street parking will cause problems for both the residents in the neighborhood and those who want to park on the street just once or twice a week.

I urge you to withhold approval of the development plans for that neighborhood.

James A Willmore
18795 Partridge Circle
Eden Prairie. MN 55346

From: Helen Holtti <hholtti@yahoo.com>

Sent: Thursday, December 28, 2017 9:58 AM

Subject: Moore and Marshall apartments

To: Englund, Cherie (CI-StPaul) <cherie.englund@ci.stpaul.mn.us>

Cherie,

I am writing to express my opposition to the apartment on Marshall and Moore. My daughter lives at 1999 Iglehart- just around the corner. I am a senior citizen and come over to visit my daughter often. On her side of the block is 11+ students living on the odd side of the block. Parking near her home is already practically impossible. If 50+ more cars are added - I will be carrying groceries for blocks!!! The students aren't aware of not blocking crosswalks or leaving a space in front even though they have been asked to do so. Plus 14+ busses go down Moore and Iglehart every school day making the streets very busy and congested. If a bus is traveling down the street you have to wait for it to pass - there is only room for one vehicle at a time because there are many cars on both sides of the street.

Please pass my email on to all members

Helen Holtti

651-280-7509

Sent from my iPhone

From: Madison Pierce <madison27pierce@gmail.com>
Sent: Thursday, December 28, 2017 11:41:34 AM
Subject: Marshall & Moore Apartment Development

Hello,

I'm a resident at 1990 Marshall avenue (across the street from the proposed development) and I oppose this project. While increased density in the neighborhood is essential, the units are simply unaffordable. We don't need to add to the luxury apartment bubble we currently see in the Twin Cities. It's not hard to see why \$800/bedroom/month is unrealistic for most people.

I would support the project if the units were affordable. I don't even care what they look like! I'm not annoyed by students being around the neighborhood! I'm not concerned about noise or parking and I don't even mind living across the street from a development project. I support density but I cannot support the construction of more luxury apartments.

Madison Pierce
1990 Marshall Avenue

From: Emma Kopp <em.kopp94@gmail.com>

Sent: Thursday, December 28, 2017 11:53:28 AM

To: Englund, Cherie (CI-StPaul); Diatta, YaYa (CI-StPaul); Johnson, Tony (CI-StPaul); dan.edgerton@stantec.com; blindeke@gmail.com; cedrick.baker@gmail.com; adejoy@esndc.org; christopher.james.ochs@gmail.com; ecr@trios-llc.com; Henningson, Samantha (CI-StPaul); #CI-StPaul_Ward4; #CI-StPaul_Ward7

Subject: Statement of Opposition to the Marshall & Moore Apartments

Dear Zoning Committee,

I am writing to urge you to join me and many others in opposing construction of the proposed Marshall and Moore Apartments.

First and foremost, Mr. Schwartzman's plans do not, in any way, support affordable housing. \$800 a room, for student or otherwise, is not affordable housing.

While I understand and feel the weight of a growing housing crisis in the Twin Cities, and namely our Merriam Park community, this proposition is not the correct way to go about it. The construction of apartments like the Marshall and Moore Apartments do nothing to conserve the history and charm of our neighborhood and will lead directly to more gentrification of the area.

Developing these apartments is a disgusting corporate debasement of our community and I urge you to oppose the Marshall and Moore Apartments project.

With respect,
Emma Kopp
1696 Marshall Ave
763-412-0704

MOHRMAN, KAARDAL & ERICKSON, P.A.

ATTORNEYS AND COUNSELORS AT LAW

150 SOUTH FIFTH STREET
SUITE 3100
MINNEAPOLIS, MINNESOTA 55402

ERICK G. KAARDAL

TELEPHONE: 612/341-1074
FACSIMILE: 612/341-1076
WRITER'S E-MAIL: KAARDAL@MKLAW.COM

December 21, 2017

Via U.S. Mail & E-mail

Mr. Yaya Diatta
Zoning Administrator
City of Saint Paul
Department of Safety and Inspections
375 Jackson Street, Suite 220
Saint Paul, MN 55101-1806

Re: "Marshall & Moore Apartments"
1973/1977 Marshall Avenue lots (Revised SPR File # 17-206385)

Dear Mr. Diatta:

As previously noted, I represent a number of Merriam Park residents and the Church of St. Mark, St. Paul, Minnesota, in this matter. While we were disappointed that the original Marshall & Moore Apartments site plan review application submitted on October 18, 2017, was not rejected by the City as incomplete, my clients would like to convey their appreciation to City staff for their expert and thorough site plan review process.

We have now reviewed the revised site plan review application (the "Revised Application") that the Saint Paul Department of Safety & Inspections received, on December 14, 2017, for the proposed multiple-family residential project named "Marshall & Moore Apartments" (the "Project") on the combined 1973/1977 Marshall Avenue lots (SPR File # 17-206385). I am writing because the Revised Application, as submitted, is defective and does not fundamentally comply with prescribed City of Saint Paul medium-density RM2 multiple-family residential zoning dimensional standards, Chapter 63 Zoning Code – Regulations of General Applicability, and certain federal and state statutes currently in effect for these two City lots. The City previously informed the applicant that he would not be permitted to seek any zoning or other variances from the City for the Project, in light of interim Ordinance 17-54 (moratorium) now in effect over west Marshall Avenue between Wilder and Wheeler streets. Accordingly, the Revised Application for SPR File # 17-206385 should be denied, and the City should inform the applicant of its denial of the Revised Application within the timeframe established pursuant to Minn. Stat. § 15.99, Subd. 3. In the event DSI does not deny the Revised Application and instead submits it to the Planning Commission, we respectfully request that you include this letter and

exhibits with the Zoning Committee Staff Report you deliver to the Planning Commission and its Zoning Committee.

Additionally, the Revised Application is defective because several key components of the Project, if built, would create nuisance for and unreasonably affect abutting properties and the owners of such properties. Further, the applicant has not proposed reasonable protection provisions for adjacent and neighboring properties to address unreasonable interference and nuisance, such as adequate surface water drainage, sound and light buffers, preservation of views, light, and air, and those aspects of design, which may have substantial effects on neighboring uses. For example, the Project proposes approximately 3,100 square feet (nearly 22% lot coverage) of paved driveway in the side yard within four feet of the shared property line with the adjacent property, 1969 Marshall Avenue. 1969 Marshall is only two feet from this shared property line. While driveways in side yards are not unusual for single-family residential dwellings, a side-yard driveway to access nine parking spaces (eight enclosed and one surface space) in a residential zoning district, with traffic, car and garage-door noises, lights, and exhaust during all times of the day, creates an unreasonable, noncompliant interference and unacceptable nuisance for the abutting properties and owners. As the Project is proposed, there is no way to eliminate this nuisance.

The Revised Application still does not provide complete information, such that a comprehensive review of the proposed development can be completed, or one may determine whether the Project, as designed, could actually be built. City requirements are clear that a complete application must include specific information regarding not only existing conditions at the site, but also information about the proposed development and its likely impact on surrounding areas. In this case, the applicant failed to provide key information including, but not limited to specific information requested by Tia Anderson in her November 8, 2017, Site Plan Review Report and certain structural drawings. According to the Revised Application, the building will be constructed with a concrete garage structure, with a portion underground. From the sketches on the Revised Application's Garage Plan and Building Section sheet, which is not drawn to scale, it is impossible to determine whether the building's parking structures will accommodate load requirements and fire separation codes. The architectural documents submitted do not establish accurate parking structure clear heights directly below and outside the building, slab and beam depths, and lighting requirements, and likely do not reflect designed column locations further reducing the underground parking count and maneuvering lane widths.

In addition, the defective and incomplete nature of the Revised Application is illustrated by the attached exhibits A through F. In summary, the Project, as designed:

1. Exceeds the maximum height permitted in RM2 zoning districts;
2. Requires major variances to encroach in the rear and interior side-yard setbacks for an off-street parking facility;

3. Requires major variances to encroach in the rear and interior side-yard setbacks for the “underground” parking structures;
4. Does not meet minimum off-street parking requirements;
5. Requires major variances to exceed the maximum lot coverage in RM2 zoning districts from 35% to 54.1%;
6. Requires major variances to exceed the maximum density in RM2 zoning districts;
7. Requires major variances to reduce the minimum lot area per dwelling from 1,500 square feet to 1,248 square feet;
8. Requires major variances to reduce the minimum off-street parking from 30 to 20 spaces;
9. At 49 units per acre, proposes a density standard that materially exceeds St. Paul’s *Generalized 2030 Future Land Use Comprehensive Plan* for a Residential Corridor, such as west Marshall Avenue;
10. Does not comply with requirements set forth in the *Union Park Community Plan*;
11. Does not preserve certain unique geologic, geographic, or historically significant characteristics of the City;
12. Does not adequately protect adjacent and neighboring properties through reasonable provision for such matters as surface water drainage, sound and light buffers, preservation of views, light, and air, and those aspects of design, which may have substantial effects on neighboring uses;
13. Does not arrange the Project’s building, uses, and facilities in order to assure abutting properties and property occupants will not be unreasonably affected;
14. Does not provide safety and convenience of both vehicular and pedestrian traffic both within the site and in relation to access streets, including traffic circulation features, the locations and designs of entrances and exits, and parking areas within the site;
15. Does not demonstrate availability and capacity of storm and sanitary sewers, including solutions to any drainage problems in the area of the Project;
16. Proposes insufficient landscaping, fences, wall, and parking necessary to meet the City’s residential zoning objectives; and
17. Does not provide site accessibility in accordance with provisions of the Americans with Disabilities Act, including parking spaces and accessible routes.

Additional deficiencies with the Revised Application include, but are not limited to:

1. As noted above, structural drawings were not submitted for review;
2. Snow storage still appears insufficient for the proposed impervious surface area;

3. No elevator equipment or hydraulic pump room (approximately 5' x 10') is shown adjacent to the elevator shaft in the parking garage or first floor; if installed in the parking garage, would potentially interfere with parking spaces and access; and
4. Trash and recycling area still appears insufficient for the proposed minimum occupancy of 61 residents.

The City has demonstrated its long-term commitment to ensuring that redevelopment along west Marshall Avenue occurs in a careful, well-planned, and zoning compliant manner through the City Council's unanimous approvals of: (1) Resolution 17-1582, which authorized the zoning study of land use and land use classifications along Marshall Avenue between Mississippi River Boulevard and Hamline Avenue and (2) Ordinance 17-54, an interim ordinance pursuant to Minn. Stat. § 462.355, Subd. 4, pending completion of the West Marshall Avenue Study, specifically between Wilder and Wheeler streets, to preserve the status quo and protect the integrity of the West Marshall Avenue Study's recommendations. Approving an application, such as this one, would directly contravene the important planning commitment that was made by the City.

We look forward to your confirmation that the Revised Application has been denied, with notice to the applicant within the City's timeframe pursuant to Minn. Stat. § 15.99, Subd. 3.

Sincerely,



Erick G. Kaardal

EGK/mg

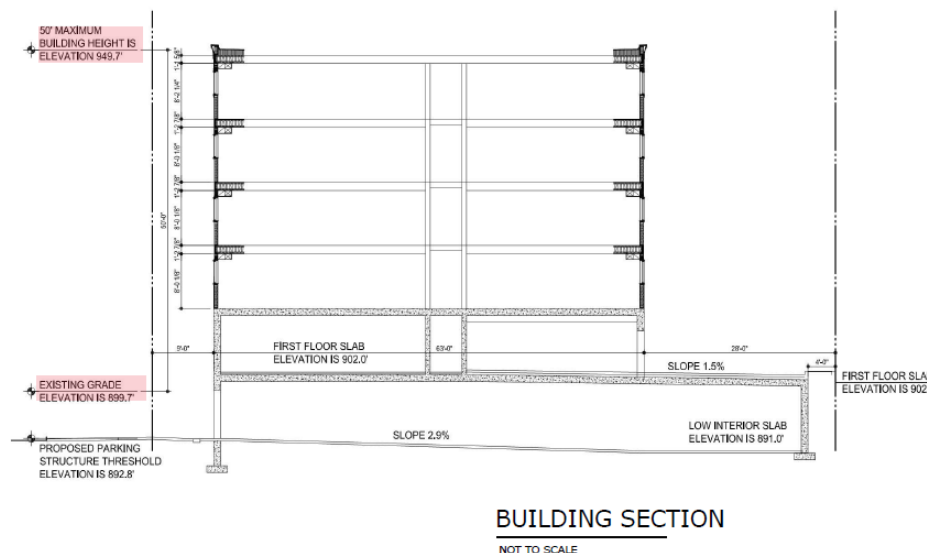
Enclosures

cc: Samuel J. Clark, City Attorney
Ricardo X. Cervantes, Director, Department of Safety and Inspections
Wendy Lane, St. Paul Zoning Administrator, Department of Safety and Inspections
Lawrence R. Zangs, Site Plan Review Coordinator, Department of Safety and Inspections
Tia Anderson, Senior City Planner, Department of Safety and Inspections
Council President Russ Stark, Ward 4 Councilmember
Tammera R. Diehm, Winthrop & Weinstine

Marshall & Moore Apartments Project Exceeds the Maximum Height Permitted in RM2 Zoning

The maximum height in the RM2 Medium-Density Multiple-Family Residential District is 50' above the "established grade" per Code Section 60.203.-B and per Table 66.231 in the Zoning Code. The proposed height is shown to be 50.0' from the established grade to the roof deck, with no margin for error. Project's Building Section Sheet (12/14/17) shows established grade to be 899.7' and the roof deck height is 949.7'. The garage entry is stated as 892.0'

The "underground" garage structure clearances are insufficient to meet the minimum required for the building code (minimum clearance for non-handicap parking structures is 7'-6"). Additionally, concrete garage structure floor depths appear understated. Once true scaled plans are shown, the building will prove to exceed the maximum height permitted in RM2 Zoning.



Marshall & Moore Apartments Project Requires Major Variance to Encroach in the Interior Side Yard Setbacks for Off-Street Parking Facility

Ordinances:

Section 60.217. – P. Parking facility, off-street. All areas, spaces and structures designed, used, required or intended to be used for the parking of more than three (3) motor vehicles. This definition is intended to include adequate driveways, accessways, parking bays, garages of a combination thereof, but does not include public roads, streets, highways and alleys.

Section 63.312. – Setback. Except as otherwise provided in section 66.442(a) or section 66.431(b) off-street parking spaces shall not be within a required front or side yard and shall be a minimum of four (4) feet from any lot line.

Section 63.106. – Projections into yards. There are no exemptions for parking facilities in the required interior side yard.

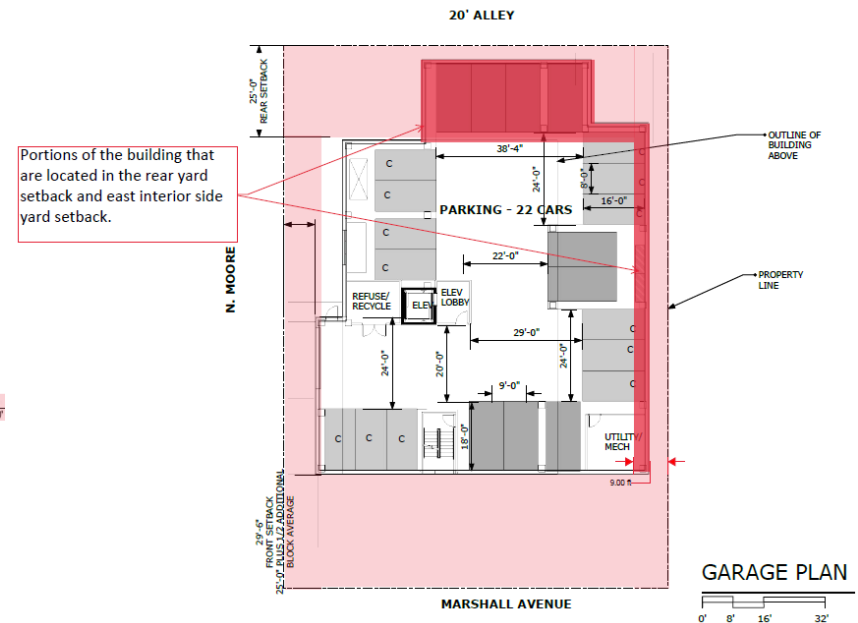
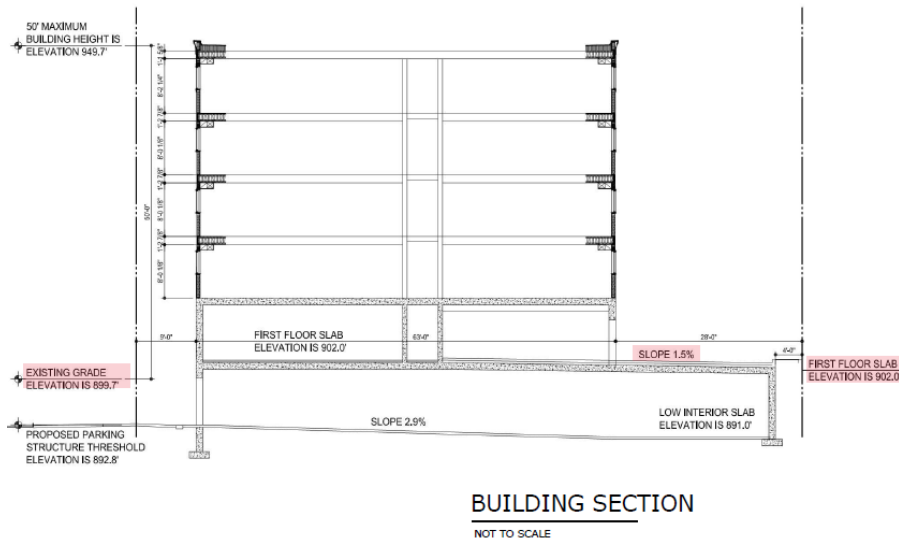
The proposed above-ground parking facility, accessed from the public alley, is located 4' from the east interior side property line and requires a major variance to encroach into the east interior side setback, required to be 9' from the property line.

EXHIBIT B - 1 of 1



Marshall & Moore Apartments Project Requires Major Variances to Encroach in the Rear and Interior Side Yard Setbacks for Parking Structure

The proposed first floor slab elevation is 902' according to the Building Section plan (12/14/17). The established grade elevation is 899.7'. Therefore, the "underground" parking structure is 2.3' taller than established grade nearest the structure and 1.94' above established grade at the eastern edge (1.5% slope). As such, the structure fails to comply with the side and rear yard building setbacks required for the above-grade portions of the principal structure.



The Marshall & Moore Apartments Project Requires Major Variance to Increase the Maximum Lot Coverage

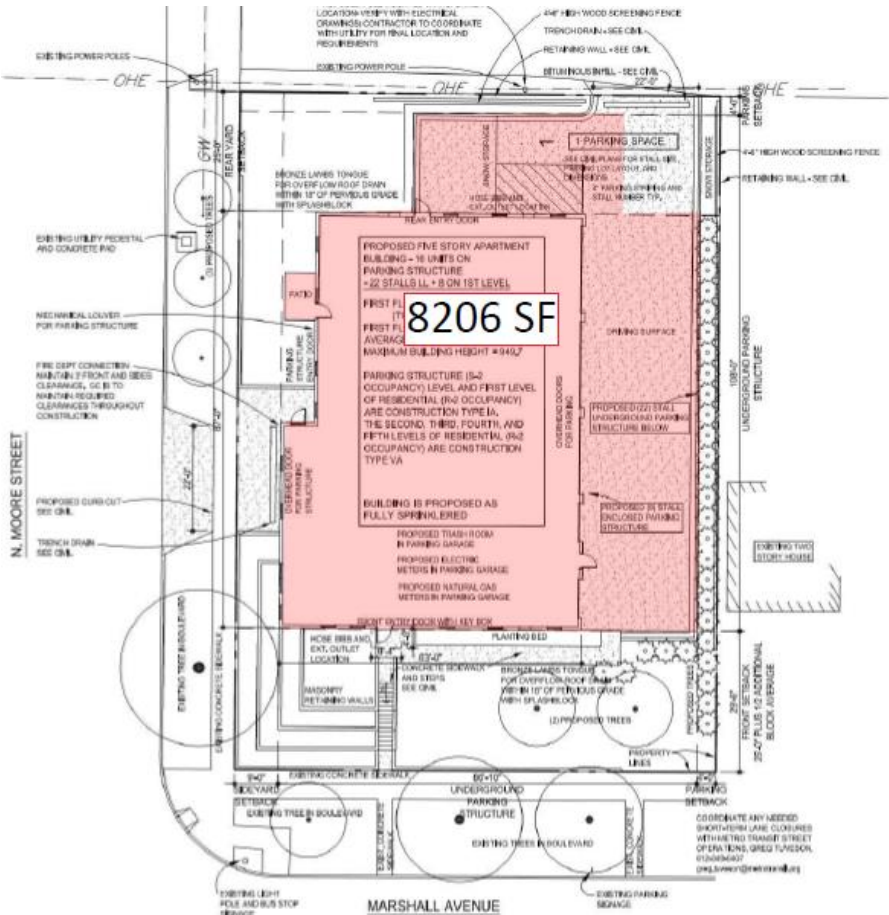
Ordinances:

Section 66.232. In residential districts, principal buildings shall not cover more than thirty-five (35) percent of any zoning lot.

The Marshall & Moore Apartments Project:

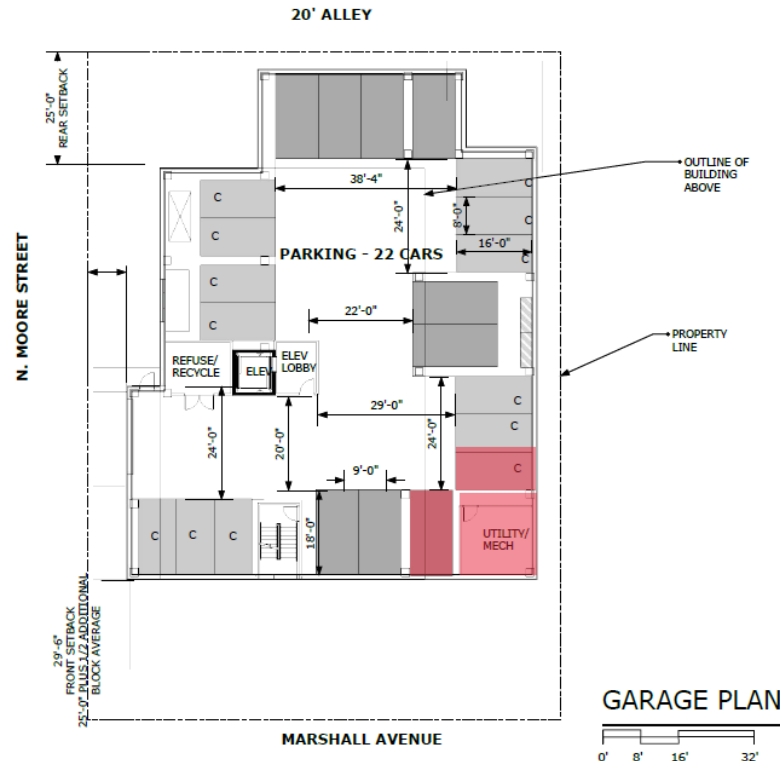
As previously demonstrated, the “underground” parking structure is 2.3’-1.94’ taller than established grade (see Exhibit C). Therefore, the entire footprint of the structure, including the “underground” parking structure, shall be included in the calculation for lot coverage.

8,206 SF / 15,169 SF = .541 OR 54.1% of the total site area



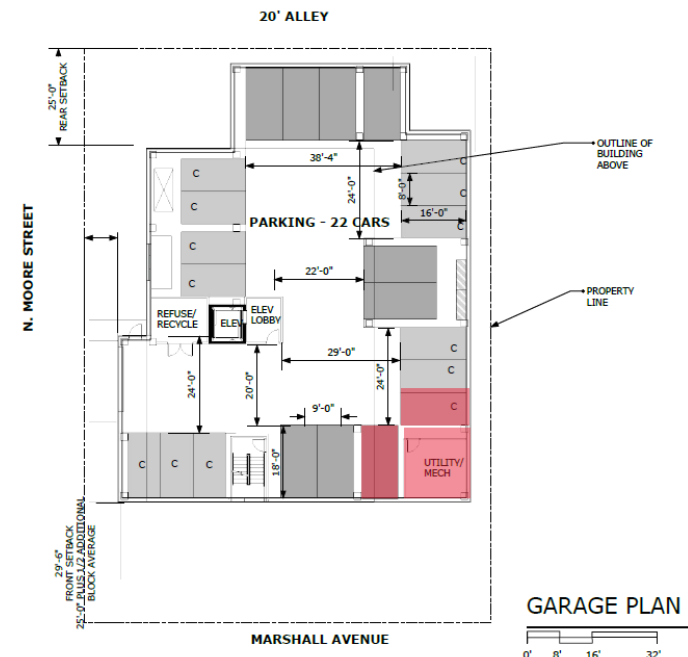
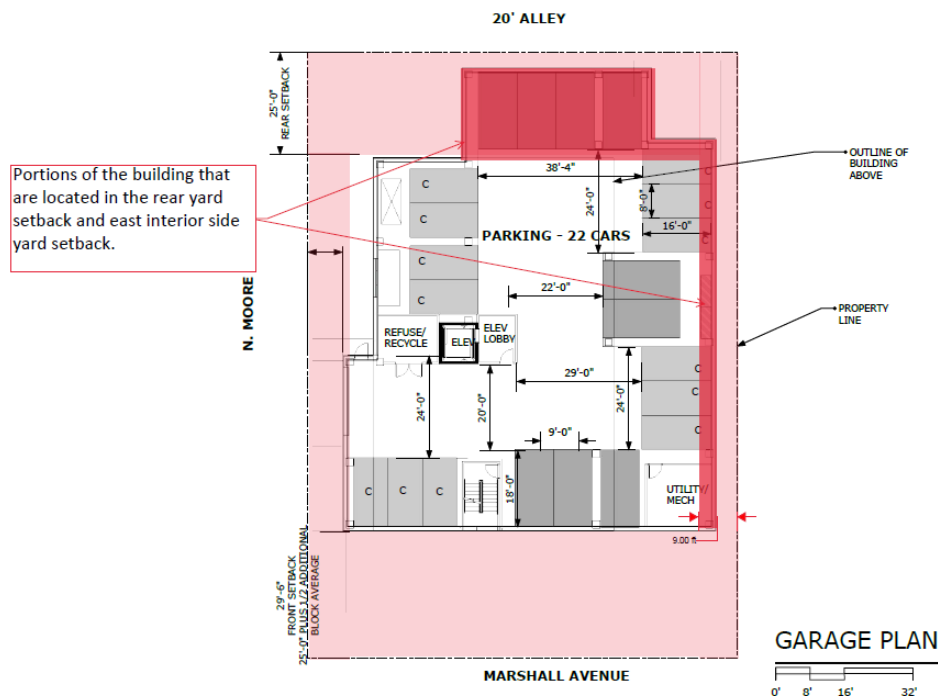
Marshall & Moore Apartments Project Does Not Meet Minimum Off-Street Parking Requirements

The proposed utility and mechanical room is located in the southeast corner of the “underground” parking structure. This room is flanked on both sides with parking spaces, rendering the room inaccessible. This will result in the loss of 1 parking stall to accommodate the required access to the utility and mechanical room.



Marshall & Moore Apartments Project Does Not Meet Minimum Off-Street Parking Requirements

As previously demonstrated, the “underground” parking is above the established grade (see Exhibit C) and 10 stalls do not comply with building setbacks and the utility and mechanical room is inaccessible (see Exhibit E), which results in the loss of 1 more parking stall. Only 20 of the proposed 31 parking stalls would not require a major variance, as shown in the Project application. A minimum of 30 parking stalls are required for 13 – 4-bedroom units and 3 – 3-bedroom units. The Project requires a variance to reduce the minimum off-street parking from 30 spaces to 20 spaces.



Marshall & Moore Apartments Project Requires Major Variance to Exceed the Maximum Density in RM2

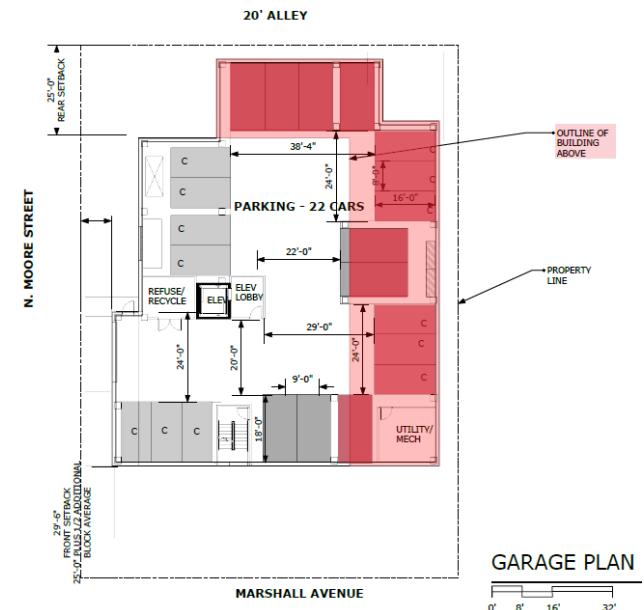
Ordinance:

Note (c) to Zoning Code Table 66.231 states parking spaces within a multiple-family structure or otherwise completely underground qualify for the density bonus.

The Project application requests a density bonus for 30 enclosed stalls. The “underground” parking structure is not located within the envelope of the multiple-family structure or otherwise completely underground (see Exhibit C). Therefore, the 13 spaces in the “underground” parking garage shown below do not qualify for a density bonus. This determination is further supported by the interpretation provided by DSI on November 26, 2014, regarding 2136-2144 Grand Avenue.

The position of the Zoning Administrator is that a literal reading of the Zoning Code as well as a common sense interpretation says that your proposed garages would not qualify for the lot area bonus:

- The Zoning Code defines Structured Parking as “Off-street parking that is placed within a ramp, deck, underground, enclosed building, or tuck-under garage.” It also says a lot area bonus can be given for “parking spaces within an above-ground parking structure, except for the top level.” (Emphasis added.) So even if it was determined that your garages were Parking Structures, the second floor are not proposed to be used for parking and so they cannot be considered as part of a “Parking Structure” under the definition. That leaves the first floor parking as the only (and therefore top) level of parking and this clearly cannot be counted for the lot area bonus.
- The principal underlying the lot area bonus is that a residential development that puts land needed for parking to use for additional levels of either parking or housing warrants a bonus of an additional units. Essentially, when the land area is used multiple times, more of the property remains for landscaping, storm water infiltration, and light and air for the neighbors. Your proposed garages do not meet this standard.



Marshall & Moore Apartments Project Requires Major Variance to Exceed the Maximum Density in RM2

The Project application requests a density bonus for 30 enclosed stalls. 13 spaces in the “underground” garage do not qualify for a bonus as they do not meet the standards in the Notes to Table 66.231 (c). One additional stall in the “underground” parking structure must be eliminated due to accessibility conflicts with the utility/mechanical room (see Exhibit E). As such, only 16 spaces would qualify for the bonus. The reduced bonus results in a maximum of 13 apartment units allowed; 3 units fewer than proposed.

LOT AREA DATA (See Project Site Plan)

13 Dwellings @ 1,500 SF	TOTAL	19,500 SF
 LOT DIMENSIONS FR AREA =	100 X 141 SF	14,171 SF
1/2 ALLEY=	10 X 99.8 SF	998 SF
16 ENCLOSED STALLS X 300	(BONUS)	<u>4,800 SF</u>
		19,969 SF

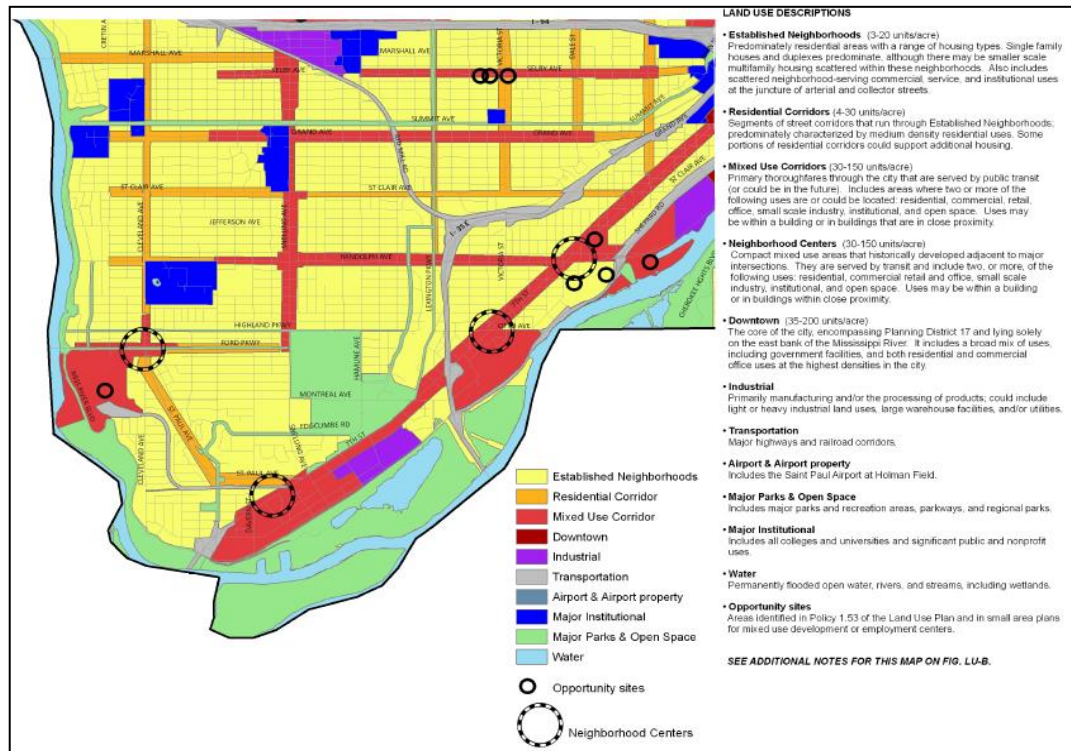
19,969 SF / 16 Dwelling Units = 1,248 SF/DU where 1,500 SF/DU is Required

Analysis of Findings Required for Site Plan Review

1. The city's adopted comprehensive and development or project plans for sub-areas of the city.

At 49 units per acre, Marshall & Moore Apartments density exceeds the St. Paul's *Generalized 2030 Future Land Use Comprehensive Plan*. Marshall Avenue is a designated Residential Corridor in the St. Paul's *Generalized 2030 Future Land Use Comprehensive Plan*. The guided density range for Residential Corridors is 4-30 dwelling units per acre and expressly stated in Strategy 1.1 and the Land Use Table.

FIGURE LU-K. GENERALIZED 2030 LAND USES, SOUTHWEST SECTOR



Analysis of Findings Required for Site Plan Review

1. *The city's adopted comprehensive and development or project plans for sub-areas of the city (continued).*

Strategy 1.1 from the Generalized 2030 Future Land Use Comprehensive Plan

LU 1.1 Guide the development of housing in Established Neighborhoods, commercial areas within Established Neighborhoods, and in Residential Corridors. This policy is intended to provide for the development of housing in Established Neighborhoods, Residential Corridors and adjacent commercial areas consistent with the prevailing character and overall density of these areas. The density goals are residential development of 3-20 dwelling units per acre in Established Neighborhoods and residential development of 4-30 dwelling units per acre in Residential Corridors and adjacent commercial areas.

LU 1.2 Permit high density residential development in Neighborhood Centers, Mixed-Use Corridors, the Central Corridor, and Downtown. Residential development in the Central Corridor is addressed on page LU-14. For Mixed-Use Corridors, the City should permit residential development of 30-150, including Neighborhood Centers within Mixed-Use Corridors. Residential development in Downtown should be permitted at a density of 35-200 dwelling units per acre.

Residential corridors are specifically guided for medium density. Conversely, high density residential development is guided for Neighborhood Centers, Mixed-Use Corridors, the Central Corridor, and Downtown. For Mixed-Use Corridors such as Snelling, University, and vast segments of Selby, Grand, and Randolph Avenues, and West 7th Street, the City should permit residential development of 30-150 dwellings/acre. Likewise, Neighborhood Centers and Mixed-Use Corridors would expect the same density range (30-150 dwellings/acre). 49 dwellings per acre, as designed in the Marshall & Moore Apartments, is consistent with development density in Downtown, which allows 35-200 dwellings/acre. **The proposed density of 49 units per acre is inconsistent with the St. Paul Comprehensive Plan for a Residential Corridor, including Marshall Avenue.**

Analysis of Findings Required for Site Plan Review

1. The city's adopted comprehensive and development or project plans for sub-areas of the city (continued).

FIGURE LU-Q. SAINT PAUL RESIDENTIAL USES BY 2030 LAND USE CATEGORY

From Saint Paul Land Use Table in 5-Year Stages

Saint Paul Residential Uses by 2030 Land Use Category

Residential Land Uses	Min Density (units/acre)	Max Density (units/acre)	Total acres, 2030	Percent of land that is residential	Residential acres, 2030*	Gross acres, 2010-2030	Percent of residential acres projected to redevelop, 2010-2030	Net Residential acres to redevelop			Net new households, low buildout	Net new households, high buildout
								2010-2020	2020-2030	2010-2030		
Established Neighborhood	3	20	17,465	86%	15,020	-2.0	11.8%	-0.1	-0.1	-0.2	-4	-1
Residential Corridor	4	30	1,523	76%	1,157	-109.0	10.1%	-4.2	-4.2	-8.4	-251	-33
Mixed Use Corridor (Include NCs)	30	150	3,423	72%	2,465	1,525.0	13.3%	48.7	97.4	146.0	4,381	21,905
Downtown	35	200	326	75%	245	41.0	10.0%	1.0	2.1	3.1	108	615
Total			22,737		18,886	1,455.0		45.4	95.1	140.5	4,234	22,486

*NCs = Neighborhood Centers

* Residential acres for Established Neighborhood include the Met. Council-defined categories of Single Family Detached, Single Family Attached, Multifamily, Mixed Use Residential and Undeveloped. Residential acres for the remaining 3 land uses includes these categories as well as Mixed Use Commercial, and Retail and Other Commercial. Downtown includes all of the previous categories as well as Office.

Households Forecast (by Metropolitan Council)

	2000	2010	2020	2030
Housing units (households)	112,109	120,000	127,000	133,000

17,912 households forecasted citywide from 2010-30

September 29, 2009

The density range for Residential Corridors is emphasized as 4-30 dwelling units /acre, according to Figure LU-Q, St. Paul Residential Uses by 2030 Land Use Category in the St. Paul *Generalized 2030 Future Land Use Comprehensive Plan*. These categories are established to ensure the forecasted household projections by the Metropolitan Council have been met. The Comprehensive Plan refers only to density bonuses for affordable housing, but not enclosed parking. Therefore, it is not possible to inflate the lot area, based on a zoning code bonus. **The proposed density is 49 dwelling units per acre and is inconsistent with the St. Paul Comprehensive Plan.**

Analysis of Findings Required for Site Plan Review

1. *The city's adopted comprehensive and development or project plans for sub-areas of the city (continued).*

Union Park Community Plan (2016)

Land Use and Economic Development

Land Use Objectives and Strategies

LU2. Preserve the well-kept, traditional feel and scale of the neighborhood.

LU2.1 Maintain and establish zoning that preserves lower-density, single-family homes and duplexes outside of mixed-use corridors.

LU2.2 Encourage the continued use and rehabilitation of existing structures, districts, and landscapes to preserve the historic character of residential and commercial districts.

LU2.3 Ensure that new development fits within the character and scale of adjacent neighborhoods.

LU2.4 Preserve and increase the number of trees and green spaces within the neighborhoods and within new development; promote the creation of pocket parks, community gardens, and other public and public-private spaces.

The *Union Park Community Plan* land use objectives and strategies are intended to preserve the feel and scale of the neighborhood. Further, this Plan prioritizes the preservation of lower-density uses outside of Mixed-Use Corridors. Marshall Avenue is a Residential Corridor, not a Mixed-Use Corridor. Further, rehabilitation of existing structures to preserve the historic character is a strategy to ensure long-term compatibility with the surrounding neighborhood. Finally, LU2.3 states that new development shall fit within the character and scale. The proposed height is 20 feet/2-stories taller than structures along Marshall Avenue and will be over 56 feet above the public sidewalk. **The Project is inconsistent with the LU2. Land Use Objectives and Strategies in the *Union Park Community Plan*.**

Analysis of Findings Required for Site Plan Review

1. *The city's adopted comprehensive and development or project plans for sub-areas of the city (continued).*

Union Park Community Plan (2016)

Housing

Housing Objectives and Strategies

H1. Preserve Union Park's pedestrian-scale neighborhoods, while promoting a range of housing types and affordability to meet the needs of people at different life stages with different housing needs.

H1.1 Support multi-unit mixed-use development in mixed-use corridors that can accommodate higher density levels, while minimizing impacts on adjacent lower density areas, and discourage multi-unit housing and retail uses that are incompatible with single-family residential areas.

H1.2 Support efforts to develop a wide range of housing affordability levels, promoting more affordable housing along major transit routes including Snelling Bus Rapid Transit and the Green Line Light Rail line.

H1.3 Support housing development designed to promote pedestrian, bicycle, and public transit activity.

H1.5 Encourage owner occupation of single-family and multi-family homes.

The *Union Park Community Plan* housing objectives and strategies are intended to preserve the pedestrian-scale of the neighborhood and providing a range of housing types and affordability. The Plan mandates minimizing impacts on lower density areas and uses incompatible with single-family residential areas, outside of Mixed-Use Corridors. The proposed height is over 20 feet/2 stories taller than structures along Marshall Avenue and will be over 56 feet from the public sidewalk. The density is 49 units per acre, which exceeds all surrounding residential densities in the area. Further, the Plan encourages a range of affordability – there are no affordable units proposed. **The Project is inconsistent with H1. Housing Objectives and Strategies in the *Union Park Community Plan*.**

Analysis of Findings Required for Site Plan Review

1. *The city's adopted comprehensive and development or project plans for sub-areas of the city (continued).*

Union Park Community Plan (2016)

Housing

Housing Objectives and Strategies

H2. Preserve and improve the character and maintenance of Union Park's neighborhoods for the next 10 years and beyond.

H2.2 Encourage rehabilitation of existing housing stock.

H2.2a New residential construction shall be consistent with the character of the surrounding homes, while minimizing impact on the neighborhood.

H2.3 Identify methods to encourage property owners to enhance energy efficiency of homes and rely more upon renewable energy sources.

H2.4 Develop incentives that encourage resident upkeep of structures and landscaping.

H2.5 Study methods to hold absentee property owners more accountable for properties (i.e., through imposing a requirement that local caretaker contact information be filed with the City).

The *Union Park Community Plan* housing objectives and strategies are intended to preserve and improve the character and maintenance of the neighborhood. The Plan mandates minimizing impacts on the neighborhood and stating that new construction shall be consistent with the character of the surrounding homes. The exterior materials are predominately fiber cement board and metal panel – exteriors not found in the neighborhood. Brick is the primary exterior material found on all existing multiple-family dwellings in the immediate area. **The Project is inconsistent with the H2. Housing Objectives and Strategies in the *Union Park Community Plan*.**

Analysis of Findings Required for Site Plan Review

1. *The city's adopted comprehensive and development or project plans for sub-areas of the city (continued).*

Union Park Community Plan (2016)

Housing

Housing Objectives and Strategies

H3. Recognize and accommodate student-housing needs while respecting the rights and concerns of all community stakeholders (students, families, colleges, landlords, businesses, etc.).

H3.1 Develop incentive programs that foster responsible student-renters and responsible landlords.

H3.2 Engage the University of St. Thomas, Macalester College and Concordia University in an effort to clearly define and accommodate their current and anticipated student-housing needs.

H3.3 Explore ways to engage and educate the community about the needs, rights, responsibilities, and concerns associated with student housing.

H3.4 Explore ways to promote college and university student involvement in community affairs.

H3.5 Prioritize the development of multi-unit student housing in mixed-use corridors over the expansion of single-family rental units in traditional neighborhoods.

The *Union Park Community Plan* housing objectives and strategies recognize and accommodate student-housing needs, while respecting the rights and concerns of all community members. The Plan prioritizes new development of multi-unit student housing on mixed-use corridors over the expansion of single-family rental units in traditional neighborhoods. Marshall Avenue is a Residential Corridor, not a Mixed-Use Corridor. **The Project is inconsistent with the H3. Housing Objectives and Strategies in the *Union Park Community Plan*.**

Analysis of Findings Required for Site Plan Review

1. *The city's adopted comprehensive and development or project plans for sub-areas of the city (continued).*

Union Park Community Plan (2016)

Historic Preservation

HP1. Integrate historic significance into Union Park's housing, environmental, land use, and economic development decision-making processes, supporting preservation over demolition.

HP2. Identify, evaluate, designate, and preserve historic resources in the District.

HP2.1 Coordinate with the City's Heritage Preservation Commission and support and/or implement a survey to identify and evaluate the District's historic resources, including buildings, structures, objects, archaeological sites, districts, and landscapes, drawing on available funding sources such as the state Arts and Culture Heritage Fund.

HP2.2 Support the ongoing survey of historic resources and consider and engage the community on the designation of historic resources, such as buildings, structures, objects, archaeological sites, historic districts, and landscapes as Saint Paul heritage preservation sites or historic districts.

HP2.3 Promote ongoing preservation and continued use of all designated sites in the Union Park District.

HP2.4 Collect and inventory information on the District's history, including historical books and articles, information about historic buildings and businesses, and biographical information on significant people who have lived in the District.

The two existing structures are potential historic resources integral to establishing a Marshall Avenue or Merriam Park historic district in the future. The demolition of these resources will impact the harmonious and continuous pattern of historic structures along this corridor. **The Project is inconsistent with the HP2. Historic Preservation Objectives and Strategies in the *Union Park Community Plan*.**

Analysis of Findings Required for Site Plan Review

2. *Applicable ordinances of the City of St. Paul.*

See Exhibits A-F analyses of the following major variances:

- Major variance to increase the maximum permitted height in the RM2 District.
- Major variance to reduce the required east interior side yard setback for the parking facility from 9' to 4'.
- Major variance to reduce the required rear yard setback for the proposed “underground” parking structure from 25' to 4'.
- Major variance to reduce the required east interior side yard setback for the proposed “underground” parking structure from 9' to 4'.
- Major variance to increase the maximum lot coverage from 35% to 54.1% of the total site area.
- Major variance to reduce the minimum lot area per dwelling from 1,500 SF to 1,248 SF.
- Major variance to reduce the minimum off-street parking from 30 to 20 spaces.

3. *Preservation of unique geologic, geographic or historically significant characteristics of the city and environmentally sensitive areas.*

1977 Marshall Avenue, constructed in 1906:

- Architecturally significant structure, designed by Charles W. Beuchner (worked for renowned St. Paul architect Clarence H. Johnston); Beuchner’s notable commissions include three in use today on University Avenue:
 - Raymond/University Historic District (Specialty Building [1908] at Raymond and Carleton Place Lofts [1909]); and
 - Fire station at St. Albans Street (1908)

Analysis of Findings Required for Site Plan Review

3. *Preservation of unique geologic, geographic or historically significant characteristics of the city and environmentally sensitive areas (continued).*

1977 Marshall Avenue, constructed in 1906 (continued):

- Built for industrialist Albert A. Fry & wife, Agnes Loudon Fry (owned Minneapolis factory for the Loudon Machinery Co.)
- Original garage/carriage house still intact.
- Original exterior wood siding and intricate architectural detail and leaded glass windows largely intact.
- Interior has later modifications with some original architectural features intact.

1973 Marshall Avenue, constructed in 1906:

- Harry Metcalfe house.
- Architect: not listed on the original building permit.
- Most original exterior architectural elements intact, except has later, wide asbestos siding.
- Beautiful original interior woodwork, built-ins, and art glass windows are almost entirely intact.

1977 Marshall Avenue

Albert A. & Agnes Loudon Fry house, built in 1906

Architect: Charles W. Buechner (worked for Clarence H. Johnston; notable commissions include three used today on University Avenue: Specialty Building [1908] at Raymond, Carleton Place Lofts [1909], and fire station at St. Albans Street [1908])



1977 Marshall Avenue (View No. 2)

Albert A. & Agnes Loudon Fry house, with original carriage house behind

Architect: Charles W. Buechner



1973 Marshall Avenue
Harry Metcalfe house, built in 1906
Architect: Not shown on original building permit



Analysis of Findings Required for Site Plan Review

4. *Protection of adjacent and neighboring properties through reasonable provision for such matters as surface water drainage, sound and sight buffers, preservation of views, light and air, and those aspects of design which may have substantial effects on neighboring uses.*

Surface water drainage

- Over 3,100 SF of paved, impervious surface in the east side yard located 4' of the shared eastern property line.
 - Adjacent structure (1969 Marshall Avenue) to the east is located less than 2' from the shared property line.
 - The driveway slopes 1.5% towards the east and there is only a 4' wide buffer. This will undoubtedly cause off-site surface water drainage directly on to the adjacent property during storm events and snow melt, ensuring flooding of the property and basement of 1969 Marshall Avenue.

Sound and site buffers

- The proposed garages on the east wall will impact the adjacent properties with additional car noises and lights in a side yard. There is a 4'6" planned screen which will not be sufficient given the possible traffic and additional parking likely to occur on the driveway.
- The proposed balconies are a nuisance on the west elevation and the increased height of the structure over all others in the area cannot be mitigated. The upper balcony is 275 SF, which has an occupant allowance of up to 18 people, which will generate excessive noise and encourage large gatherings. The sounds project from the higher elevation for a greater distance. Additionally, gatherings on the balconies are likely to occur in hours when the on-site management office is closed, which will impact City resources as necessary to respond to nuisance complaints.

Analysis of Findings Required for Site Plan Review

4. *Protection of adjacent and neighboring properties through reasonable provision for such matters as surface water drainage, sound and sight buffers, preservation of views, light and air, and those aspects of design which may have substantial effects on neighboring uses (continued).*

Preservation of views, light and air:

- The developer demonstrated significantly greater shadows cast during the Union Park Land Use Committee meeting on 12/18/2017. The properties to the east and west will have full shadows cast on yards and houses, before and after midday. The proposed structure will be located on the north side of Marshall Avenue, casting shadows on neighboring properties in excess of 100' throughout the year. Alternatively, the existing character of the typical Marshall Avenue three-story building does not have these same impacts.
- There are no 5-story buildings located on Marshall Avenue, between Dale Street and Cretin Avenue. This building will be visible for several blocks in all directions.
- The proposed structure will exceed 56' from the public sidewalk and will exceed the heights of all other dwellings in the area by over 20'/2 stories.
- The proposed parking area on the east side of the property will negatively impact air quality due to car exhaust within 6' of windows on the adjacent property.

Substantial effects on neighboring land uses

- The property owner has indicated to the Union Park Land Use Committee (on 10/30/2017 and 12/18/2017) he will not assign parking to each of the proposed dwellings. Instead, parking will be charged separately and will be available on a 'first-come, first-served' basis. This will inherently lead to greater demand for on-street parking, where parking restrictions are in place near 4 Seasons A+ Elementary and St. Marks schools. Additionally, there is higher on-street parking demand due to religious institutions at Moore and Iglehart and Moore and Dayton Avenues.
- The on-site management office is intended for all of MCR Property Holdings LLC, and will expand the use of the property for noncompliant commercial purposes and increase traffic to the property.

Analysis of Findings Required for Site Plan Review

5. *The arrangement of buildings, uses and facilities of the proposed development in order to assure abutting property and/or its occupants will not be unreasonably affected.*

1969 Marshall Avenue

- The impacts of the proposed structure will reasonably affect the property owner and occupants of this property due to excessive stormwater runoff, noises, exhaust and lights from cars and dwellings and the visual impact and loss of views from the front and west sides of the property. See analysis in Finding 4 for additional information.
- The proposed driveway will undoubtedly be used for additional, outdoor parking. All of the parking on the east side of the building will be accessed via the public alley.
- The shadows cast on the entirety of the structure and yard occur at midday through sunset, all-year, as demonstrated by the shadow studies provided by the Applicant.
- The on-site management office is intended for all of MCR Property Holdings LLC, and will expand the use of the property for noncompliant commercial purposes and increase traffic to the property.

1972 and 1980 Iglehart Avenue and 1985 Marshall Avenue

- The impacts to light and air exceed a reasonable amount afforded to properties in the City of St. Paul. The mass of the structure will be fortress-like and over-bearing on the single- and two-family dwellings and their rear yards. There are no 5-stories along Marshall Avenue, between Dale Street and Cretin Avenue.
- The proposed drive aisles will inherently be used for additional, outdoor parking. All of the parking on the east side of the building will be accessed via the public alley introducing significant traffic impacts on a residential alley.
- The proposed structure will be located on the north side of Marshall Avenue, casting shadows in excess of 100' throughout the year.
- The proposed balconies are a nuisance on the west elevation and there are no mitigating strategies proposed by the Applicant.
- The on-site management office is intended for all of MCR Property Holdings, LLC, and will expand the use of the property for noncompliant commercial purposes and increase traffic to the property.

Analysis of Findings Required for Site Plan Review

6. *Creation of energy-conserving design through landscaping and location, orientation and elevation of structures.*

The applicant has failed to provide energy conserving design, including but not limited to, solar energy systems, geothermal heating, passive or highly insulated building envelopes.

7. *Safety and convenience of both vehicular and pedestrian traffic both within the site and in relation to access streets, including traffic circulation features, the locations and design of entrances and exits and parking areas within the site.*

- The property owner has indicated to the Union Park Land Use Committee on 10/30/2017 and 12/18/2017 that they will not assign parking to each of the proposed dwellings. Instead, parking will be charged separately and will be available on a 'first-come, first-served' basis.
 - This will inherently lead to greater demand for on-street parking as there is no assurances any of the occupants will use the enclosed parking structures if there is no fee to park on-street.
 - The immediate area does not have City permit parking in place.
 - Parking restrictions are in place near 4 Seasons A+ Elementary and St. Mark's schools, and at the intersection of Marshall and Moore for a city bus stop.
 - There are two nearby religious institutions (Evangelical Formosan Church and the Church of St. Mark), with either no or limited off-street parking available.
- On-site bicycle parking/storage is insufficient for 61 intended occupants. The plan shows 4 bike spaces.
- A Travel Demand Management Plan has not been provided, giving no means to analyze the actual demand for parking by residents, guests, on-site management and traffic created by the on-site management office.

Analysis of Findings Required for Site Plan Review

8. *The satisfactory availability and capacity of storm and sanitary sewers, including solutions to any drainage problems in the area of the development.*

The Project was reviewed at a site plan review meeting on Tuesday, November 7, 2017. The project has been significantly modified and updated since then, as required by Public Works, which impedes the analysis for this finding.

9. *Sufficient landscaping, fences, walls and parking necessary to meet the above objectives.*

- There is insufficient landscaping proposed to mitigate stormwater runoff to the east.
- The proposed landscape buffer is insufficient to provide a reasonable screen to adjacent properties from the proposed on-site parking area and driveways.
- See Exhibit E for parking analysis. Parking is insufficient as proposed by the applicant without approval of major variances.

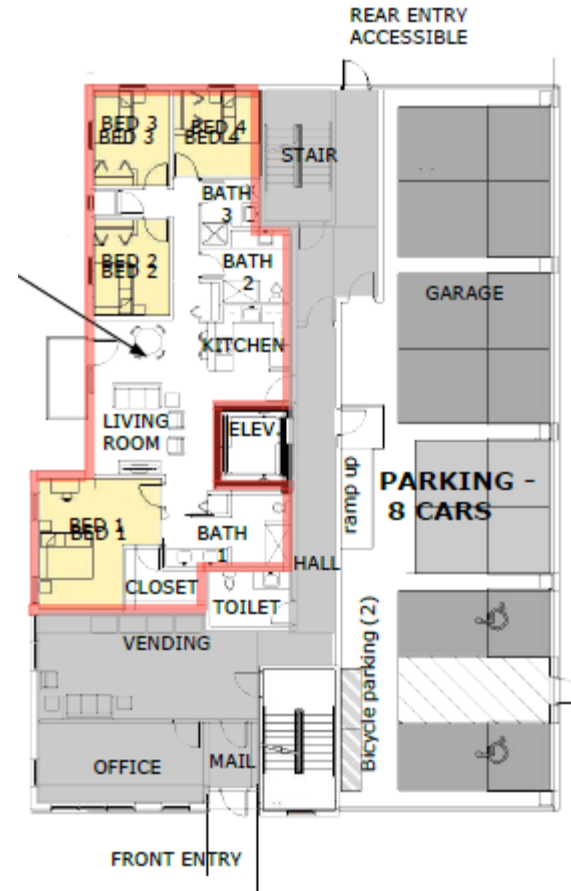
Analysis of Findings Required for Site Plan Review

10. *Site accessibility in accordance with the provisions of the Americans with Disabilities Act (ADA), including parking spaces, passenger loading zones and accessible routes.*

The public entrance is located on the south side of the proposed structure, facing Marshall Avenue. This entrance is not accessible from the public street or sidewalk. The proposed accessible entrance is at the rear of the building. **The project does not comply with Section 1105 Accessible Entrances of the 2015 MN Accessibility Code, because less than 60% of the entrances are accessible.**

Section 1105 Accessible Entrances

1105.1 Public entrances. In addition to *accessible* entrances required by Sections 1105.1.1 to 1105.1.5, at least 60 percent of all *public entrances* to each building, facility and tenant space shall be *accessible*.



Analysis of Findings Required for Site Plan Review

10. *Site accessibility in accordance with the provisions of the Americans with Disabilities Act (ADA), including parking spaces, passenger loading zones and accessible routes (continued).*

There are two parking stalls proposed in the parking garages along the east elevation. These accessible parking stalls are required to have a height clearance of 8' 2". The proposed parking garage doors are shown to be less than 8' in height, and therefore, do not comply with the Accessible Parking Section 1106 of the 2015 MN Accessibility Code.

Additionally, the accessible open parking space at the rear appears to have a slope greater than 1:48 (2%) and therefore does not comply with the Accessible Parking Section 1106 of the 2015 MN Accessibility Code.



11. *Provision for erosion and sediment control as specified in the "Ramsey Erosion Sediment and Control Handbook".*

No comment