

## ZONING COMMITTEE STAFF REPORT

1. **APPLICANT:** LeCesse Development Corporation **FILE #** 17-016-413
  2. **TYPE OF APPLICATION:** Site Plan Review **HEARING DATE:** 4/13/2017
  3. **LOCATION:** 246 – 286 Snelling Ave S, between St. Clair and Stanford Avenues
  4. **PIN & LEGAL DESCRIPTION:** 102823220164; 102823220165; 102823220166;  
102823220167; 102823220168; 102823220169 (Sylvan Park Addition to the City of Saint Paul,  
All of Lots 1-10 Block 4)
  5. **PLANNING DISTRICT:** 14 **PRESENT ZONING:** B2
  6. **ZONING CODE REFERENCE:** §61.402(c)
  7. **STAFF REPORT DATE:** 4/6/2017 **BY:** Tia Anderson
  8. **DATE RECEIVED:** 3/3/2017 **DEADLINE FOR ACTION:** 5/2/2017
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- A. **PURPOSE:** Site Plan Review for a five and one-half story mixed residential/ commercial building fronting Snelling Avenue South at St Clair Avenue.

As proposed, the Site Plan includes:

- 128 residential units
- 1,800 square feet of ground level commercial space
- 204 structured parking spaces

(See attached site plan.)

- B. **PARCEL SIZE:** 60,982 square feet (approx. 1.4 acres)
- C. **EXISTING LAND USE:** surface parking lot, mixed commercial
- D. **SURROUNDING LAND USE:**  
North: Mixed use (B2)  
East: Commercial, single-family residential (B2, R4)  
South: Commercial (B2)  
West: Mixed use, single family residential (B2, R4)

- E. **ZONING CODE CITATION:**
- §61.402(c) - Findings for site plan review.
  - §66.314 - Intent, T3 traditional neighborhood district.
  - §66.331 - Traditional Neighborhood District Density and dimensional standards.
  - §66.342 - Parking requirements in T3—T4 traditional neighborhood districts.
  - §66.343 - Traditional neighborhood district design standards.

- F. **HISTORY/DISCUSSION:**
- The property is currently developed with commercial buildings and associated surface parking, with limited landscaping. A surface parking lot at the northern end of the property serves several nearby businesses under a shared parking agreement.

- The A-line Bus Rapid Transit began operations along Snelling Avenue in 2016.
- A South Snelling Avenue Zoning Study is currently underway by the City.
- LeCesse Development has a purchase agreement for all properties proposed for redevelopment. LeCesse Development applied for rezoning the parcels to Traditional Neighborhood 3 (T3).
- Based on T3 zoning standards, the project is seeking a conditional use permit (to allow additional height) in conjunction with the project's Site Plan approval.
- A Site Plan Review Committee meeting was held for the proposed project on 3/31/2017.

G. **DISTRICT COUNCIL RECOMMENDATION:** The Macalester Groveland Community Council's Land Use Committee most recently met with the applicant on 3/22/2017, and received **comments from a number of residents**. The MGCC Land Use Committee submitted a letter with recommendations concerning the Site Plan in furtherance of the District Plan's goals (see attached MGCC letter dated April 5, 2017).

H. **FINDINGS:** Section 62.108(c) of the Zoning Code says that in "order to approve the site plan, the planning commission shall consider and find that the site plan is consistent with" the findings listed below.

1. *The city's adopted comprehensive plan and development or project plans for sub-areas of the city.*

The site plan meets this finding. The proposed development is consistent with the Comprehensive Plan. Applicable Land Use Strategies include:

- The future land use map of the Saint Paul Comprehensive Plan (2010) identifies Snelling Avenue and a segment of St Clair Avenue including the subject property and properties immediately to the east, as a Mixed-Use Corridor. Mixed-Use Corridors are intended to accommodate a mix of commercial, residential, civic, and institutional uses, with residential units at densities of 30-150 units per acre. The proposed development is approximately 92 units per acre. This Mixed-Use Corridor adjoins a Residential Corridor along St Clair Avenue and Established Neighborhood to the east.
- LU 1.2 – Permit high density residential development in Neighborhood Centers, Mixed-Use Corridors, the Central Corridor, and Downtown.
- LU 1.21 – Balance the following objectives for Mixed-Use Corridors through the density and scale of development: accommodating growth, supporting transit use and walking, providing a range of housing types, and providing housing at densities that support transit.
- LU 1.25 – Promote the development of more intensive housing on Mixed-Use Corridors where supported by zoning that permits mixed-use and multifamily residential development.
- LU 1.23 – Guide development along Mixed-Use Corridors... recognize community circumstances and preferences as stated in City adopted summaries of small area plans and district plans, while still providing additional housing opportunities at densities that support transit.

Macalester-Groveland Community Plan (2016) Strategy LU1 is to: Support land use that preserves Macalester-Groveland as a uniquely connected, walkable, mixed-use sustainable neighborhood with a pedestrian-oriented human-scale streetscape.

- LU 1.1 – Maintain and/or establish zoning that encourages compact development along mixed-use corridors that incorporate a mix of uses.
- LU 1.2 – Maintain and/or establish zoning that encourages higher-density (taller) development at the intersection of mixed-use corridors and lower-density (shorter) development at mid-block of mixed-use corridors.

2. *Applicable ordinances of the City of Saint Paul.*

The site plan meets this finding. If T3 rezoning and a CUP for height are approved, then the site plan meets the standards in the T3 zoning district for height, setbacks, density, parking, and design per the following:

- §66.314 - Intent, T3 traditional neighborhood district.
- §66.331 - Traditional Neighborhood District Density and dimensional standards.
- §66.342 - Parking requirements in T3—T4 traditional neighborhood districts.
- §66.343 - Traditional neighborhood district design standards.
- §63.110 – Building design standards.
- §63.207 - Parking requirements by use.

Conditions for site plan approval should include that: 1) architectural materials and detailing shall be of the same quality as shown on the site plan, or better, 2) building façade articulation along the bottom 25' of the building shall include elements that relate to the human scale, such as required door and window openings, texture, projections, awnings and canopies, ornament, public art, etc., and 3) The project meets all T3 traditional design standards, as stated in Sec. 66.343 of the Saint Paul Zoning Code, as determined by the Planning Administrator.

3. *Preservation of unique geologic, geographic or historically significant characteristics of the city and environmentally sensitive areas.*

The site plan meets this finding. The property is typical of the intent of a T3 neighborhood district, which provides for higher-density pedestrian- and transit-oriented mixed-use development. The property does not have unique historical or environmental significance.

4. *Protection of adjacent and neighboring properties through reasonable provision for such matters as surface water drainage, sound and sight buffers, preservation of views, light and air, and those aspects of design which may have substantial effects on neighboring land uses.*

The site plan meets this finding. Through the review process, the developer has made several site plan changes to improve the relationship of the development to the homes to the east and the streetscape along Snelling Ave. Specific to the findings:

- The stormwater system shall meet City standards for run-off rate control. The system will consist of pipes buried below grade located on the east side of the building in the landscaped setback area.
- The building's proposed rear setback is 12' from the 15' alley. The rear side of the building employs a step-back design. An outdoor terrace sits above the parking structure. The four residential floors are a reasonable distance from the abutting homes.
- The setback areas to the south and east will be landscaped. Existing boulevard trees will be protected where possible and new boulevard trees will be planted as required.
- The off-street parking will be provided as two levels of structured parking. Deliveries, trash and recycling will occur within the parking structure or street. There will be no alley access to the parking structure.

- The proposed site plan shall adhere to §66.343. – Traditional Neighborhood District Design Standards, including:
  - *Transitions to lower-density neighborhoods.* Transitions in density or intensity shall be managed through careful attention to building height, scale, massing and solar exposure.
  - *Building facade articulation.* The bottom twenty-five (25) feet of buildings shall include elements that relate to the human scale. These should include doors and windows, texture, projections, awnings and canopies, ornament, etc.
  - *Door and window openings - minimum and character.* Windows shall be designed with punched and recessed openings, in order to create a strong rhythm of light and shadow. Glass on windows and doors shall be clear or slightly tinted, and allow views into and out of the interior. Window shape, size and patterns shall emphasize the intended organization of the facade and the definition of the building.
  - *Materials and detailing.* Mixed use buildings shall be constructed of high-quality materials such as brick, stone, textured cast stone, tinted masonry units, concrete, glass or metal. All building facades visible from a public street or walkway shall employ materials and design features similar to those of the front facade.
  - *Parking location and design.* Off-street parking shall be provided within a principal structure, underground, or to the rear of buildings to the greatest extent possible. Vehicular entrances to structured parking shall be minimized so that they do not dominate the street frontage of the building.

5. *The arrangement of buildings, uses and facilities of the proposed development in order to assure abutting property and/or its occupants will not be unreasonably affected.*

The site plan meets this finding for the reasons listed in finding 4 above:

- Stormwater management system;
- Spatial buffer provided by 15' alley right-of-way and 12' rear setback.
- Step-back building design;
- Landscape buffers;
- Structured parking;
- No alley access to building or parking lot;
- Deliveries from the parking structure or street;
- Trash and recycling haulers from the parking structure or street.

6. *Creation of energy-conserving design through landscaping and location, orientation and elevation of structures.*

The site plan meets this finding. Higher density development is inherently more energy-conserving because it has fewer exterior walls and roof per dwelling unit than low density housing. The proposed development is located in a commercial area with good public transit and thus is conducive to walking, biking, and using public transit rather than driving.

7. *Safety and convenience of both vehicular and pedestrian traffic both within the site and in relation to access streets, including traffic circulation features, the locations and design of entrances and exits and parking areas within the site.*

The site plan can meet this finding. The site plan as submitted, proposes traffic for the development to enter the site at one of two parking ramp entrances: a right-in only on Snelling Ave and from either direction on St Clair Ave. Traffic will leave the site from the two parking ramp exits: right-out only on Snelling Ave and to either direction onto St Clair



Ave. There is no vehicular access from the alley. Pedestrian entrances to the parking structure are proposed off both Snelling and St Clair Avenues. Sidewalks will continue at grade across driveways. Bicycle parking will be provided in bike storage rooms inside the parking structure.

The applicant shall submit an updated Traffic Impact Study signed by a licensed traffic engineer for the project. The study will look at existing right-of-way conditions, estimated traffic generated by the project, turning movements, etc. Public Works will review and provide feedback on the Study.

In addition, a Travel Demand Management Plan shall be submitted for review and feedback by St Paul Smart Trips, the City's designated transportation management organization. The TDMP is intended to implement comprehensive plan policies calling for balance and choice in transportation options in order to reduce motor vehicle travel and thus traffic congestion in the city, enhance the efficiency of transportation facilities and infrastructure, improve air quality, conserve energy and enhance productivity.

A condition for site plan approval should include that the design for vehicle ingress and egress, and pedestrian access be approved by Public Works Transportation Planning and Safety and MnDOT. The design shall be informed by an updated Traffic Impact Study, Travel Demand Management Plan, and vehicle turning movement templates.

8. *The satisfactory availability and capacity of storm and sanitary sewers, including solutions to any drainage problems in the area of the development.*

The site plan meets this finding. The parcels were found to have a high water table, forcing the development to be less than a story below ground. Stormwater from the building would be piped to an underground system on the east side of the structure. The stormwater would go to the public storm sewer in St Clair Avenue at a controlled rate that meets City standards. The technical details for this system are being worked out between the applicant and Public Works Sewers. A condition for site plan approval should include that technical details for the stormwater management solution be approved by the Public Works Sewer Division.

9. *Sufficient landscaping, fences, walls and parking necessary to meet the above objectives.*

The site plan meets this finding. A condition of Site Plan approval should include that a final landscape plan be approved by the City Forester. The setback areas to the south and east will be landscaped and used for stormwater management. Existing boulevard trees will be protected where possible and new boulevard trees will be planted as required.

A minimum of 158 off-street parking spaces are required (5 for commercial plus 153 spaces for residential) less any allowed reductions for T3 zoning, shared parking, or bicycle parking.

- The zoning code requires off-street parking for commercial space at one parking space per 400 square feet of gross floor area. The off-street parking requirement for residential units is based on the number of rooms per unit.
- Required off-street parking may be reduced by 25% for residential uses in a T3 zoning district per Leg. Code Sec. 66.342. - Parking requirements in T3—T4 traditional neighborhood districts. In addition, on-street parking located along the frontage of a property may be used to meet parking requirements for that property.

Sufficient parking will be provided. 204 off-street parking spaces are proposed within two levels of structured parking. Of the 204 parking spaces, LeCesse Development has stated its intention to provide 29 off-street parking spaces that could be leased or somehow made available for use by neighboring establishments.

10. *Site accessibility in accordance with the provisions of the Americans with Disabilities Act (ADA), including parking spaces, passenger loading zones and accessible routes.*

The site plan meets this requirement. The floors of the commercial spaces and lobbies will match the sidewalk grades. The plan proposes 7 accessible parking spaces to meet the ADA standards required for lots of 201 – 300 parking spaces. The sidewalks have accessible crossings.

11. *Provision for erosion and sediment control as specified in the "Ramsey Erosion Sediment and Control Handbook."*

The site plan includes an erosion and sediment control plan that meets this standard.

#### **I. STAFF RECOMMENDATION:**

Based on the findings above, the staff recommends approval of the site plan to allow a mixed-use development at 246 - 286 Snelling Ave S., subject to the following conditions:

1. The lots for which the development is proposed are rezoned to T3 traditional neighborhood.
2. A Conditional Use Permit for height above 55' is approved, or the development revises the site plan to meet the T3 density and dimensional standards without a CUP.
3. Architectural materials and detailing shall be of the same quality as shown on the site plan, or better.
4. Building façade articulation along the bottom 25' of the building shall include elements that relate to the human scale, such as required door and window openings, texture, projections, awnings and canopies, ornament, public art, etc.
5. The project meets all T3 traditional design standards, as stated in Sec. 66.343 of the Saint Paul Zoning Code, as determined by the Planning Administrator.
6. The design for vehicle ingress and egress, and pedestrian access shall be approved by Public Works Transportation Planning and Safety and MnDOT. The design shall be informed by an updated Traffic Impact Study, Travel Demand Management Study, and vehicle turning movement templates.
7. Technical details for the stormwater management solution shall be approved by the Public Works Sewer Division.
8. Final landscape plan shall be approved by the City Forester.

**City of Saint Paul – Department of Safety and Inspections**  
**Site Plan Review Report**  
**Date of Report:** April 5, 2017  
**SPR File #** 17-016413  
**Address Location:** 246 – 286 Snelling Ave S  
**Project:** LeCesse Development Multi-Use Development



Thomas Hayden  
LeCesse Dev Corp  
650 S. Northlake Blvd, Ste 450  
Altamonte Springs, FL 32701

Nick Mannel  
Loucks  
7200 Hemlock Ln., Ste 300  
Maple Grove, MN 55369

Laurel Stone  
5G Studio  
800 Jackson St., Ste 500  
Dallas, TX 75202

On March 21, 2017, you met with City staff to discuss the site plan for the LeCesse Development Corp project including five and one-half story mixed residential/ commercial development with 128 residential unit, 1800 square feet commercial space and 204 structured parking spaces. The comments from that meeting are summarized below.

**1. Site Plan Process**

- a) Site Plan Review is a function delegated by the Saint Paul Planning Commission to City staff, however, a Site Plan may be referred to Planning Commission for public hearing. For this project the overall Site Plan will receive a public hearing at the Zoning Committee of the Planning Commission.
- b) Planning Commission approval of the Site Plan must be obtained before staff can sign-off on the Site Plan. Building permits will not be issued until the Site Plan has final approval and staff sign-off.
- c) Provide a pdf version of the updated Site Plan package based on the comments in this letter for review by the Site Plan Review Committee.
- d) Per Minnesota State Statute 326, the final plans submitted shall be signed by the appropriate licensed Professional, i.e. PE, LA, RLS, etc., responsible for plan development.

**2. Zoning**

Reviewer: Larry Zangs/651-266-9082 [larry.zangs@ci.stpaul.mn.us](mailto:larry.zangs@ci.stpaul.mn.us)  
Reviewer: Tia Anderson/651-266-9086 [tia.anderson@ci.stpaul.mn.us](mailto:tia.anderson@ci.stpaul.mn.us)

**Comments:**

- a) LeCesse Development has a purchase agreement for the parcels included in the proposed redevelopment. Once the purchase agreement is finalized, the applicant shall file for combination of contiguous lots with Ramsey County.
- b) The propose use of the property as a mixed-use residential/commercial development is permitted at this location in a B2 Zoning District. However, in order to meet other requirements of the zoning code and achieve development goals, the property is seeking to be rezoned to Traditional Neighborhood 3 (T3).
  - Changes to the zoning classification of the property requires (2) two public hearings, one before the Saint Paul Planning Commission and one before City Council. Allow at least 60 days to the complete the rezoning process.
- c) In T3 Zoning District, for mixed-use development, required building height is as follows:
  - Structures shall be no more than twenty-five (25) feet high along side and rear property lines abutting RL-RT2 residential districts; structures may exceed this twenty-five (25) foot height limit if stepped back from side and rear property lines a distance equal to the additional height.
  - A maximum height of 55' is allowed without a Conditional Use Permit.
  - A maximum height of ninety (90) feet may be permitted with a conditional use permit. Structures shall be stepped back one (1) foot from all setback lines for every two and one-half (2½) feet of height over seventy-five (75) feet.

- d) The proposed building height is permitted with conditions at this location. A zoning conditional use permit (CUP) must be obtained before the proposed height may be established. Issuance of this permit is a decision of the Saint Paul Planning Commission. Consideration of this permit request, by the Commission requires a public hearing.
- e) *Document the Floor Area Ratio on the Site Plan.* A mixed-use development in a T3 District requires a FAR of .5 - 3.0. In calculating the area of a lot that adjoins a dedicated public alley, for the purpose of applying minimum lot area and maximum density requirements, one-half the width of such alley adjoining the lot shall be considered part of the lot.
- f) *Indicate the setback distances on the Site Plan.* In T3 Zoning District, for mixed-use development, required set-backs are as follows:
- The front setback is 0 – 10'
  - If walls of structures facing interior lot lines contain windows or other openings, yards of not less than six (6) feet shall be provided.
  - The setback shall be a minimum of thirteen (13) feet from the centerline of an adjoining alley.
- g) Required off-street parking may be reduced by 25% for residential uses in a T3 zoning district per Leg. Code Sec. 66.342. - Parking requirements in T3—T4 traditional neighborhood districts. In addition, on-street parking located along the frontage of a property may be used to meet parking requirements for that property.
- h) *Update the off-street parking table in the Site Plan* to reflect the number of residential units, commercial gross square footage, and off-street parking spaces proposed. The parking requirement for commercial space is one off-street parking space per 400 square feet of gross floor area. The off-street parking requirement for residential units is based on the number of rooms per unit.
- The existing off-street parking on the property includes a Shared Parking agreement between the current property owner and neighboring businesses. The LeCesse Development has stated its intention to include an extra 29 off-street parking spaces that could be leased or somehow made available for use by neighboring establishments.
- i) Parking spaces and passenger loading zones for persons with disabilities shall be designed in accordance with the provisions of the Accessibility Guidelines for Buildings and Facilities of the Americans with Disabilities Act (ADA).
- j) *A Travel Demand Management Plan is required* for any development providing one hundred (100) or more accessory off-street parking spaces.
- The TDMP is intended to implement comprehensive plan policies calling for balance and choice in transportation options in order to reduce motor vehicle travel and thus traffic congestion in the city, enhance the efficiency of transportation facilities and infrastructure, improve air quality, conserve energy and enhance productivity.
  - The TDMP will be reviewed by Saint Paul Smart Trips.
- k) Bicycle parking shall be provided in a convenient, safe, and secure location according to the greater of the following:
- Off-street parking facilities shall provide a minimum of one (1) secure bicycle parking space for every twenty (20) motor vehicle parking spaces, disregarding fractional bicycle spaces.
  - For dwelling units, a minimum of one (1) secure bicycle parking space shall be provided for every fourteen (14) dwelling units. A fractional space up to and including one-half (½) shall be disregarded, and any fraction over one-half (½) shall require one (1) secure bicycle parking space.
- l) The project shall comply with general building design standards per Leg. Code Sec. 63.110:
- A primary entrance of principal structures shall be located within the front third of the structure; be delineated with elements such as roof overhangs, recessed entries, landscaping, or similar design features; and have a direct pedestrian connection to the street.
  - Building materials and architectural treatments used on sides of buildings facing an abutting public street should be similar to those used on principal facades.
  - *Provide the percentage of window and door openings on new facades on the Site Plan.* For principal buildings, above grade window and door openings shall comprise at least fifteen (15) percent of the total area of exterior walls facing a public street or sidewalk. Windows may be clear, translucent, or opaque.

- The visual impact of rooftop equipment shall be reduced through such means as location, screening, or integration into the roof design. Screening shall be of durable, permanent materials that are compatible with the primary building materials. Exterior mechanical equipment such as ductwork shall not be located on primary building facades.
- m) Continue to work with City Staff to ensure the development meets the building design Leg. Code Sec. 66.343. – Traditional Neighborhood District Design Standards, including:
- *Transitions to lower-density neighborhoods.* Transitions in density or intensity shall be managed through careful attention to building height, scale, massing and solar exposure.
  - *Buildings anchor the corner.* New buildings on corner lots shall be oriented to the corner and both public streets.
  - *Building facade articulation.* The bottom twenty-five (25) feet of buildings shall include elements that relate to the human scale. These should include doors and windows, texture, projections, awnings and canopies, ornament, etc.
  - *Entrance location.* There shall be a primary pedestrian building entrance on all arterial or collector streets. At a corner location where both streets are arterial or collector streets, this standard may be satisfied with a single entrance at the corner. In multi-tenant buildings, any ground floor use with street frontage shall have an entrance facing the street.
  - *Door and window openings - minimum and character.* Windows shall be designed with punched and recessed openings, in order to create a strong rhythm of light and shadow. Glass on windows and doors shall be clear or slightly tinted, and allow views into and out of the interior. Window shape, size and patterns shall emphasize the intended organization of the facade and the definition of the building.
  - *Materials and detailing.* Mixed use buildings shall be constructed of high-quality materials such as brick, stone, textured cast stone, tinted masonry units, concrete, glass or metal. All building facades visible from a public street or walkway shall employ materials and design features similar to those of the front facade.
  - *On-street parking.* Streets shall generally have parking on both sides to buffer pedestrians, calm traffic and supplement off-street parking unless the space is needed to accommodate traffic volume, emergency vehicles, transit or deliveries. Parking bump-ins are permitted in special cases (such as adjacent to large development sites) in conjunction with a redevelopment project that has at least three-hundred (300) feet of street frontage.
  - *Parking location and design.* Off-street parking shall be provided within a principal structure, underground, or to the rear of buildings to the greatest extent possible. Vehicular entrances to structured parking shall be minimized so that they do not dominate the street frontage of the building.
  - *Entrance location for transit access.* New and existing retail, office and multifamily housing shall coordinate with the transit agency in locating bus stops and related improvements. Building entrances shall be located to provide easy access to bus stops and shelters.
  - *Street trees.* Street trees in the street right-of-way, as prescribed by the city forester and section 69.600 of the subdivision regulations, and other landscape improvements shall be provided along all streets at regular intervals to help define the street edge, buffer pedestrians from vehicles, and provide shade.
- n) Update the demo sheet to reflect buried overhead lines.
- o) Provide detail section of the retaining wall along the alley. Seek to tie into the new retaining wall section to the south.

### 3. Lighting

- a) Exterior lighting shall meet Zoning Code Sec. 63.116. - Exterior lighting.
- All outdoor lighting shall be shielded to reduce glare and shall be so arranged as to reflect lights away from all adjacent residential districts or adjacent residences in such a way as not to exceed three (3) footcandles measured at the residence district boundary.
  - All lighting in all districts used for the external illumination of buildings shall be placed and shielded so as not to interfere with the vision of persons on adjacent highways or adjacent property.



4. **Signs**

Reviewer: Ashley Skarda/651-266-9013 [ashley.skarda@ci.stpaul.mn.us](mailto:ashley.skarda@ci.stpaul.mn.us)

Comments:

- a) Multi-use developments require a Master Sign Plan as a means to ensure adequate signage for tenants.
- b) Business signs require a separate review and Sign Permit from the Department of Safety and Inspections. Site plan approval does not constitute approval of signs shown on the site plan. Contact Ashley Skarda of DSI Zoning regarding signs.

5. **Planning**

Reviewer: Josh Williams/651-266-6659 [josh.williams@ci.stpaul.mn.us](mailto:josh.williams@ci.stpaul.mn.us)

Comments:

Rezoning application and CUP for height are scheduled for Planning Commission public hearing at the April 13, 2017, Zoning Committee meeting.

6. **District Council**

The site is located in the District 14 Macalester Groveland Community Council. A copy of the site plan was provided to the District Council for comments. The project is meeting with District 14's Land Use Committee on March 22. Testimony is anticipated for the Planning Commission public hearing.

7. **Parkland Dedication**

As a part of this project, the City requires payment of a Parkland Dedication fee (in lieu of the property owner needing to dedicate a portion of the property as park land). The fee collected is used by the City for acquiring park land or other park capital improvements in the area.

Staff estimates the dedication fee will be \$106,232. This amount is calculated based on the net increase in the number of residential units being provided and the value of the property with a credit for the previous commercial uses. Payment is included with building and inspection fees when the building permit is ready to be issued.

8. **Public Works Records and Mapping**

Contact Number: 651-266-6150

Comments:

Once the process for combining lots has been initiated, please contact the City's Mapping & Records office at 651.266.6150 for assistance with address needs for the site.

9. **Public Works Construction**

Reviewer: Scott Brimer/651-266-6225 [scott.brimer@ci.stpaul.mn.us](mailto:scott.brimer@ci.stpaul.mn.us)

Comments:

No issues at this time.

10. **Public Works Transportation Planning**

Reviewer: David Kuebler/651-266-6217 [david.kuebler@ci.stpaul.mn.us](mailto:david.kuebler@ci.stpaul.mn.us)

Comments:

- a) Please be advised that a Temporary Pedestrian Access Route (TPAR) and/or a Temporary Traffic Control (TTC) plan may be required as part of the Right-of-Way (ROW) permitting process. Said TTC or TPAR plans must be approved by the City prior to the ROW Permitting office issuing a permit(s).
- b) Please provide turning movement exhibits showing the design and control vehicles accessing the site. The design vehicle may be something similar to a large passenger vehicle and the control vehicle may be a single unit truck.
- c) Please provide a sight triangle exhibit for review.
- d) Please provide a landscaping plan for review against the sight triangles.
- e) Please provide an updated survey that correctly labels Snelling Avenue as Minnesota Trunk Highway 51.

- f) Please extend the viewport of the plan sheets such that the north side of Stanford Ave can be referenced.
- g) Please provide an updated Traffic Impact Study signed by an Engineer licensed in Minnesota. Also provide a Travel Demand Management Plan (TDMP).
- h) Continue to work with the City regarding the accesses being proposed. Modifications may be need on the number and location based on the results of the revised traffic study.
- i) Include street signage plans with proposed street sign removals, additions, replacements.
- j) On Plan Sheet C1-2:
- Please show the curb sawcut line on Snelling for each instance of curb removal.
  - Please show removal areas on St. Clair.
  - Please provide information regarding the bus stop, i.e. removal by others.
  - Please clarify whether an easement will be need on the parcel adjacent to the south parcel line for work that is likely to impact said neighboring property.
  - Please provide information regarding protecting the existing retaining wall adjacent to the alley.
- k) On Plan Sheet C2.1 please provide more information regarding the retaining wall such as top of wall and bottom of wall elevations.
- l) Please remove references to the alley ROW being assumed. Based on the Survey the ROW is a known dimension.
- m) Update the Site Plan with the following notes or modifications to notes:
- Please replace Note 1 under "City of St. Paul Notes" with the following:  
SIGNING: Signs regulating parking and/or traffic on private property shall be installed by the property owner or contractor outside of the public right-of-way (ROW). Removal of signs within the public ROW shall be completed by the City. New signs or the reinstallation of existing signs, as approved by Public Works Traffic Engineering, regulating parking and/or traffic in the public ROW for this development shall be installed by the City at the expense of the development. Contact Rick Solheid of Public Works 651-266-9781 two weeks in advance of needed sign work.
  - Replace Note 8 under "City of St. Paul Notes" with the following":  
INSPECTION CONTACT: The developer shall contact the Right of Way inspector Dick Rohland at 651.485.1688 one week prior to beginning work to discuss traffic control, pedestrian safety and coordination of all work in the public right of way. Note: If a one week's notice is not provided to the City, any resulting delays shall be the sole responsibility of the Contractor.  
  
As part of the ROW permitting process, two weeks before any work begins that impacts the ROW in any way the developer shall provide to the ROW Inspector the name and contact information of the Construction Project Manager or Construction Project Superintendent. If this information is not provided there may be a delay in obtaining permits for the work in the ROW. Said delays will be the sole responsibility of the developer
  - Please change the contact information in Note 4 under "City of St. Paul Notes" from John McNamara to Don Bjorkman and add the following to the note:  
"Access to signal controller and lighting cabinets must be maintained at all times. If fencing is required for a job site, a key or other means of access must be provided to the City of St. Paul's Traffic Operations Department. Contact Don Bjorkman, General Foreman Signals and Lighting at 651.266.9780 for more information."
  - Please delete Note 11 under "City of St. Paul Notes" as it is covered by other notes.
  - Please eliminate Note 12 under "City of St. Paul Notes" as it references Kellogg Boulevard.
  - Add the following note to the plans:  
SAFE WORK SITE REQUIREMENTS: The Contractor shall provide a continuous, accessible and safe pedestrian walkway that meets ADA and MN MUTCD standards if working in a sidewalk area, and traffic control per MN MUTCD requirements for work in the public right of way.



- Add the following note to the plans:  
ENCROACHMENTS: Per Chapter 134 of the Legislative Code, no person shall construct and maintain any projection or encroachment within the public right-of-way.

Construction of the development that necessitates temporary use of the Right-of-Way (ROW) for construction purposes shall be limited to equipment, personnel, devices and appurtenances that are removable following construction. Encroachment permits will not be granted for devices such as tie backs, rock bolts, H-piles, lagging, timbers, sheet piling, etc. that the owner is seeking to abandon in the ROW.

Section 3201.3 of the Minnesota Building Code defers final authority of encroachments into public rights-of-way/public property to the local authority. City Legislative Code governs management of the public rights-of-way. Provided such installations are approved by Public Works, footings may be allowed to encroach into City ROW no more than twelve (12) inches at depths below eight (8) feet as provided for in Minnesota Building Code Section 3202.1. Said encroachments would require an encroachment permit from the City per Chapter 134 of the Legislative Code.

Encroachments into County or State ROW are not allowed unless authorization has been granted from said agency.

Encroachments installed in the ROW without authorization will be removed at no expense to the City/County/State.

- Add the following note to the plans:  
ROADWAY RESTORATION: As per the City's "Standard Specification for Street Openings" policy, restoration on roadway surfaces less than 5 years old will require full width mill and overlay or additional degradation fees. Degradation fees are determined by contacting the Right of Way Service Desk at (651) 266-6151. Pavement restoration shall be completed by the St. Paul Public Works Street Maintenance Division. All related costs are the responsibility of the developer/contractor. Contact Street Maintenance at (651) 266-9700 for estimate of costs for pavement restoration.

#### 11. MnDOT

Reviewer: Karen Scheffing/651-234-7794 [karen.scheffing@dot.state.mn.us](mailto:karen.scheffing@dot.state.mn.us)

##### Comments:

- A copy of the Site Plan was provided to MnDOT for review. See attached March 20, 2017 letter with MnDOT's feedback.
- Any use of or work within or affecting MnDOT right of way requires a permit.
  - Permit forms are available at: <http://www.dot.state.mn.us/metro/maintenance/permits.html>
  - Include one to one set of plans formatted to 11X17 with each permit application. Please submit/send all permit applications and 11X17plan sets to: [metropermitapps.dot@state.mn.us](mailto:metropermitapps.dot@state.mn.us).
  - Direct any questions regarding permit requirements to Buck Craig (651-234-7911) of MnDOT's Metro Permits Section.

#### 12. Metro Transit

Reviewer: Jake Rueter [jacob.rueter@metrotransit.org](mailto:jacob.rueter@metrotransit.org)

##### Comments:

- A copy of the Site Plan was provided to Metro Transit for review.
- If there is interest from the building owner/manager in hosting real time transit information on a screen they provide in their lobby, Metro Transit can provide transit information customized for this location for display. If interested, the owner can contact Transit Information, Laura Matson (612-349-7771).
- Please coordinate any needed short term lane closures with Metro Transit Street Operations, Greg Tuveson (612-349-5407).
- Coordinate with Metro Transit on any potential bus stop modifications or disruptions.

### 13. Public Works Sidewalks

Reviewer: Al Czaia/651-266-6108

[al.czaia@ci.stpaul.mn.us](mailto:al.czaia@ci.stpaul.mn.us)

#### Comments:

- a) Contractor is responsible for damage to the mainline sidewalk, curb, drive access and boulevard landscaping cause during the construction. Contractor advised to document pre-existing condition of the right of way prior to commencement of the construction.
- b) Sidewalk grades must be carried across driveways.
- c) Update the Site Plan with the following notes:
  - CONSTRUCTION IN RIGHT OF WAY: All work on curbs, driveways, and sidewalks within the public right of way must be done to City Standards and Specifications by a contractor licensed to work in the City right-of-way under a permit from Public Works Sidewalk Section (651-266-6108). Sidewalk grades must be carried across driveways.
  - RIGHT OF WAY RESTORATION: Restoration of asphalt and concrete pavements are performed by the Public Works Street Maintenance Division. The contractor is responsible for payment to the City for the cost of these restorations. The contractor shall contact Public Works Street Maintenance to set up a work order prior to beginning any removals in the street at 651-266-9700. Procedures and unit costs are found in Street Maintenance's "General Requirements - All Restorations" and are available at the permit office.

### 14. Public Works Sewers

Reviewer: Larry Ackerman/6510-266-6223

[Larry.Ackerman@ci.stpaul.mn.us](mailto:Larry.Ackerman@ci.stpaul.mn.us)

Reviewer: Anca Sima/651-266-6237

[anca.sima@ci.stpaul.mn.us](mailto:anca.sima@ci.stpaul.mn.us)

#### Comments:

- a) Verification needed that existing storm water rate control is maintained when Anca Sima, PW Sewers, returns from vacation.
- b) Finalize stormwater solution that satisfies requirements of PW Sewers, CRWD, as well as Plumbing code. Consider the depth of the stormwater pipes and need for insulation if above frost level due to grade changes, or modification of the grade and retaining wall along the alley.
- c) Sanitary and/or storm sewer service passing within 10 feet of the building are governed by the MN Plumbing Code. Specification for pipe material selection and notes for required air test of the piping, compliant with the MN State Plumbing Code, must be shown on the plan.
- d) Specify the pipe material. PVC required in the ROW: SCH40, SDR 26, c900.
- e) Update the Site Plan with the following notes:
  - SEWER REMOVAL/ABANDONMENT PERMIT: Plumbing Contractor to obtain "Removal Permits" from Public Works to cut off existing sewer connections services to the property. Call St Paul PW permit desk (651-266-6234) for information on obtaining this permit.
  - SEWER CONNECTION PERMIT: License house drain contractor to obtain (Sewer Connection Permit) to construct new sanitary and storm connection in street from main to the property. Call St Paul PW permit desk (651-266-6234) for information on obtaining this permit.

### 15. Water Quality/Erosion Control

Reviewer: Wes Saunders-Pearce/651-266-9112

[wes.saunders-pearce@ci.stpaul.mn.us](mailto:wes.saunders-pearce@ci.stpaul.mn.us)

#### Comments:

- a) Erosion and sediment control provisions are satisfactory as shown.
- b) Ensure that stormwater drainage into the underground parking ramp is controlled in accordance with Plumbing Code requirements.

### 16. Water Utility

Reviewer: Jeffrey Murphy/ 651-266-6276

[jeffrey.murphy@ci.stpaul.mn.us](mailto:jeffrey.murphy@ci.stpaul.mn.us)

#### Comments:

- a) Verify service sizing and location(s)
- b) Verify utilization of combination service or dedicated fire/domestic services



- c) Confirm with MNDOT whether Snelling can be opened up or not
  - Disconnects at property only due to MNDOT requirements would still result in the collection of charges for disconnects at main for each required cut off.
    - Plumbing permits then required for disconnects at property
- d) Verify with St. Paul Fire if any hydrants must be added
- e) Services to be cut off at the water main (or capped as property per MNDOT) as discussed:
  - 246 S SNELLING AVE – 1" CP – 57 E E SNELLING-IN FROM ST CLAIR
  - 250 S SNELLING AVE - 5/8" LD - 82 S S ST CLAIR
  - 252 S SNELLING AVE – 2" CP - 102 1/2 S S ST CLAIR 35 1/2 E W SNELLING
  - 260 S SNELLING AVE – 2" CP - 55 N N BERKLEY
  - 264 S SNELLING AVE - 3/4" LD - 44 N N BERKLEY
  - 268 S SNELLING AVE – 1.5" CP - 6 1/2 N HYDT 5 1/2 S N BERKELEY
  - 272 S SNELLING AVE – 1.5" CP - 23 S S BERKELEY 38 E W SNELLING
  - 286 S SNELLING AVE - 3/4" LD - 138 N N STANFORD
- f) Provide profile view of all proposed utility crossings.
- g) Show stationing and degree of bends on plan sheets.
- h) The following work shall be performed by SPRWS on an actual cost basis. An estimate will be provided and payment in the amount of the estimate must be received before the work can be scheduled. Work of this type is currently being scheduled 4 to 6 weeks after payment and required signatures have been received:
  - Pipework within right of way for *size type*.
  - Pipework within right of way for *size type*.
  - Cut off of existing unused water services at the main.
  - Inspection of water facility work performed by owner's contractor.
- i) Ratio of fire suppression to domestic takeoff must be no less than 4:1.
- j) Plumbing permit applications to be made with SPRWS at 1900 Rice Street, Saint Paul, MN.
- k) Before construction of a new water service can be scheduled, SPRWS must receive a Water Service Contract signed by the owner and all required payments.
- l) Provide completed project data sheets to determine meter sizing.
- m) Furnish one set of interior fire suppression mechanical plans for review and approval by SPRWS plumbing inspection unit.
- n) Furnish one set of revised site plans for review. Following approval by SPRWS, furnish one set of approved plans.
- o) Update the Site Plan with the following notes:
  - Water services to be installed according to SPRWS "Standards for the Installation of Water Mains.
  - A four-sided trench box is required on all excavations deeper than 5 feet where underground work or inspection is to be performed by SPRWS. Ladders are required and must extend 3 feet above the surface of the trench. Sidewalks, pavements, ducts and appurtenant structures shall not be undermined unless a support system or another method of protection is provided. Trenches in excess of 20 feet in depth must be signed off by a registered professional engineer. Excavated material must be kept a minimum of 2 feet from the edge of the trench.
  - Maintain 8 feet of cover over all water mains and services.
  - Pipe material for 8" Ductile Iron Pipe must be Class 52, Pipe material for 6" and 4" Ductile Iron Pipe must be Class 53. The exterior of ductile iron pipe shall be coated with a layer of arc-sprayed zinc per ISO 8179. The interior cement mortar lining shall be applied without asphalt seal coat.
  - Pipe must be wrapped in V-Bio Polywrap encasement.
  - Maintain 3 feet vertical separation between water and sewer pipes or a 12 inch separation with 4 inch high density insulation per SPRWS Standard Plate D-10 for typical water main offsets.
  - Refer to SPRWS "Standards for the Installation of Water Mains" Standard Plate D-11 for restrained pipe requirement.



- All pipe work inside of property to be performed by a plumber licensed by the State of Minnesota and Certified by the City of Saint Paul. SPRWS requires separate outside and inside plumbing permits for each new water service.
- All unused existing water services to be cut off by SPRWS. Excavation and restoration by owner's contractor. New water services will not be turned on until required cutoffs have been performed.
- Water facility pipework within right of way to be installed by SPRWS. Excavation and restoration by owner's contractor.
- The contractor providing excavation is responsible for obtaining all excavation and obstruction permits required by any governing authority.

## 17. Fire

Reviewer: Angie Wiese/651-266-8953 [angie.wiese@ci.stpaul.mn.us](mailto:angie.wiese@ci.stpaul.mn.us)

### Comments:

- Visibility of the Fire Department connection must be maintained on the address side of the building in a visible, accessible location at all times without obstruction by fences, bushes, trees, walls, or other objects for a minimum of 3 feet to the front and each side. This requirement is applicable for the duration of the construction time as well.
- A metal sign or collar with raised letters at least 1 inch in size must be mounted on all Fire Department connections serving fire sprinklers, standpipes, or fire pump connections. Such signs shall read AUTOMATIC SPRINKLERS or STANDPIPES or TEST CONNECTION or a combination thereof as applicable.
- Automatic Fire Sprinkler System Required. Plans and permits required by licensed contractor. Flow test is required to determine adequacy of water service for proposed fire suppression system. The fire prevention section of the Dept. of Safety and Inspections reviews sprinkler plans, issues permits and does the necessary inspections of the installation. Contact Katie Le Tourneau-Bjorge at 651-266-8954 with questions on obtaining this permit and the procedure for arranging a time to perform this test.
- Update the Site Plan with the following notes:
  - Contractor to maintain access to the fire department connection for fire department personnel at all times during the construction period.

## 18. City Forestry

Reviewer: Zach Jorgensen/651-632-2437 [zach.jorgensen@ci.stpaul.mn.us](mailto:zach.jorgensen@ci.stpaul.mn.us)

### Comments:

- Please provide a landscape plan for review by forestry staff. Additional comments will be provided following the review.
  - Note, new street trees will be required as part of this project. Tree species selections and planting details should be coordinated with the City Forester.
- Update the demo sheet to show the removal or protection of the three existing street trees located on Snelling Avenue.
  - Existing street trees to remain are to be protected at all times. Trees damaged or removed during construction shall be restored or replaced to the satisfaction of, and at no cost to, the City as determined by the Forestry manager.
  - The location of tree protection fences shall be shown on the plan.
  - A tree protection fencing detail shall be included in the plan set.
- Boulevard restoration is to include the following:
  - Where driveways, sidewalks or other surface paving are removed all concrete, asphalt and base materials shall be removed.
  - Boulevard soils are to be protected during construction. Soils not protected during construction are to be restored. Soil compaction due to construction activities shall be mitigated and soils restored prior to final grading.
  - Final grade shall be established with a minimum of 4" of topsoil.
- Concrete washouts shall not be located within the drip line of a tree.

e) Update the Site Plan with the following notes:

- The removal, pruning, and/or planting of trees on the public boulevard requires an approved permit from the City Forester (651-632-5129). Tree work must be completed by a licensed tree contractor.
- Construction supplies, materials, spoils, equipment, and vehicles shall not be stored or operated within the drip line of any public street tree or on turf boulevards without prior written approval from the City Forester. If the boulevard must be used for construction activities, site access routes, material storage or other related activities, protective measures approved by the City Forester shall be taken to reduce soil compaction and protect tree(s) from damage.
- **Street trees** shall be protected by establishing a tree protection zone using 4' tall fencing installed at the drip line of the tree. Tree protection fencing shall be installed prior to the start of any site work and maintained for the duration of the project. Proposed work within, or changes to the location of tree protection fencing shall be reviewed by the City Forester prior to alteration.
- Contractor shall contact the City Forester (651-632-2437), prior to demolition or other land disturbance associated with site construction, to verify tree protection measures.
- Tree planting details shall include the following:
  - Expose root flare and set at grade.
  - Remove burlap and ropes from top 1/3rd of root ball, cut wire basket down to second horizontal wire from the bottom, and dispose of off-site.
  - Contractor is responsible to maintain trees in a plumb position throughout the maintenance period.

19. **Parks and Recreation**

Reviewer: Paul Sawyer/651-266-6417

[paul.sawyer@ci.stpaul.mn.us](mailto:paul.sawyer@ci.stpaul.mn.us)

Comments:

- a) No concerns

20. **Capitol Region Watershed District**

Reviewer: Forrest Kelley/651-644-8888

[forrest@capitolregionwd.org](mailto:forrest@capitolregionwd.org)

Comments:

- a) The project area is an acre or more in size and will require a permit from the Capitol Region Watershed District. A copy of the watershed permit or other written authorization from the district **must be provided before the site plan** will be approved or any permits are issued, allowing construction to proceed.
- b) CRWD permit application #17-006 has been received and is currently under review. As of 3-6-17, 9 conditions remain before Board decision.

21. **MPCA Permit**

This project will be affecting more than one acre. A General Storm Water Permit for Construction Activity from the Minnesota Pollution Control Agency is required. No land disturbance activity for the project is allowed, until this permit is obtained and is in addition to any City or watershed district permits required. Call the Brian Green MPCA Statewide Compliance Coordinator for the Storm water Program MPCA at 507-206-2610 if you have questions about the process for obtaining this permit.



## 22. Plumbing

Reviewer: Rick Jacobs/651-266-9051 [rick.jacobs@ci.stpaul.mn.us](mailto:rick.jacobs@ci.stpaul.mn.us)

### Comments:

- a) Internal primary and secondary roof drains need to go to storm sewer. Rate control shall be met outside the building footprint.
- b) Finalize stormwater solution that satisfies requirements of PW Sewers, CRWD, as well as Plumbing code. Consider the depth of the stormwater pipes and need for insulation if above frost level due to grade changes, or modification of the grade and retaining wall along the alley.
- c) Sanitary and/or storm sewer service passing within 10 feet of the building are governed by the MN Plumbing Code. Specification for pipe material selection and notes for required air test of the piping, compliant with the MN State Plumbing Code, must be shown on the plan. This system must be reviewed and approved by Rick Jacobs, Senior Plumbing Inspector (651-266-9051) to ensure that it meets Plumbing Code standards.
- d) Tie area drains at parking garage entrances into the storm sewer.

## 23. Building Code Requirements

Reviewer: James Williamette/651-266-9077 [james.williamette@ci.stpaul.mn.us](mailto:james.williamette@ci.stpaul.mn.us)

- e) This proposal will require a building (grading) permit from this office to proceed with the grading activity.
- f) This proposal will require a building permit to proceed. The building permit is issued only after all necessary city staff have approved and signed off on the proposed design. In addition to the building permit, separate permits are required for any plumbing, electrical and mechanical work, elevator installation and any fire sprinkler modifications. These permits must be obtained and the work performed by city licensed contractors in each of the respective trades.
- g) One PDF and two sets of complete construction documents stamped by public works must be submitted with the building permit application to the DSI Main Office/ Permit Desk
  - The construction documents shall include architectural, structural, mechanical, electrical and plumbing plans signed and stamped by design professionals registered in the State of Minnesota. The architect shall provide a complete code analysis and a color-coded exit plan showing all fire rated walls and shafts and include exit access and travel distances. The plans shall have the energy code noted on the plans along with the compliance path chosen. The submittal should include compliance documents detailing how the energy code requirements are met.
  - Plans must be dimensioned, drawn to scale and sufficiently detailed to denote the scope of work to be performed and the method of construction.
  - Mechanical ventilation plans will need to be prepared by a mechanical engineer, registered with the State of Minnesota. In some cases, a "Master in the Trade" may prepare plans. The ventilation contractor should contact our senior warm air inspector Gary Reinsberg (651-266-9064) or by e-mail at [Gary.reinsberg@ci.stpaul.mn.us](mailto:Gary.reinsberg@ci.stpaul.mn.us) The Energy code and path must be noted on these plans also.
  - The plumbing and electrical contractors for this project should contact our office if they have questions about whether engineered plans need to be submitted with their permit request. The senior plumbing inspector is Rick Jacobs at 651-266-9051 [Rick.jacobs@ci.stpaul.mn.us](mailto:Rick.jacobs@ci.stpaul.mn.us) and the senior electrical inspector is Dan Moynihan at 651-266-9036 [Dan.monihan@ci.stpaul.mn.us](mailto:Dan.monihan@ci.stpaul.mn.us)

Note: **The building permit will not be issued until all signoffs are received** from Zoning, HPC, HVAC, Public Works, or other departments that are assigned to the project.

**24. Service Availability Charge (SAC)**

The proposed project will need a SAC determination before a building permit can be issued. You must submit a copy of the plans to the Metropolitan Council Environmental Services (MCES) for a SAC determination. Please see their website at <https://metro council.org> for additional information. If MCES determines that a SAC fee is due, the City will collect that payment with the building permit fee. SAC Questions and Determination Review Submittal Information email:

[SACprogram@metc.state.mn.us](mailto:SACprogram@metc.state.mn.us) or call 651-602-1770 to speak to a SAC representative.



Report Prepared By:

Tia Anderson

Senior City Planner

Dept. of Safety and Inspection

375 Jackson St - Suite 220

Saint Paul MN. 55101-1806

Direct: 651-266-9086

E-Mail: [tia.anderson@ci.stpaul.mn.us](mailto:tia.anderson@ci.stpaul.mn.us)

Cc: File, Site Plan Review Staff, CRWD, Metro Transit, MnDOT, City Council Ward 3 Office, District 14  
Macalester Groveland Community Council



Minnesota Department of Transportation –Metro District  
1500 W County Road B2  
Roseville Mn 55113  
March 20, 2017

Larry Zangs  
City of St. Paul  
Department of Public Safety and Inspections  
375 Jackson Street, Suite 220  
St. Paul, MN 55101

Name: Snelling and Saint Clair redevelopment  
MnDOT Review Number: P17-021  
Location: SW quad MN51 and Saint Clair Ave  
City: Saint Paul County: Ramsey  
CS: 6125

Dear Mr. Zangs:

The Minnesota Department of Transportation (MnDOT) has reviewed the above referenced plat in compliance with Minnesota Statute 505.03, subdivision 2, Plats. Before any further development, please address the following issues:

***Traffic:***

Additional information is needed from the developer regarding the potential impacts to MN51.

- Show the lane widths and parking lane width on northbound MN51.
- Show the parking areas and bus stops and proposed “no parking”.
- Please indicate if parking be eliminated to accommodate a right turn lane at the right in/right out.
- Show details of the proposed “splitter island” at the proposed right in/right out access.
- Please indicate if the median will be raised or striped.
- Show turning movement diagrams for all turns in/out of the development on both Snelling and St. Clair.

For questions regarding these comments please contact Gayle Gedstad at 651-234-7815 or [gayle.gedstad@state.mn.us](mailto:gayle.gedstad@state.mn.us).

***Water Resources***

A MnDOT drainage permit will be required to ensure that current drainage rates to MnDOT right-of-way will not be increased. The drainage permit application, including the information below, should be submitted to:

Minnesota Department of Transportation  
Metropolitan District - Permit Office  
1500 W. County Road B-2  
Roseville, MN 55113

The following information must be submitted with the drainage permit application:

- 1) A grading plan showing existing and proposed contours.
- 2) Drainage area maps for the proposed project showing existing and proposed drainage areas. Any off-site areas that drain to the project area should also be included in the drainage area maps. The direction of flow for each drainage area must be indicated by arrows.



- 3) Drainage computations for pre and post construction conditions during the 2, 10, 50 and 100 year rain events.
- 4) Time of concentration calculations.
- 4) An electronic copy of any computer modeling used for the drainage computations.
- 5) See also the attached Drainage Permits Checklist for more information.

Once a drainage permit application is submitted, a thorough review will be completed and additional information may be requested. Please direct questions concerning drainage issues to Nick Olson (651-234-7542) or ([nicholas.olson@state.mn.us](mailto:nicholas.olson@state.mn.us)) of MnDOT's Water Resources section.

***Right of Way:***

Please contact Todd Grugel (651-366-3531) or Joe Zilka (contact info below) regarding ADA requirements. If this property is to be platted it would be helpful to know the ADA requirements so that the plat can incorporate some dedicated right of way if possible.

For questions regarding these comments please contact Matt Aguirre at 651-234-7599 or [matthew.aguirre@state.mn.us](mailto:matthew.aguirre@state.mn.us).

***ADA:***

With the removal of the driveways across from Berkeley ave, the developer should add a curb ramp to receive the pedestrians travelling north-south on Berkley who will access the development. Please reference MnDOT ADA curb standard plans and driveway/sidewalk plans attached to this letter. Also, please see the attached map with additional details.

For questions regarding these comments please contact Joe Zilka at 651-366-3311 or [joseph.zilka@state.mn.us](mailto:joseph.zilka@state.mn.us).

***Design***

- Please reference MnDOT Standard Plan 5-297.254, Driveway and Sidewalk Details for the access on to MN51. As can be seen from the detail, the sidewalk is integrated into the entrance. No pedestrian curb ramps are used.
- The Demolition plan, Sheet C1-2, shows the existing bus shelter removed. It does not show on Site Plan, Sheet C2-1. Please explain whether it will be replaced.
- Add a note to protect the existing pedestrian curb ramp at St. Clair and match the existing grades.
- Include tree protection on the plan as there are several trees in the boulevard.
- Please include plans for pedestrian accommodations through the work zone during construction.
- There will be median project constructed south of this site on Snelling. Identify plans for medians within this section of MN51.

For questions regarding these comments please contact Nancy Jacobson at 651.234.7647 or [Nancy.L.Jacobson@state.mn.us](mailto:Nancy.L.Jacobson@state.mn.us)

***Permits:***

Cut and cap the water services at the property line. MN51 was recently resurfaced and MnDOT doesn't want 6 large patches in the new bituminous. Besides a drainage permit, an access permit is required. Permit #1723 is required for the sanitary connection. Permit #1723 is required for the access and water service removals. In addition, any use of or work within or affecting MnDOT right of way requires a permit.

Permit forms are available from MnDOT's utility website: <http://www.dot.state.mn.us/utility/forms.html>. Please include one 11x17 plan set and one full size plan set with each permit application.

Please direct any questions regarding permit requirements to Buck Craig (651-234-7911) of MnDOT's Metro Permits Section.

**Review Submittal Options:**

MnDOT's goal is to complete the review of plans within 30 days. Submittals sent in electronically can usually be turned around faster. There are four submittal options. Please submit either:

1. One (1) electronic pdf. version of the plans. MnDOT can accept the plans via e-mail at [metrodevreviews.dot@state.mn.us](mailto:metrodevreviews.dot@state.mn.us) provided that each separate e-mail is under 20 megabytes.
2. Three (3) sets of full size plans. Although submitting seven sets of full size plans will expedite the review process. Plans can be sent to:

MnDOT – Metro District Planning Section  
Development Reviews Coordinator  
1500 West County Road B-2  
Roseville, MN 55113

3. One (1) compact disc.
4. Plans can also be submitted to MnDOT's External FTP Site. Please send files to: <ftp://ftp2.dot.state.mn.us/pub/incoming/MetroWatersEdge/Planning> Internet Explorer doesn't work using ftp so please use an FTP Client or your Windows Explorer (My Computer). Also, please send a note to [metrodevreviews.dot@state.mn.us](mailto:metrodevreviews.dot@state.mn.us) indicating that the plans have been submitted on the FTP site.

If you have any questions concerning this review, please contact me at (651) 234-7784.

Sincerely,

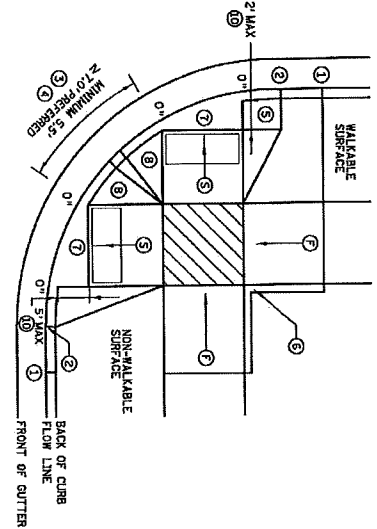


Karen Scheffing  
Principal Planner

**Copy sent via E-Mail to:**

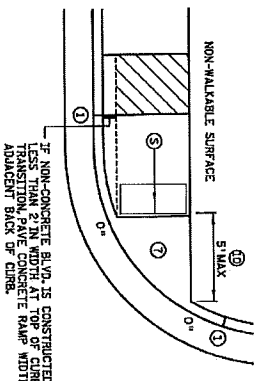
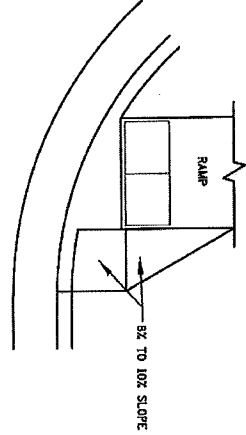
Nick Olson, Water Resources  
Gayle Gedstad, Traffic  
Mark Lindeberg, Area Engineer  
Matt Aguirre, Right of Way  
Nancy Jacobson, Design  
Buck Craig, Permits  
Tia Anderson, Saint Paul  
Russ Owen, Met Council  
Gregg Gund, Ramsey County Surveys  
Matt Wernet, Surveys



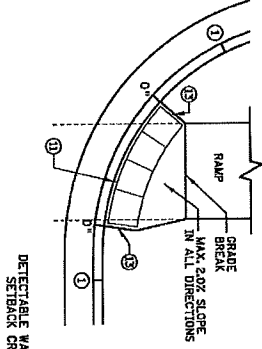


COMBINED DIRECTIONAL ③

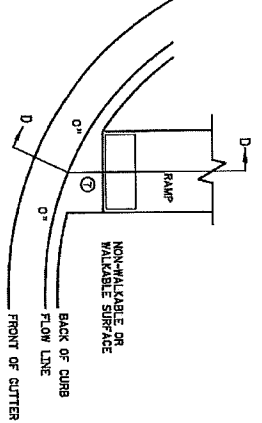
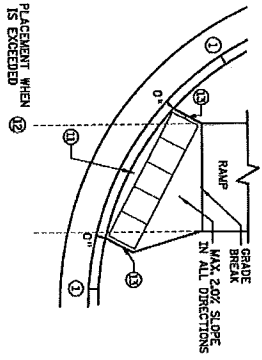
DIRECTIONAL RAMP WALKABLE FLARE



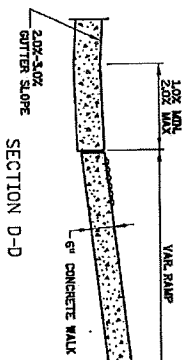
STANDARD ONE-WAY DIRECTIONAL ②



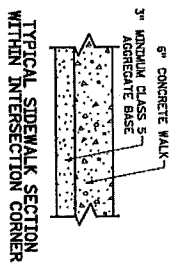
ONE-WAY DIRECTIONAL WITH DETECTABLE WARNING AT BACK OF CURB



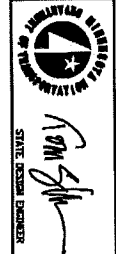
CURB FOR DIRECTIONAL RAMPS ④



SECTION D-D



TYPICAL SIDEWALK SECTION WITHIN INTERSECTION CORNER



APPROVED  
 1-23-2017

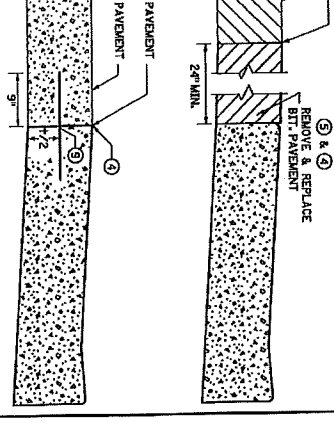
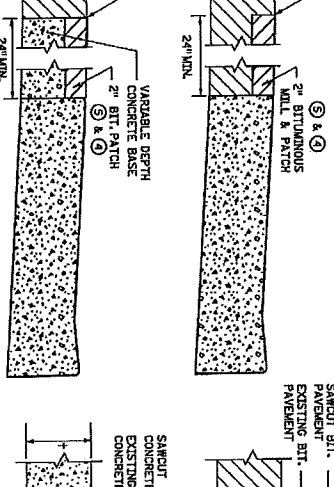
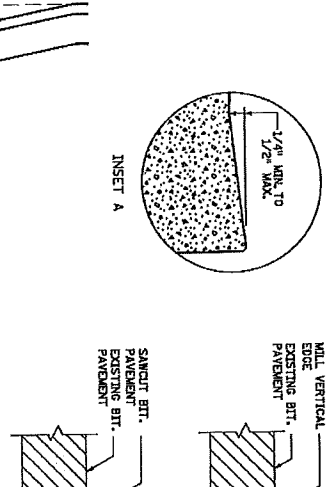
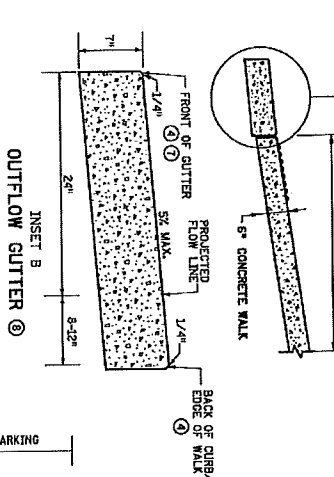
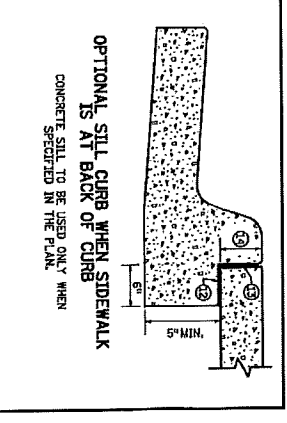
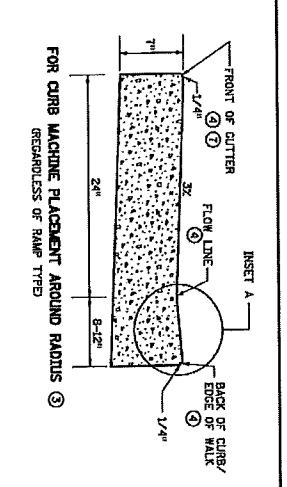
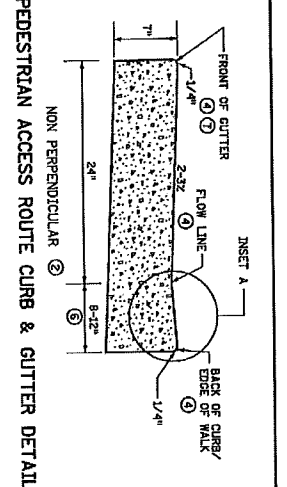
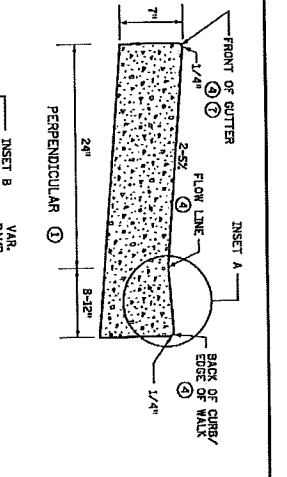
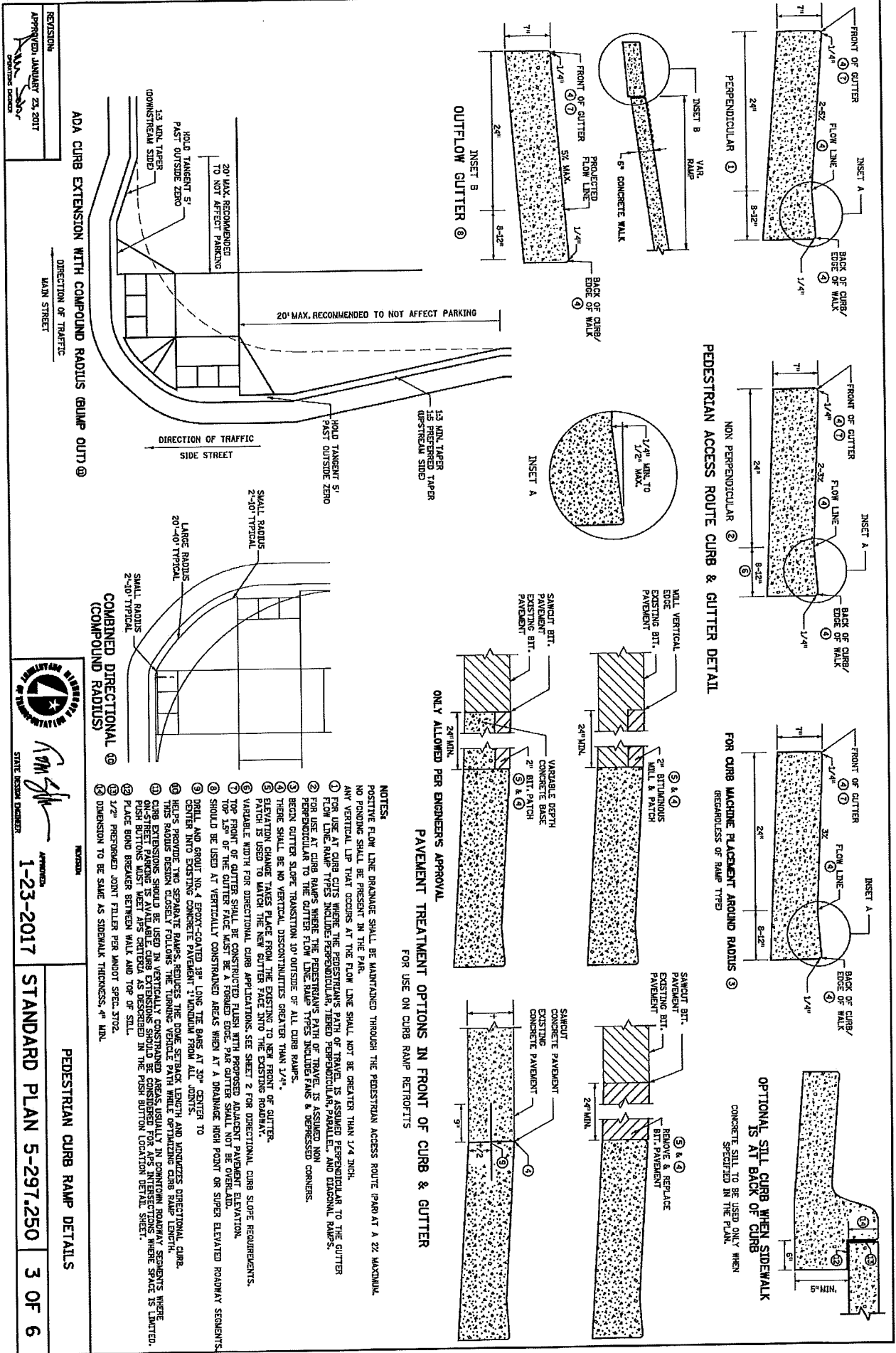
PEDESTRIAN CURB RAMP DETAILS  
 STANDARD PLAN 5-297.250  
 2 OF 6

NOTES:

1. LANDINGS SHALL BE LOCATED ANYWHERE THE PEDESTRIAN ACCESS ROUTE GRAD CHANGES AND SHALL BE APPROACHING WALK IS INVERSE GRADE.
2. INITIAL CURB RAMP LANDINGS SHALL BE CONSTRUCTED WITHIN 15' FROM THE BACK OF CURB, WITH 6" FROM THE BACK OF CURB BEING THE REFERRED DISTANCE, ONLY APPLICABLE WHEN THE INITIAL RAMP RUNNING SLOPE IS OVER 5.0%.
3. SECONDARY CURB RAMP LANDINGS ARE REQUIRED FOR EVERY 30' OF VERTICAL RISE WHEN THE LONGITUDINAL SLOPE IS GREATER THAN 5.0%.
4. CONTRACTION JOINTS SHALL BE CONSTRUCTED ALONG ALL GRADE BREAKS WITHIN THE PARALLEL JOINTS SHALL BE USED AT THE TOP GRADE BREAK OF CONCRETE FLARES ADJACENT TO WALKABLE SURFACES.
5. ALL GRADE BREAKS WITHIN THE PAR SHALL BE PERPENDICULAR TO THE PATH OF TRAVEL.
6. TO ENSURE INITIAL RAMPS AND INITIAL LANDINGS ARE PROPERLY CONSTRUCTED, LANDINGS SHALL BE CONSTRUCTED WITHIN THE PAR SHALL BE PERPENDICULAR TO THE PATH OF TRAVEL.
7. THE TOP OF CURB SHALL MATCH PROPOSED ADJACENT WALK GRADE.
8. WHEN THE BOLD/CURB IS 4" WIDE OR LESS, THE TOP OF CURB TAPER SHALL MATCH THE RAMP SLOPES TO REMOVE NEGATIVE BOLD/CURB SLOPES FROM THE TOP BACK OF CURB TO THE PAR.
9. ALL RAMP TYPES SHOULD HAVE A MINIMUM 7' LONG RAMP LENGTH.
10. MINIMUM WIDTH OF DETECTABLE WARNING IS REQUIRED FOR ALL RAMPS. DETECTABLE WARNINGS SHALL CONTINUOUSLY EXTEND FOR A MIN. OF 24" IN THE PATH OF TRAVEL. DETECTABLE WARNING TO COVER ENTIRE WIDTH OF SHARED-USE PATH, AND THE ENTIRE PAR WIDTH OF THE WALK. DETECTABLE WARNING SHOULD BE BUILT UP TO THE GRATE FROM THE TOP OF THE GRATE TO THE BIRTH OF THE WALK. DETECTABLE WARNING SHALL BE SETBACK BY MINIMUM TO 6" MAXIMUM FROM THE BACK OF CURB. DETECTABLE WARNING SHALL BE SETBACK BY MINIMUM TO 6" MAXIMUM FROM THE BACK OF CURB. DETECTABLE WARNING SHALL BE SETBACK BY MINIMUM TO 6" MAXIMUM FROM THE BACK OF CURB. DETECTABLE WARNING SHALL BE SETBACK BY MINIMUM TO 6" MAXIMUM FROM THE BACK OF CURB.
11. VERTICAL CURB HEIGHT.
12. 2" HIGH CURB WHEN USING A 2" LONG RAMP.
13. 3" MINIMUM CURB HEIGHT (5.5' MIN. DISTANCE REQUIRED BETWEEN DOWNS).
14. THE "RAMP" IN BETWEEN THE RAMPS SHOULD NOT BE IN THE PATH OF TRAVEL FOR COMBINED DIRECTIONAL RAMPS. IF THIS OCCURS MODIFY THE RAMP LOCATION OR SWITCH RAMP TO A TAPER/DEPRESSED CORNER.
15. WHEN USING CONCRETE PAVED FLARES ON THE OUTSIDE OF DIRECTIONAL RAMPS AND ADJACENT TO A WALKABLE SURFACE, DETECTABLE WARNING FLARES SHOULD BE USED. SEE THE DETAIL ON THIS SHEET.
16. RADIAL SHALL ALWAYS BE USED WHEN FEASIBLE. A CURB IS USED SHALL BE SETBACK OUTSIDE THE SETBACK SHALL BE USED OVER A CURB TO REMOVE TRIPPING HAZARDS AND FACILITATE SNOW & ICE REMOVAL.
17. MAX. 2.0% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK AND DRAIN TO FLOW LINE. SHALL BE CONSTRUCTED INTERVAL WITH CURB AND CUTTER.
18. 82 TO 10% WALKABLE FLARE.
19. PLACE DOWNS AT THE BACK OF CURB WHEN ALLOWABLE SETBACK CRITERIA IS EXCEEDED.
20. FRONT EDGE OF DETECTABLE WARNING SHALL BE SET BACK 2" MAXIMUM WHEN ADJACENT TO WALKABLE SURFACE AND 5" MAXIMUM WHEN ADJACENT TO NON-WALKABLE SURFACE WITH ONE CORNER SET 3" FROM BACK OF CURB. A WALKABLE SURFACE IS DEFINED AS A PAVED SURFACE ADJACENT TO A CURB RAMP ADJACENT TO A WALKABLE SURFACE THAT COULD BE TRAVELLED BY A USER WHO IS VISUALLY IMPAIRED.
21. RECTANGULAR DETECTABLE WARNINGS MAY BE SETBACK UP TO 9" FROM THE BACK OF CURB WITH CORNERS SET 3" FROM BACK OF CURB. IF 9" SETBACK IS EXCEEDED USE RADIAL DETECTABLE WARNINGS.
22. FOR DIRECTIONAL RAMPS WITH THE DETECTABLE WARNING PLACED AT THE BACK OF CURB, THE DETECTABLE WARNING SHALL COVER THE ENTIRE WIDTH OF THE WALK/PATH. THIS ENSURES THE DETECTABLE EDGE AND HELPS ELIMINATE THE CURB TAPER OBSTRUCTING THE PATH OF PEDESTRIAN TRAVEL.
23. THE CONCRETE WALK SHALL BE FORMED AND CONSTRUCTED PERPENDICULAR TO THE BACK OF CURB. MAINTAIN 3" BETWEEN EDGE OF DOWNS AND EDGE OF CONCRETE.
24. TO BE USED FOR ALL DIRECTIONAL RAMPS, EXCEPT WHERE DOWNS ARE PLACED ALONG THE BACK OF CURB.

LEGEND

|  |  |
|--|--|
|  | THESE LONGITUDINAL SLOPE FLARES SHALL BE THE STARTING POINT IF THE CURB IS TO BE CONSTRUCTED WITHIN THE PAR.                                       |
|  | INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE BETWEEN 5.0% MINIMUM AND 8.5% MAXIMUM IN THE DIRECTION SHOWN AND THE CROSS SLOPE SHALL NOT EXCEED 2.0%. |
|  | INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE GREATER THAN 2.0% AND LESS THAN 5.0% IN THE DIRECTION SHOWN.  |
|  | INDICATES CURB HEIGHT. DIMENSIONS AND MAX 2.0% SLOPE IN ALL DIRECTIONS. LANDING SHALL BE FULL WIDTH OF ADJACENT PARS.                              |



**PAVEMENT TREATMENT OPTIONS IN FRONT OF CURB & GUTTER**  
FOR USE ON CURB RAMP RETROFITS

- NOTES:**
- POSITIVE FLOW LINE DRAINAGE SHALL BE MAINTAINED THROUGH THE PEDESTRIAN ACCESS ROUTE (PAR) AT A 2% MAXIMUM. NO PONDING SHALL BE PRESENT IN THE PAR.
  - ANY VERTICAL LIP THAT OCCURS AT THE FLOW LINE SHALL NOT BE GREATER THAN 1/4" INCH.
  - FOR USE AT CURB CUTS WHERE THE PEDESTRIAN PATH OF TRAVEL IS ASSUMED PERPENDICULAR TO THE GUTTER FLOW LINE, RAMP TYPES INCLUDE PERPENDICULAR, TIERED PERPENDICULAR, PARALLEL, AND DIAGONAL RAMP.
  - FOR USE AT CURB RAMP WHERE THE PEDESTRIAN PATH OF TRAVEL IS ASSUMED NON PERPENDICULAR TO THE GUTTER FLOW LINE, RAMP TYPES INCLUDE FANS & DERESSED CORNERS.
  - BEFORE GUTTER SLOPE TRANSITION 10' OUTSIDE OF ALL CURB RAMP.
  - THERE SHALL BE NO VERTICAL DISCONTINUITIES GREATER THAN 1/4".
  - ELEVATION CHANGE TAKES PLACE FROM THE EXISTING TO NEW FRONT OF GUTTER.
  - PARCH IS USED TO MATCH THE NEW GUTTER FACE INTO THE EXISTING ROADWAY.
  - VARIABLE WIDTH FOR DIRECTIONAL CURB APPLICATIONS, SEE SHEET 2 FOR DIRECTIONAL CURB SLOPE REQUIREMENTS.
  - TOP FRONT OF THE GUTTER SHALL BE CONSTRUCTED FLUSH WITH PROPOSED ADJACENT PAVEMENT ELEVATION.
  - TOP 1/2" OF THE GUTTER FACE MUST BE A FORMED EDGE, PAR GUTTER SHALL NOT BE OVERLAP.
  - SHOULD BE USED AT VERTICALLY CONSTRAINED AREAS WHEN AT A BRANCHED HIGH POINT OR SUPER ELEVATED ROADWAY SEGMENTS.
  - RAIL AND GROUT NO. 4 FERRY-COATED 3/8" LONG THE BARS AT 30" CENTER TO CENTER INTO EXISTING CONCRETE PAVEMENT 1" MINIMUM FROM ALL JOINTS.
  - IF THE PROPOSED TWO SEPARATE RAMPERS REDUCES THE EDGE STRAP LENGTH AND DIMINISHES DIRECTIONAL CURB, THIS RADIUS DESIGN CLOSELY FOLLOWS THE TURNING VEHICLE PATH WHILE OPTIMIZING CURB RAMP LENGTH.
  - CURB EXTENSIONS SHOULD BE USED IN VERTICALLY CONSTRAINED AREAS, USUALLY IN DOWNSTREAM ROADWAY SEGMENTS WHERE ONE-STREET PARKING IS AVAILABLE. CURB EXTENSIONS SHOULD BE CONSIDERED FOR PAR INTERSECTIONS WHERE SPACE IS LIMITED. PUNCH BOLTS MUST MEET AFS CRITERIA AS DESCRIBED IN THE PUNCH BOLT LOCATION DETAIL SHEET.
  - PLACE BOND BREAKER BETWEEN WALK AND TOP OF SILL.
  - 1/2" PREFORMED JOINT FILLER PER MINUT SPEC. 5702.
  - DIMENSION TO BE SAME AS SIDEWALK THICKNESS, 4" MIN.

APPROVED: **1-23-2017** **STANDARD PLAN 5-297.250** **3 OF 6**

PEDESTRIAN CURB RAMP DETAILS



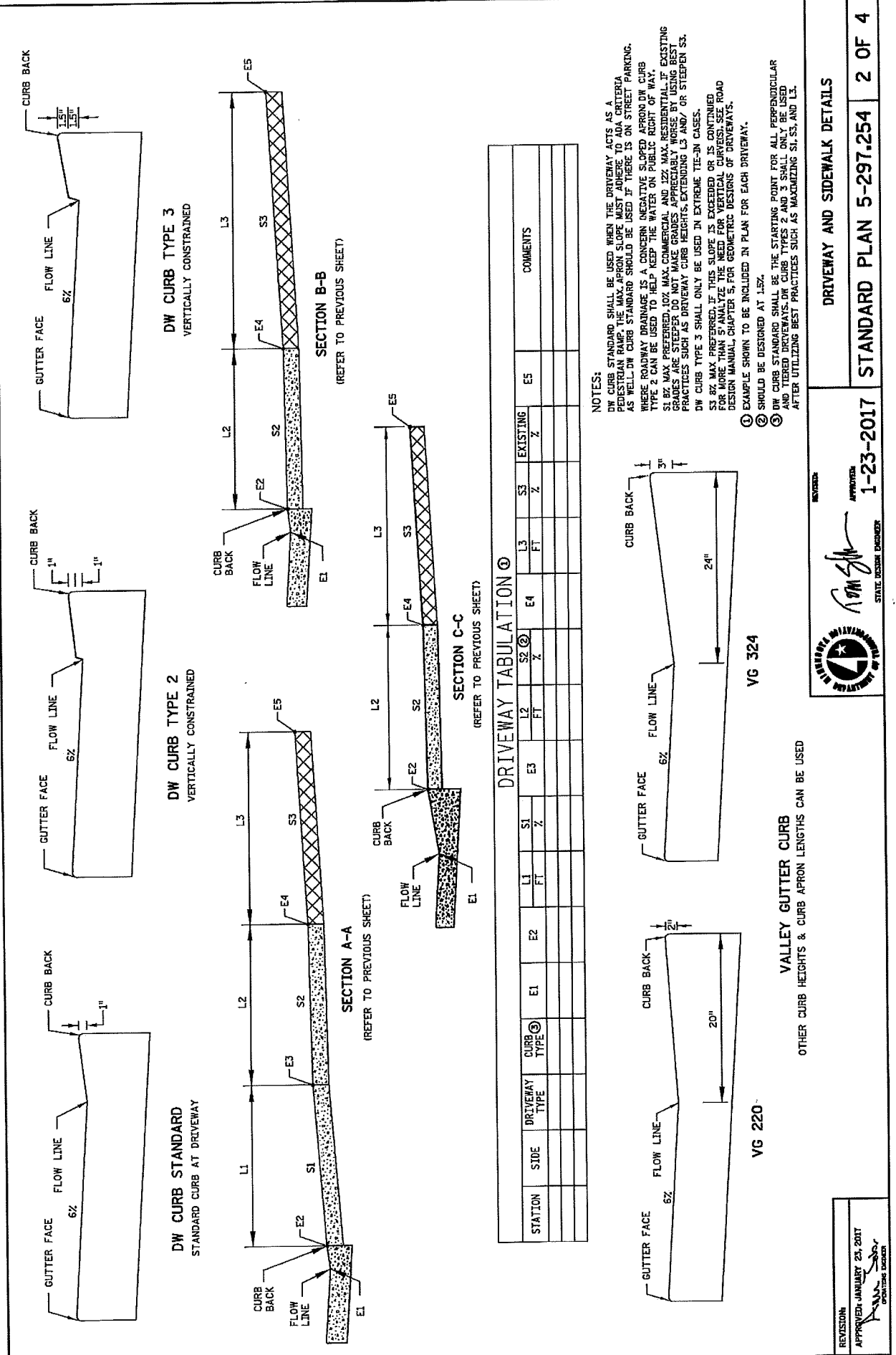












**DRIVEWAY TABULATION ①**

| STATION | SIDE | DRIVEWAY TYPE | CURB TYPE ② | EXISTING |    |    |    |    | COMMENTS |    |   |
|---------|------|---------------|-------------|----------|----|----|----|----|----------|----|---|
|         |      |               |             | L1       | E1 | E2 | E3 | E4 |          | E5 |   |
|         |      |               |             | FT       | Z  | FT | Z  | FT | Z        | FT | Z |
|         |      |               |             |          |    |    |    |    |          |    |   |
|         |      |               |             |          |    |    |    |    |          |    |   |
|         |      |               |             |          |    |    |    |    |          |    |   |
|         |      |               |             |          |    |    |    |    |          |    |   |

**NOTES:**

DW CURB STANDARD SHALL BE USED WHEN THE DRIVEWAY ACTS AS A PEDESTRIAN RAMP. THE MAX. APRON SLOPE MUST ADHERE TO ADA STREETS AS WELL AS DW CURB STANDARD SHOULD BE USED IF THERE IS ON STREET PARKING. WHERE ROADWAY DRAINAGE IS A CONCERN NEGATIVE SLOPED APPROX DW CURB TYPE 2 CAN BE USED TO HELP KEEP THE WATER ON PUBLIC RIGHT OF WAY. S1 E2 MAX. PREFERRED, 10% MAX. COMMERCIAL APPROACH. S2 MAX. PREFERRED, 10% MAX. COMMERCIAL APPROACH. S3 MAX. PREFERRED, 10% MAX. COMMERCIAL APPROACH. DRIVES BY USING BEST PRACTICES SUCH AS DRIVEWAY CURB HEIGHTS, EXTENDING L3 AND/OR STEEPEN S3. DW CURB TYPE 3 SHALL ONLY BE USED IN EXTREME TIE-IN CASES. S3 8% MAX. PREFERRED, IF THIS SLOPE IS EXCEEDED OR IS CONTINUED DR MORE THAN 10' SEE ROAD DESIGN MANUAL, CHAPTER 5, FOR GEOMETRIC DESIGN OF DRIVEWAYS. EXAMPLE SHOWN TO BE INCLUDED IN PLAN FOR EACH DRIVEWAY.

① SHOULD BE DESIGNED AT 1.5%.

② DW CURB STANDARD SHALL BE THE STARTING POINT FOR ALL PERPENDICULAR AND TIERED DRIVEWAYS. DW CURB TYPES 2 AND 3 SHALL ONLY BE USED AFTER UTILIZING BEST PRACTICES SUCH AS MAXIMIZING S1, S3, AND L3.

**DRIVEWAY AND SIDEWALK DETAILS**

**STANDARD PLAN 5-297.254 2 OF 4**

REVISION  
APPROVED JANUARY 23, 2017  
DATE

1-23-2017

STATE DESIGN ENGINEER











**SMEEING MULTIFAMILY**

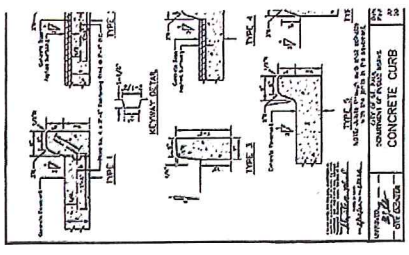
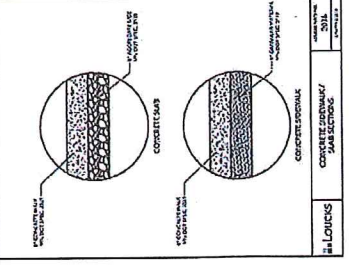
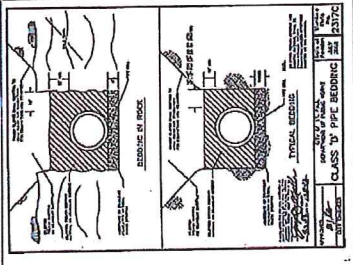
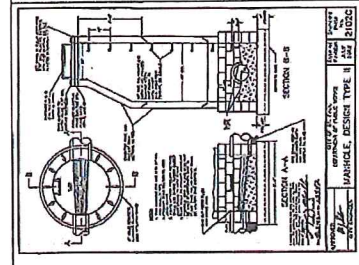
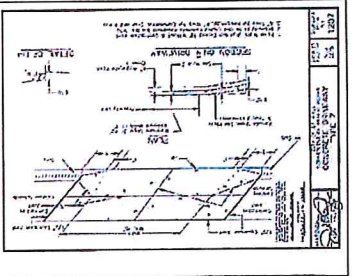
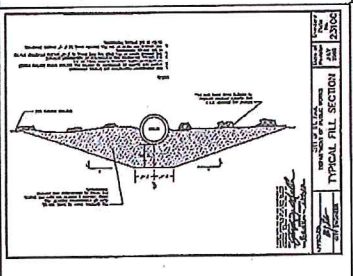
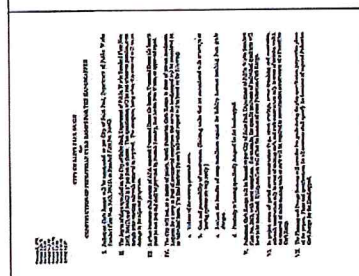
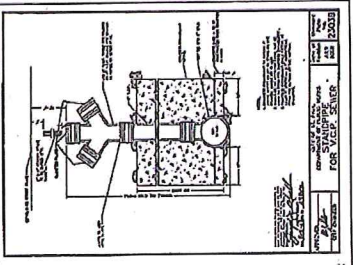
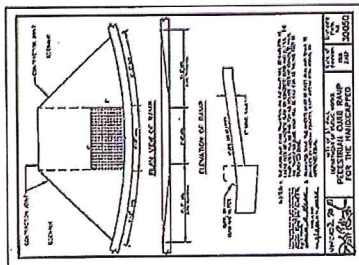
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**LOCKS**  
 CHALLENGING  
 LANDSCAPING  
 LANDSCAPE ARCHITECTURAL

**LOCK CONSTRUCTION**  
 4784

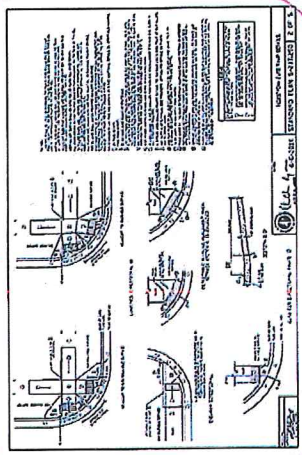
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**DETAILS C8-1**



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*Handwritten note:* Use updated curbs  
 Standard plans









320 South Griggs Street  
St. Paul, MN 55105  
www.macgrove.org

651-695-4000  
mgcc@macgrove.org

April 5, 2017

Josh Williams  
City of Saint Paul, Dept. of Planning and Economic Development  
25 W. Fourth Street  
Saint Paul, MN 55102

Dear Josh;

On March 22, 2017, the Housing and Land Use Committee (“HLU”) of the Macalester-Groveland Community Council (“MGCC”) held a public meeting, at which it considered the site plan for a proposed mixed-use structure at the properties located at 246-286 Snelling Ave. The applicant, who has appeared before the HLU to discuss the project on three prior occasions, appeared to speak to the site plan and to answer questions.

Prior to the meeting, the HLU received twenty-four (24) comments expressing a variety of opinions regarding the current site plan design and additional feedback was shared at the meeting. After speaking with the applicant, considering neighborhood feedback, consulting the Macalester Groveland Long Range plan, and assessing the merits of the application, the HLU makes the following observations and recommendations concerning the site plan dated 3/22/2017:

- A stated goal of the Long Range plan is to “Support land use that preserves Macalester-Groveland as a uniquely connected, walkable, mixed-use sustainable neighborhood with a *pedestrian-oriented human-scale streetscape*.”
- Another stated goal of the Long Range plan is to “explore creating and/or implementing design standards that promote pedestrian-friendly streetscapes—*especially along mixed-use corridors*.”
- The HLU provides the following feedback in furtherance of those goals:
  - Beautify Alley Wall: The wall as shown on plans dated 3/22/207 is long, uninterrupted and shown as one material. Design features or textural elements should be added to this surface.
  - Improve Streetscape: Pedestrian-friendly alternatives to a uniform façade at the street level on the Snelling side should continue to be developed. Possible options include:
    - Variation in building frontage, such as recessed “nooks” or seating areas at sidewalk level. There is excellent visual “place-making” potential for this type of design.

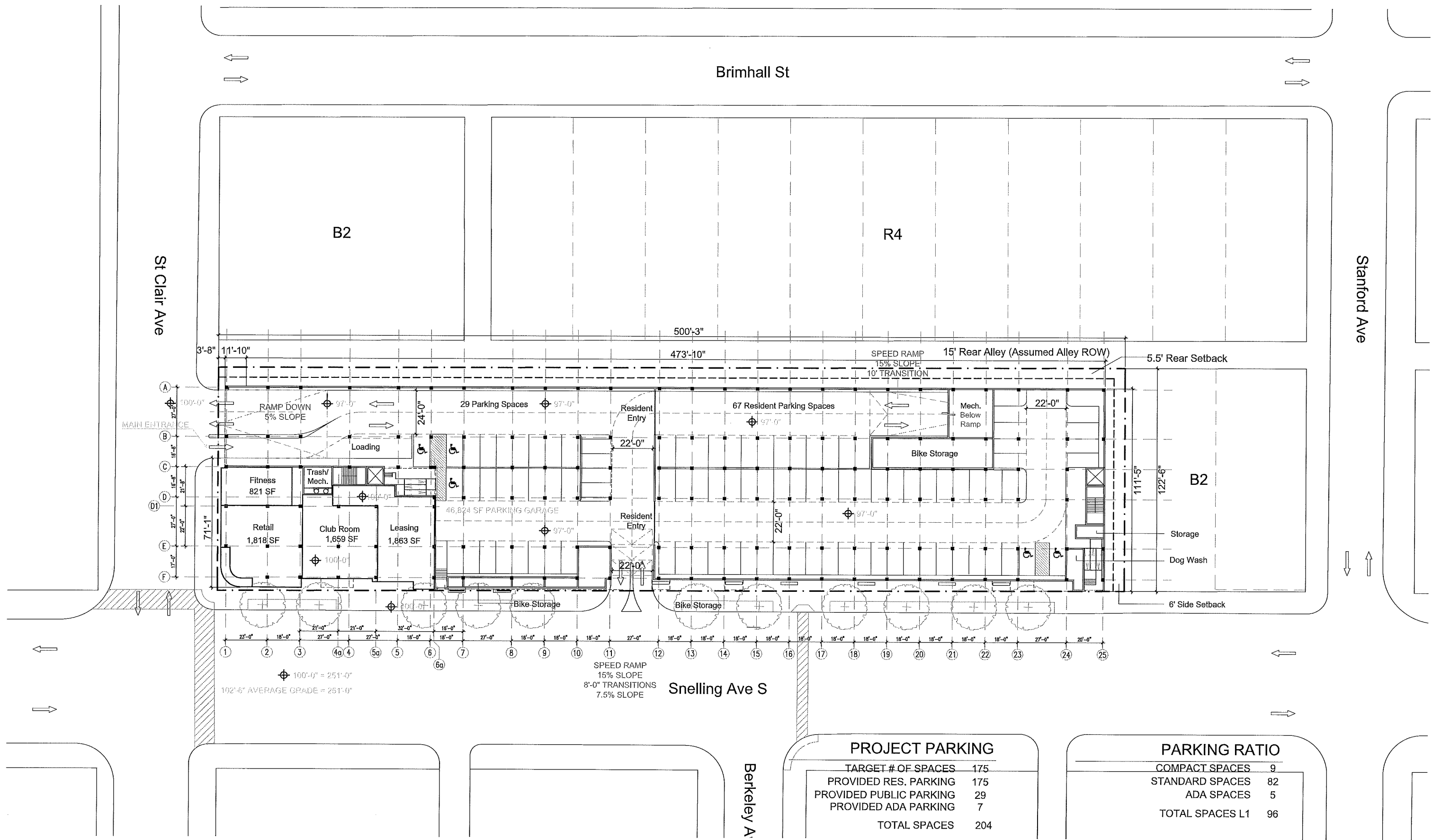
- Visually breaking up the building into different sections with distinct, yet related feel, creating a different rhythm in different sections as if they were a collection of retailers or a collection of buildings. Some sections could have awnings and windows could fluctuate on size or design. Brick could differ slightly in tone or texture to create interest.
  - Incorporate landscaping into the building façade through screening that includes planter boxes, ivy trellis etc.
  - Maximize window openings into “active” spaces and eliminate spandrel windows.
- Setback of upper floors: To the extent possible, upper residential floors should be set back to reduce the mass of the building.
- Minimize curb cuts on St. Clair: The width of the final curb cut providing access from the parking level to St. Clair Ave should be minimized, especially considering the proximity to the alley.
- Pedestrian Safety: Include pedestrian safety improvements for all driveways crossing sidewalks including signage, pavement markings and multiple mirrors.
- Provide for conversion of parking into retail: Future market conditions may allow for a reduction in parking spaces provided at this site. Design and construction should be completed in such a way as to allow for conversion of current parking area into future retail area.
- Bike parking: Facilities for bike parking should be included in three areas:
  - Outside the publicly accessible retail space
  - In the public area of the enclosed parking garage
  - On the first floor of the portion of the parking garage dedicated to residential tenants
- Roofline variation: To the extent possible, incorporate variation in the roofline and windows to create visual and architectural interest.

If you have questions or concerns, please do not hesitate to contact me.



Liz Boyer  
Executive Director  
Macalester-Groveland Community Council

cc (via email): Ward 3, City of Saint Paul  
Tia Anderson, City of Saint Paul Department of License and Inspections  
Tom Hayden, LeCesse Development Inc.

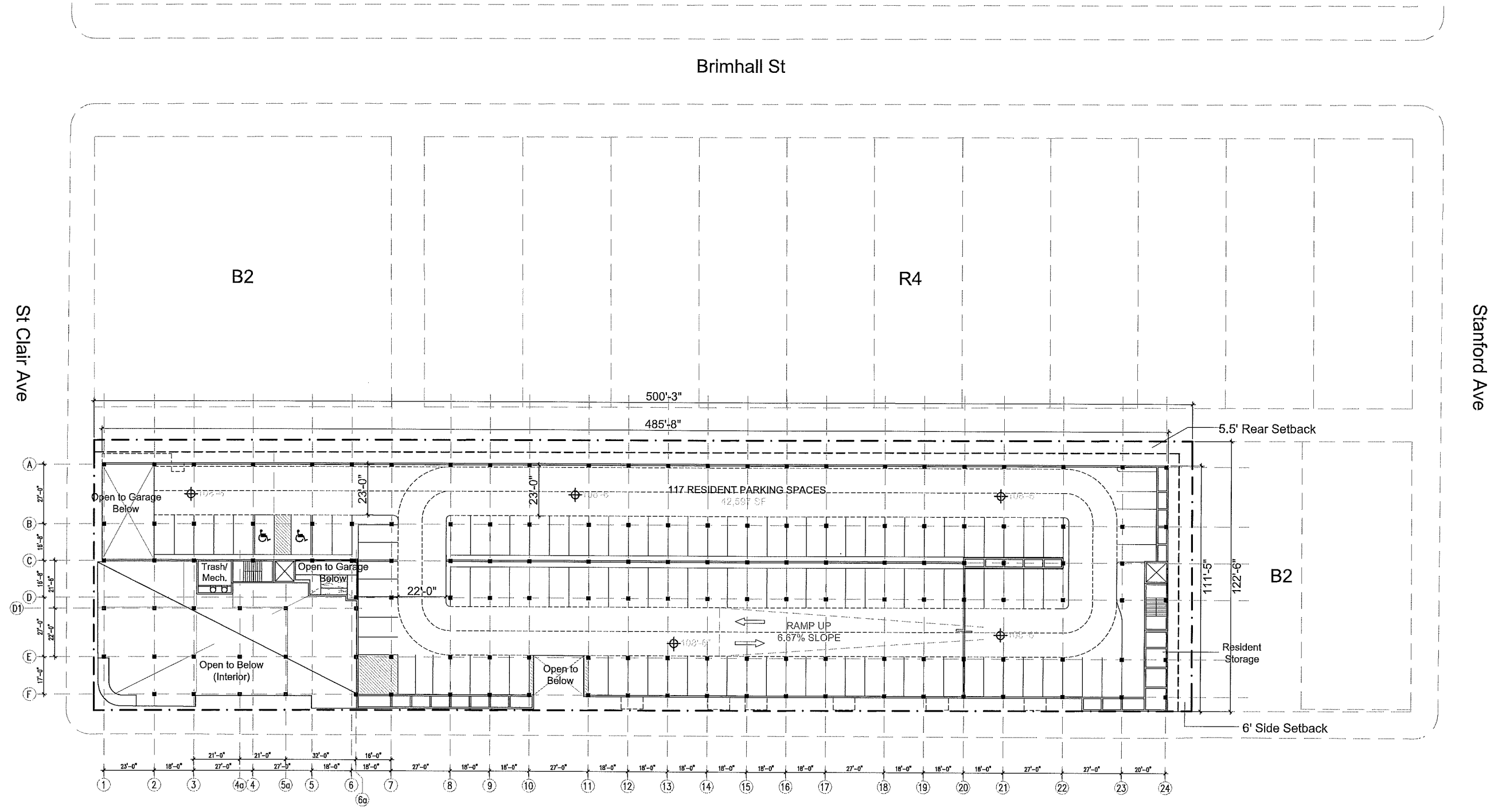


| PROJECT PARKING         |            |
|-------------------------|------------|
| TARGET # OF SPACES      | 175        |
| PROVIDED RES. PARKING   | 175        |
| PROVIDED PUBLIC PARKING | 29         |
| PROVIDED ADA PARKING    | 7          |
| <b>TOTAL SPACES</b>     | <b>204</b> |

| PARKING RATIO          |           |
|------------------------|-----------|
| COMPACT SPACES         | 9         |
| STANDARD SPACES        | 82        |
| ADA SPACES             | 5         |
| <b>TOTAL SPACES L1</b> | <b>96</b> |

Level 01





Snelling Ave S

Berkeley A

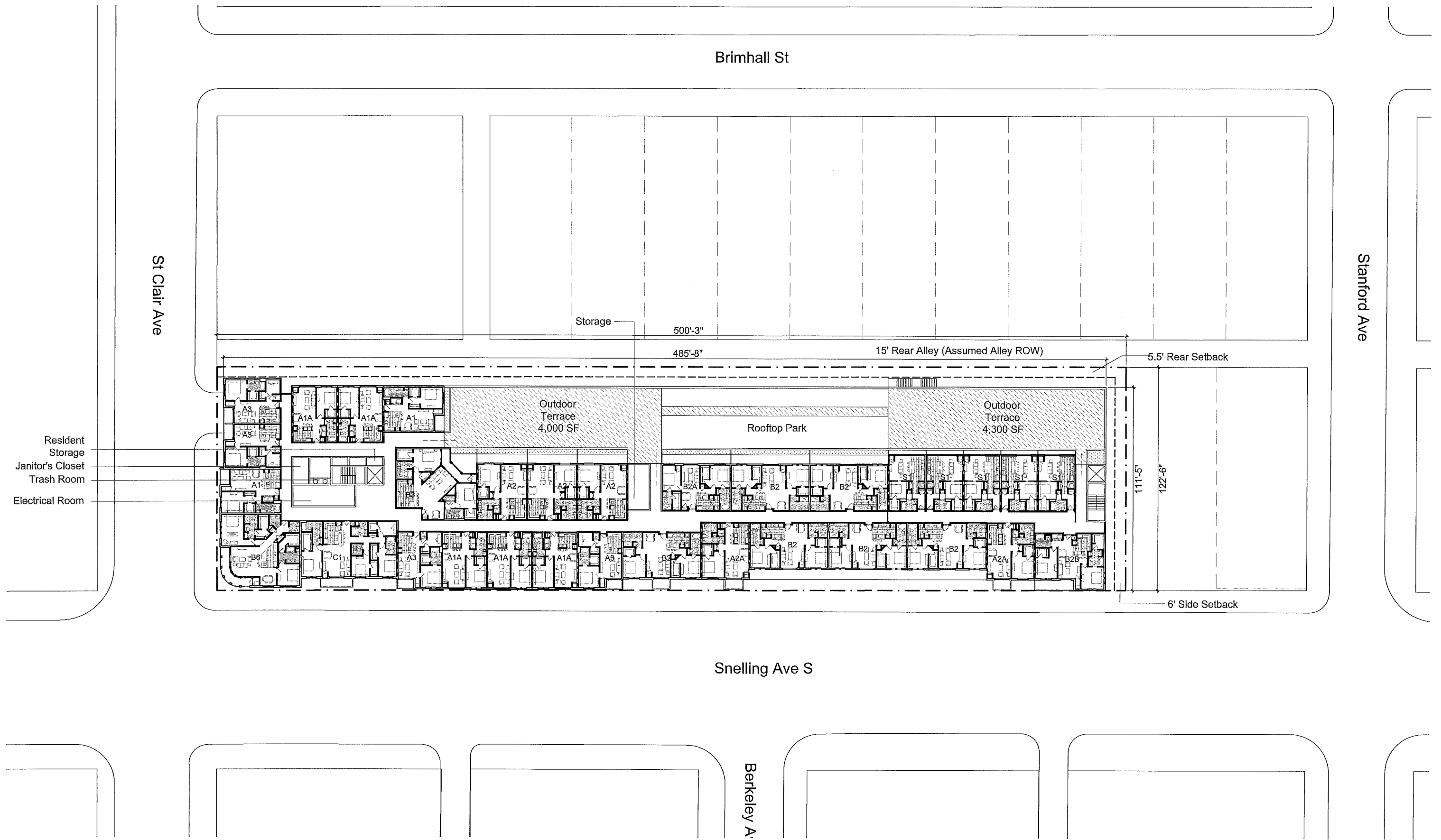
**PARKING RATIO**

|                          |            |
|--------------------------|------------|
| COMPACT SPACES           | 13         |
| STANDARD SPACES          | 102        |
| ADA SPACES               | 2          |
| <b>TOTAL SPACES L1.5</b> | <b>117</b> |

Level 01.5







Brimhall St

St Clair Ave

Stanford Ave

Storage

500'-3"

485'-8"

15' Rear Alley (Assumed Alley ROW)

5.5' Rear Setback

Outdoor Terrace  
4,000 SF

Rooftop Park

Outdoor Terrace  
4,300 SF

111'-5"

122'-6"

6' Side Setback

Resident Storage  
Janitor's Closet  
Trash Room  
Electrical Room

Snelling Ave S

Berkeley A

Level 02



Resident Storage  
 Janitor's Closet  
 Trash Room  
 Electrical Room



mural

- ↔ PARAPET  
69'-4"
- ↔ ROOF  
62'-4"
- ↔ LEVEL 5 RESIDENTIAL  
51'-6"
- ↔ LEVEL 4 RESIDENTIAL  
40'-8"
- ↔ LEVEL 3 RESIDENTIAL  
29'-10"
- ↔ LEVEL 2 RESIDENTIAL  
19'-0"
- ↔ LEVEL 1.5 GARAGE  
8'-0"
- ↔ LEVEL 1 LEASING COMMERCIAL  
0'-0"
- ↔ SEMI-BASEMENT  
-3'-0"







SIGNAGE

- PARAPET 69'-4"
- ROOF 62'-4"
- LEVEL 5 RESIDENTIAL 51'-6"
- LEVEL 4 RESIDENTIAL 40'-8"
- LEVEL 3 RESIDENTIAL 29'-10"
- LEVEL 2 RESIDENTIAL 19'-0"
- LEVEL 1.5 GARAGE 9'-6"
- LEVEL 1 LEASING/COMMERCIAL 0'-0"
- SEMI-BASEMENT -3'-0"

ENTRANCE  
SIGNAGE

SIGNAGE





display windows

- ✦ PARAPET 69'-4"
- ✦ ROOF 62'-4"
- ✦ LEVEL 5 RESIDENTIAL 51'-6"
- ✦ LEVEL 4 RESIDENTIAL 40'-8"
- ✦ LEVEL 3 RESIDENTIAL 29'-10"
- ✦ LEVEL 2 RESIDENTIAL 19'-0"
- ✦ LEVEL 1.5 GARAGE 8'-6"
- ✦ LEVEL 1 LEASING/COMMERCIAL 0'-0"
- ✦ SEMI-BASEMENT -3'-0"



vision glass at leasing/commercial

pedestrian garage entrance

display windows at garage

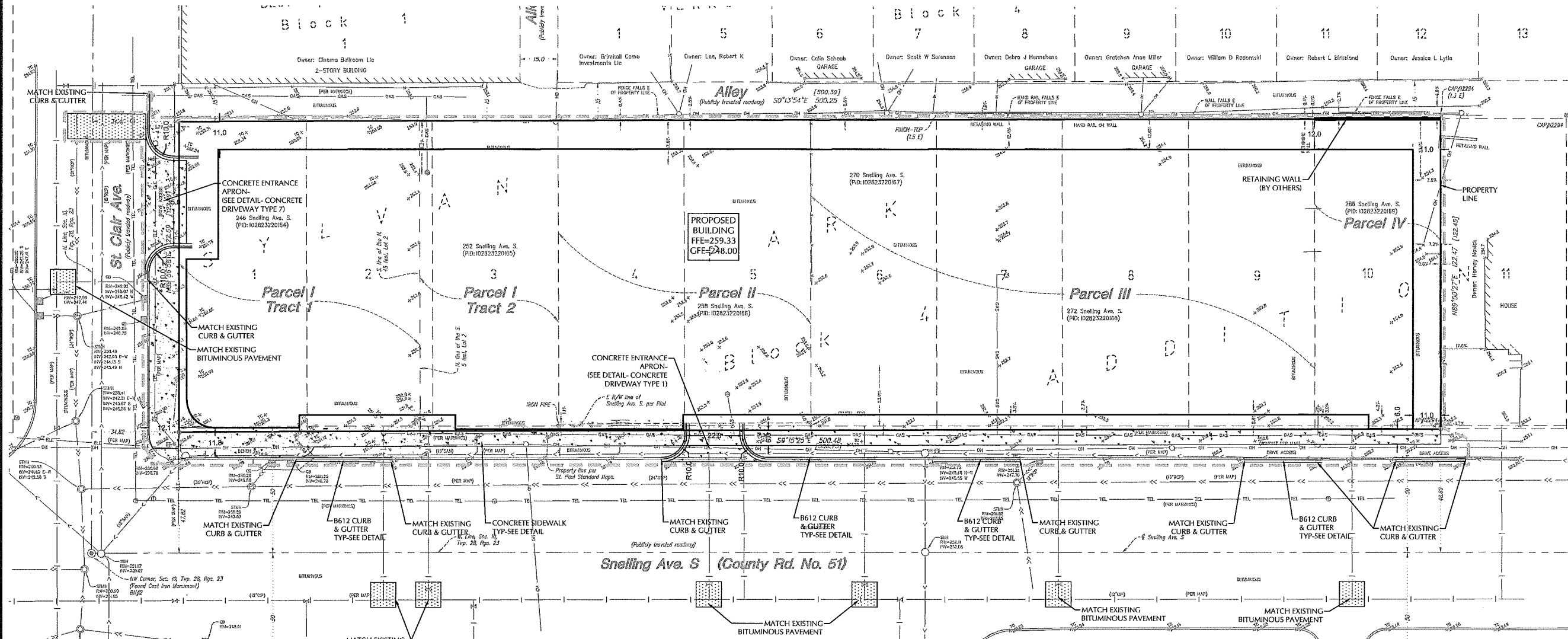
pedestrian garage entrance



- ✦ PARAPET  
69'-4"
- ✦ ROOF  
62'-4"
- ✦ LEVEL 5 RESIDENTIAL  
51'-6"
- ✦ LEVEL 4 RESIDENTIAL  
40'-8"
- ✦ LEVEL 3 RESIDENTIAL  
29'-10"
- ✦ LEVEL 2 RESIDENTIAL  
19'-0"
- ✦ LEVEL 1.5 GARAGE  
8'-6"
- ✦ LEVEL 1 LEASING/COMMERCIAL  
0'-0"
- ✦ SEMI-BASEMENT  
-3'-0"







**CITY OF ST. PAUL NOTES**

1. SIGNS REGULATING PARKING AND/OR TRAFFIC ON PRIVATE PROPERTY SHALL BE INSTALLED BY THE PROPERTY OWNER OR CONTRACTOR OUTSIDE OF THE PUBLIC RIGHT-OF-WAY. SIGNS APPROVED BY PUBLIC WORKS TRAFFIC ENGINEERING REGULATING PARKING AND/OR TRAFFIC IN THE PUBLIC RIGHT-OF-WAY FOR THIS DEVELOPMENT SHALL BE INSTALLED BY CITY FORCES AT THE EXPENSE OF THE DEVELOPMENT. CONTACT TRAFFIC ENGINEERING 651-266-6200 SIX WEEKS IN ADVANCE OF NEEDED SIGNS.
2. ALL WORK ON CURBS, DRIVEWAYS, AND SIDEWALKS WITHIN THE PUBLIC RIGHT OF WAY MUST BE DONE BY A LICENSED AND BONDED CONTRACTOR UNDER A PERMIT FROM PUBLIC WORKS SIDEWALK SECTION (651-266-6120). SIDEWALK GRADES MUST BE CARRIED ACROSS DRIVEWAYS.
3. RESTORATION OF ASPHALT AND CONCRETE PAVEMENTS ARE PERFORMED BY THE PUBLIC WORKS STREET MAINTENANCE DIVISION. THE CONTRACTOR IS RESPONSIBLE FOR PAYMENT TO THE CITY FOR THE COST OF THESE RESTORATIONS. THE CONTRACTOR SHALL CONTACT PUBLIC WORKS STREET MAINTENANCE TO SET UP A WORK ORDER PRIOR TO BEGINNING ANY REMOVALS IN THE STREET AT 651-292-6600. PROCEDURES AND UNIT COSTS ARE FOUND IN STREET MAINTENANCE'S "GENERAL REQUIREMENTS - ALL RESTORATIONS" AND ARE AVAILABLE AT THE PERMIT OFFICE.
4. THE CONTRACTOR SHALL CONTACT JOHN McNAMARA AT 651-266-9780, GENERAL FOREMAN. LIGHTING - SIGNAL MAINTENANCE, IF REMOVAL OR RELOCATION OF EXISTING FACILITIES IS REQUIRED OR IN THE EVENT OF DAMAGE TO THE LIGHTING FACILITIES. OTHER ISSUES THE CONTRACTOR SHALL ASSUME RESPONSIBILITY (AND RELATED COSTS) FOR ANY DAMAGE OR RELOCATIONS. LIGHTING ON KELLOGG BLVD & WABASHA ST SHALL BE MAINTAINED FOR THE DURATION OF CONSTRUCTION. COORDINATE WITH THE CITY OF ST. PAUL.
5. CARE MUST BE TAKEN DURING CONSTRUCTION AND EXCAVATION TO PROTECT ANY SURVEY MONUMENTS AND/OR PROPERTY IRONS. CALL SAM GIBSON OF PUBLIC WORKS SURVEYING (651-266-6075) IF YOU HAVE ANY QUESTIONS
6. PIPE WORK INSIDE PROPERTY TO BE PERFORMED BY A PLUMBER LICENSED BY THE STATE OF MINNESOTA AND CERTIFIED BY THE CITY OF SAINT PAUL. PIPE WORK WITHIN THE PUBLIC RIGHT OF WAY TO BE PERFORMED BY SPRWS.
7. ABANDONING EXISTING SEWER SERVICE OR MAKING NEW CONNECTIONS TO CITY SEWER MUST BE DONE TO CITY STANDARDS BY A LICENSED HOUSE DRAIN CONTRACTOR UNDER A PERMIT FROM PUBLIC WORKS SEWER SECTION (651-266-6234).
8. INSPECTION CONTACT: THE DEVELOPER SHALL CONTACT THE RIGHT OF WAY INSPECTOR, JIM HEHN, AT 651-485-0417 (ONE WEEK PRIOR TO BEGINNING WORK) TO DISCUSS SAFETY ISSUES SUCH AS TRAFFIC CONTROL, AND PEDESTRIAN-SAFE WALKWAYS. OTHER ISSUES THAT MAY AFFECT THE ROW SUCH AS EROSION CONTROL AND COORDINATION WITH OTHER WORK NEARBY THAT THE DEVELOPER MAY NOT BE AWARE OF SHOULD BE DISCUSSED AS WELL.
9. SAFE WORK SITE REQUIREMENTS: THE CONTRACTOR SHALL PROVIDE A CONTINUOUS, ACCESSIBLE AND SAFE PEDESTRIAN WALKWAY THAT MEETS ADA AND MN MUTCD STANDARDS IF WORKING IN A SIDEWALK AREA, AND TRAFFIC CONTROL PER MN MUTCD REQUIREMENTS FOR WORK IN THE PUBLIC RIGHT OF WAY.
10. NO PRIVATE FACILITIES IN THE RIGHT OF WAY: THE DEVELOPER IS STRICTLY PROHIBITED FROM INSTALLING PRIVATE ELECTRICAL WIRING, CONDUIT, RECEPTACLES AND/OR LIGHTING IN THE CITY'S RIGHT OF WAY. THIS INCLUDES STRINGING CONDUIT OR CABLE INTO THE PUBLIC RIGHT OF WAY TO ACCOMMODATE UTILITY FEEDS TO THE SITE. COORDINATE WITH EACH UTILITY PRIOR TO CONSTRUCTION TO DETERMINE FEED POINTS INTO THE PROPERTY. UTILITIES ARE RESPONSIBLE FOR SECURING EXCAVATION PERMITS TO RUN THEIR SERVICE INTO A SITE, AND (WHERE REQUIRED) SUBMITTING PLANS FOR REVIEW BY THE PUBLIC WORKS UTILITY REVIEW COMMITTEE.
11. ANY SIGN OF FIXTURES REMOVED WITH IN THE RIGHT OF WAY OR AS PART OF THE SITE WORK SHALL BE REPLACED BY THE CITY. THE CONTRACTOR SHALL CONTACT MIKE MILLER IN TRAFFIC OPERATIONS AT 651-266-9726.
12. AN OBSTRUCTION PERMIT MUST BE OBTAINED FROM PUBLIC WORKS (651-266-6151) IF THERE WILL BE ANY EXCAVATION IN KELLOGG BLVD OR WABASHA ST. OR IF TRUCKS/EQUIPMENT WILL BE DRIVING OVER CURBS OR IF CONSTRUCTION WILL BLOCK CITY STREETS, OR SIDEWALKS.
13. ALL UNUSED EXISTING WATER SERVICES TO BE CUT OFF BY SAINT PAUL REGIONAL WATER SERVICE, WORK SCHEDULED BY OWNER'S CONTRACTOR.

**BENCHMARK**

BENCHMARK #1: TOP OF TOP NUT OF HYDRANT IN THE SE QUAD OF SARATOGA ST. S. AND ST. CLAIRE AVE. ELEVATION = 258.69 FEET (ST. PAUL DATUM)  
 BENCHMARK #2: CAST IRON MONUMENT AT THE NW CORNER OF SECTION 10, TOWNSHIP 28, RANGE 23. ELEVATION = 250.66 FEET (ST. PAUL DATUM)  
 ELEVATIONS SHOWN ARE BASED ON CITY OF ST. PAUL DATUM.  
 CONVERSION FACTOR TO 1929 NGVD, ADD 694.10.

**CITY OF ST. PAUL PERMIT REQUIREMENTS**

1. ORDERING OBSTRUCTION AND EXCAVATION PERMITS: CONTACT PUBLIC WORKS RIGHT OF WAY SERVICE DESK AT (651) 266-6151. IT IS STRONGLY RECOMMENDED THAT CONTRACTORS CALL FOR COST ESTIMATES PRIOR TO BIDDING TO OBTAIN ACCURATE COST ESTIMATES.
2. OBSTRUCTION PERMITS: THE CONTRACTOR MUST OBTAIN AN OBSTRUCTION PERMIT IF CONSTRUCTION (INCLUDING SILT FENCES) WILL BLOCK CITY STREETS, SIDEWALKS OR ALLEYS, OR IF DRIVING OVER CURBS.
3. EXCAVATION PERMITS: ALL DIGGING IN THE PUBLIC RIGHT OF WAY REQUIRES AN EXCAVATION PERMIT. IF THE PROPOSED BUILDING IS CLOSE TO THE RIGHT OF WAY, AND EXCAVATING INTO THE RIGHT OF WAY IS NEEDED TO FACILITATE CONSTRUCTION, CONTACT THE UTILITY INSPECTOR.
4. FAILURE TO SECURE PERMITS: FAILURE TO SECURE OBSTRUCTION PERMITS OR EXCAVATION PERMITS WILL RESULT IN A DOUBLE-PERMIT FEE AND OTHER FEES REQUIRED UNDER CITY OF ST. PAUL LEGISLATIVE CODES.
5. REQUIREMENTS TO WORK IN THE PUBLIC RIGHT OF WAY: ALL UTILITIES AND CONTRACTORS WORKING IN THE PUBLIC RIGHT OF WAY MUST TO BE REGISTERED, INSURED AND BONDED, AS RECOGNIZED BY THE PUBLIC WORKS SERVICE DESK. (651-266-6151)

**SITE DATA**

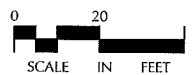
PROPERTY AREA: 1.408 AC  
 DISTURBED AREA: 1.408 AC  
 EXISTING IMPERVIOUS AREA: 1.34 AC (95%)  
 PROPOSED IMPERVIOUS AREA: 1.34 (95%)

**SITE NOTES**

1. ALL PAVING, CONCRETE CURB, GUTTER AND SIDEWALK SHALL BE FURNISHED AND INSTALLED IN ACCORDANCE WITH THE DETAILS SHOWN PER THE DETAIL SHEETS(S) AND STATE/LOCAL JURISDICTION REQUIREMENTS.
2. ACCESSIBLE PARKING AND ACCESSIBLE ROUTES SHALL BE PROVIDED PER CURRENT ADA STANDARDS AND LOCAL/STATE REQUIREMENTS.
3. ALL CURB DIMENSIONS SHOWN ARE TO THE FACE OF CURB UNLESS OTHERWISE NOTED.
4. ALL BUILDING DIMENSIONS ARE TO THE OUTSIDE FACE OF WALL UNLESS OTHERWISE NOTED.
5. TYPICAL FULL SIZED PARKING STALL IS 9' X 18' UNLESS OTHERWISE NOTED.
6. ALL CURB RADII SHALL BE 3.0' UNLESS OTHERWISE NOTED.
7. BITUMINOUS IMPREGNATED FIBER BOARD TO BE PLACED AT FULL DEPTH OF CONCRETE ADJACENT TO EXISTING STRUCTURES AND BEHIND CURB ADJACENT TO DRIVEWAYS AND SIDEWALKS.
8. SEE SITE ELECTRICAL PLAN FOR SITE LIGHTING.

**PAVEMENT TYPES**

- CONCRETE SIDEWALK
- CONCRETE SIDEWALK WITH SNOW MELT COORDINATE WITH MECHANICAL PLANS
- CONCRETE PAVEMENT
- LIGHT DUTY BITUMINOUS PAVEMENT
- HEAVY DUTY BITUMINOUS PAVEMENT



| EXISTING | CIVIL LEGEND            | PROPOSED |
|----------|-------------------------|----------|
| ○        | SANITARY MANHOLE        | ●        |
| ○        | STORM MANHOLE           | ●        |
| ○        | CATCH BASIN             | ●        |
| ○        | CLEVERT                 | ●        |
| ○        | HYDRANT                 | ●        |
| ○        | GATEVALVE               | ●        |
| ○        | POST INDICATOR VALVE    | ●        |
| ○        | LIGHT POLE              | ●        |
| ○        | POWER POLE              | ●        |
| ○        | SIGN                    | ●        |
| ○        | BENCHMARK               | ●        |
| ○        | SOIL BORINGS            | ●        |
| ○        | WATER MANHOLE           | ●        |
| ○        | TELEPHONE MANHOLE       | ●        |
| ○        | UTILITY MANHOLE         | ●        |
| ○        | ELECTRIC MANHOLE        | ●        |
| ○        | WATER SERVICE           | ●        |
| ○        | SANITARY SERVICE        | ●        |
| ○        | HANDICAP PARKING        | ●        |
| ○        | DIRECTION OF FLOW       | ●        |
| ○        | SPOT ELEVATION          | ●        |
| ○        | CONTOURS                | ●        |
| ○        | SANITARY SEWER          | ●        |
| ○        | STORM SEWER             | ●        |
| ○        | WATERSHED               | ●        |
| ○        | FENCE                   | ●        |
| ○        | FRONTSET                | ●        |
| ○        | SILT FENCE              | ●        |
| ○        | CURB & GUTTER           | ●        |
| ○        | RETAINING WALL          | ●        |
| ○        | TREELINE                | ●        |
| ○        | EASMENT LINE            | ●        |
| ○        | SETBACK LINE            | ●        |
| ○        | FENCE LINE              | ●        |
| ○        | UNDERGROUND TELE        | ●        |
| ○        | UNDERGROUND GAS         | ●        |
| ○        | OVERHEAD UTILITY        | ●        |
| ○        | UNDERGROUND FIBER OPTIC | ●        |
| ○        | UNDERGROUND ELECTRIC    | ●        |
| ○        | UNDERGROUND CABLE TV    | ●        |
| ○        | PROPERTY LINE           | ●        |
| ○        | CONIFEROUS TREE         | ●        |
| ○        | DECIDUOUS TREE          | ●        |
| ○        | PARKING COUNTS          | ●        |

**SNELLING MULTI-FAMILY**  
 ST. PAUL, MN

LEISURE DEVELOPMENT

**LOUCKS**  
 PLANNING  
 CIVIL ENGINEERING  
 LAND SURVEYING  
 LANDSCAPE ARCHITECTURE  
 ENVIRONMENTAL  
 7200 Hemlock Lane, Suite 300  
 Maple Grove, MN 55369  
 763.424.5505  
 www.louckscinc.com

**CADD QUALIFICATION**  
 CADD files prepared by the Consultant for this project are the property of the Consultant. These CADD files shall not be used on other projects, for additions to this project, or for completion of this project by anyone other than the Consultant. With the Consultant's approval, users may be combined to create copies of the CADD files for informational and reference only. All intellectual or proprietary information, including but not limited to, shall be maintained in confidence and not be disclosed to any third party without the prior written consent of the Consultant. Any use of these files is at the user's own risk.

**SUBMITTAL/REVISIONS**  
 6/17/2016 Check Set  
 2/23/2017 City Submittal

**PROFESSIONAL SIGNATURE**  
 I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.  
 Date: 4/8/16

**QUALITY CONTROL**  
 Loucks Project No. 15597  
 Project Lead NMM  
 Drawn By JDK  
 Checked By NMM  
 Review Date

**SHEET INDEX**

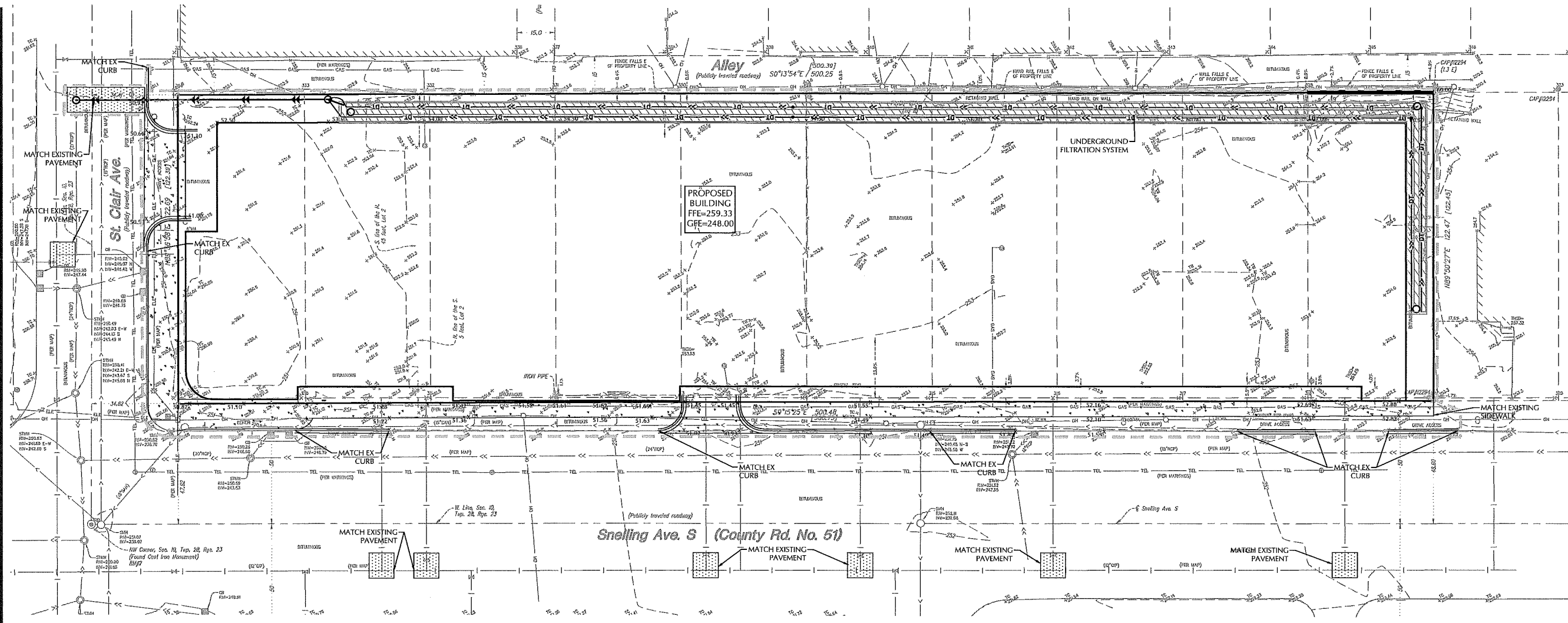
|      |                 |
|------|-----------------|
| C1-2 | DEMOLITION PLAN |
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| C3-2 | SWPPP           |
| C3-3 | SWPPP NOTES     |
| C4-1 | UTILITY PLAN    |
| C8-1 | DETAILS         |
| C8-2 | DETAILS         |

**SITE PLAN**  
**C2-1**

Plotted: 02/23/2017 12:45 PM W:\2015\1597\CADD\DATA\CIVIL\DWG\Sheet\Files\C2-1\_Site Plan

**Gopher State One Call**  
 TWIN CITY AREA: 651-454-0002  
 TOLL FREE: 1-800-252-1166





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CADD files created by the Consultant for this project are the property of the Consultant. The Consultant shall not be held responsible for any errors or omissions in the CADD files. The Consultant shall not be held responsible for any errors or omissions in the CADD files. The Consultant shall not be held responsible for any errors or omissions in the CADD files.

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**GRADING, DRAINAGE & EROSION CONTROL NOTES**

- SPOT ELEVATIONS REPRESENT FINISHED SURFACE GRADES, GUTTER/FLOW LINE, FACE OF BUILDING, OR EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- CATCH BASINS AND MANHOLES IN PAVED AREAS SHALL BE SUMPED 0.04 FEET. ALL CATCH BASINS IN GUTTERS SHALL BE SUMPED 0.16 FEET. RIM ELEVATIONS SHOWN ON PLANS DO NOT REFLECT SUMPED ELEVATIONS.
- ALL DISTURBED UNPAVED AREAS ARE TO RECEIVE MINIMUM OF 4 INCHES OF TOP SOIL AND SEED/MULCH OR SOD. THESE AREAS SHALL BE WATERED/MAINTAINED BY THE CONTRACTOR UNTIL VEGETATION IS ESTABLISHED.
- FOR SITE RETAINING WALLS "TW" EQUALS SURFACE GRADE AT TOP FACE OF WALL (NOT TOP OF WALL), "GW" EQUALS SURFACE GRADE AT WALL GRADE TRANSITION, AND "BW" EQUALS SURFACE GRADE AT BOTTOM FACE OF WALL (NOT BOTTOM OF BURIED WALL COURSES).
- REFER TO THE REPORT OF GEOTECHNICAL EXPLORATION AND REVIEW (REPORT NO. 15-0933), DATED MAY 25, 2016 AS PREPARED BY HAUGO GEOTECHNICAL FOR AN EXISTING SUBSURFACE SITE CONDITION ANALYSIS AND CONSTRUCTION RECOMMENDATIONS.
- STREETS MUST BE CLEANED AND SWEEPED WHENEVER TRACKING OF SEDIMENTS OCCURS AND BEFORE SURFACES WILL BE LEFT IDLE FOR WEEKENDS AND HOLIDAYS. A REGULAR SWEEPING SCHEDULE MUST BE ESTABLISHED.
- DUST MUST BE ADEQUATELY CONTROLLED.
- SEE SWPPP FOR ADDITIONAL EROSION CONTROL NOTES AND REQUIREMENTS.
- SEE UTILITY PLAN FOR WATER, STORM AND SANITARY SEWER INFORMATION.
- SEE SITE PLAN FOR CURB AND BITUMINOUS TAPER LOCATIONS.



**WARNING:**  
THE CONTRACTOR SHALL BE RESPONSIBLE FOR CALLING FOR LOCATIONS OF ALL EXISTING UTILITIES. THEY SHALL COOPERATE WITH ALL UTILITY COMPANIES IN MAINTAINING THEIR SERVICE AND/OR RELOCATION OF LINES.  
THE CONTRACTOR SHALL CONTACT GOPHER STATE ONE CALL AT 651-454-0002 AT LEAST 48 HOURS IN ADVANCE FOR THE LOCATIONS OF ALL UNDERGROUND WIRES, CABLES, CONDUITS, PIPES, MANHOLES, VALVES OR OTHER BURIED STRUCTURES BEFORE DIGGING. THE CONTRACTOR SHALL REPAIR OR REPLACE THE ABOVE WHEN DAMAGED DURING CONSTRUCTION AT NO COST TO THE OWNER.

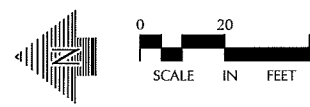
| EXISTING | CIVIL LEGEND            | PROPOSED |
|----------|-------------------------|----------|
|          | SANITARY MANHOLE        |          |
|          | STORM MANHOLE           |          |
|          | CATCH BASIN             |          |
|          | CULVERT                 |          |
|          | HYDRANT                 |          |
|          | GATE VALVE              |          |
|          | POST INDICATOR VALVE    |          |
|          | LIGHT POLE              |          |
|          | POWER POLE              |          |
|          | SIGN                    |          |
|          | BOX MANHOLE             |          |
|          | WATER MANHOLE           |          |
|          | TELEPHONE MANHOLE       |          |
|          | UTILITY MANHOLE         |          |
|          | ELECTRIC MANHOLE        |          |
|          | WATER SERVICE           |          |
|          | SANITARY SERVICE        |          |
|          | HANDICAP PARKING        |          |
|          | DIRECTION OF FLOW       |          |
|          | SPOT ELEVATION          |          |
|          | CONTOURS                |          |
|          | SANITARY SEWER          |          |
|          | STORM SEWER             |          |
|          | WATERMAIN               |          |
|          | FOREMAN                 |          |
|          | DRANKLE                 |          |
|          | SILT FENCE              |          |
|          | CURB & GUTTER           |          |
|          | RETAINING WALL          |          |
|          | TRESTLE                 |          |
|          | EASEMENT LINE           |          |
|          | SETBACK LINE            |          |
|          | FENCE LINE              |          |
|          | UNDERGROUND TELE        |          |
|          | UNDERGROUND GAS         |          |
|          | OVERHEAD UTILITY        |          |
|          | UNDERGROUND FIBER OPTIC |          |
|          | UNDERGROUND ELECTRIC    |          |
|          | UNDERGROUND CABLE TV    |          |
|          | PROPERTY LINE           |          |
|          | CONIFEROUS TREE         |          |
|          | DECIDUOUS TREE          |          |
|          | PARKING COURTS          |          |

**PROFESSIONAL SIGNATURE**  
I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer and the Licensee of the State of Minnesota.  
Date: \_\_\_\_\_  
Signature: \_\_\_\_\_  
45861

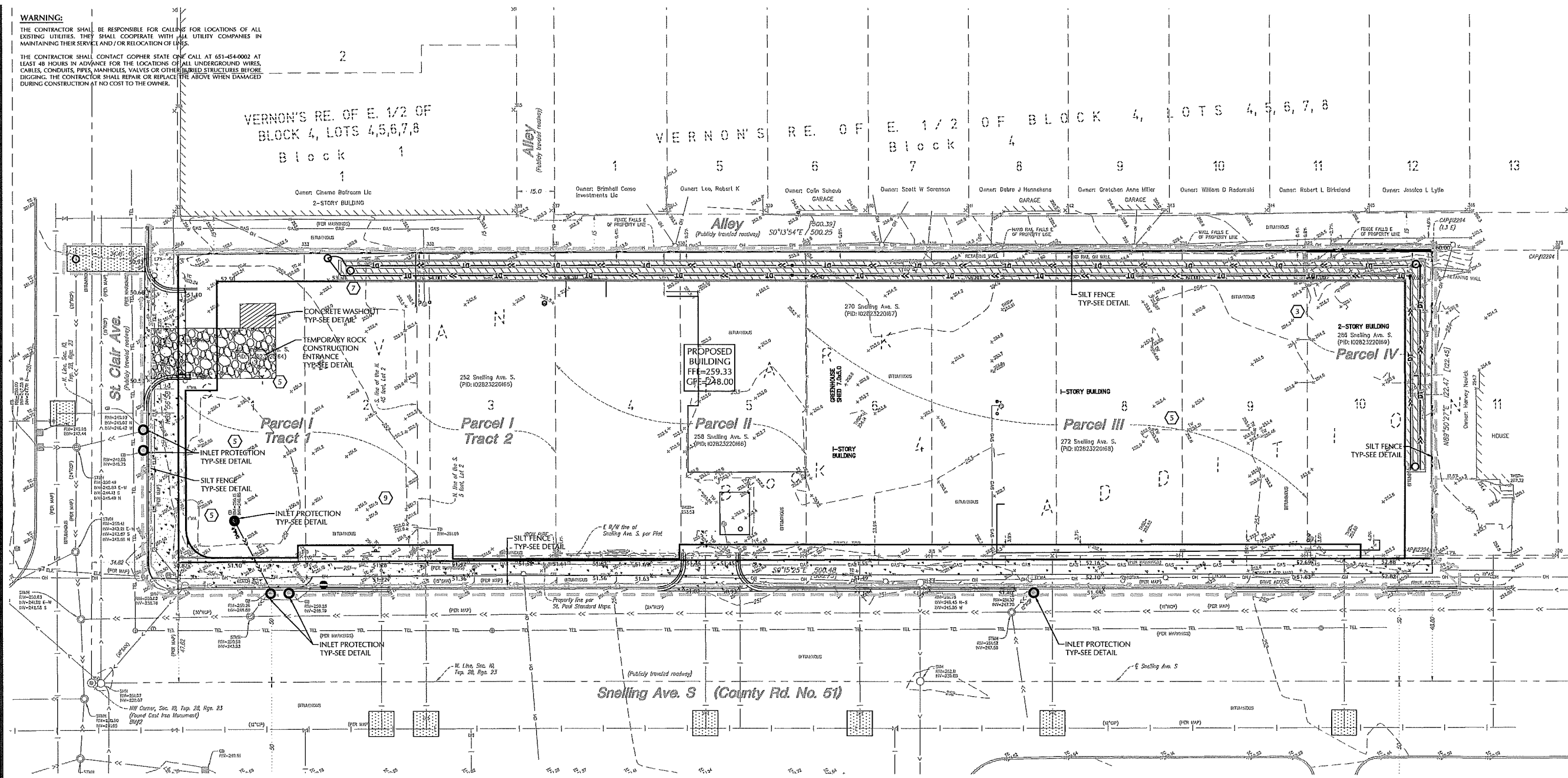
**QUALITY CONTROL**  
Loucks Project No. 15597  
Project Lead NMM  
Drawn By JDK  
Checked By NMM  
Review Date \_\_\_\_\_

**SHEET INDEX**

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|------|-----------------|
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**SNELLING MULTI-FAMILY**  
 ST. PAUL, MN

LECESSE DEVELOPMENT

**LOUCKS**  
 PLANNING  
 CIVIL ENGINEERING  
 LAND SURVEYING  
 LANDSCAPE ARCHITECTURE  
 ENVIRONMENTAL  
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 Maple Grove, MN 55369  
 763.424.5505  
 www.loucksinc.com

**CADD QUALIFICATION**  
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**SUBMITTAL/REVISIONS**  
 6/17/2016 Check Set  
 2/23/2017 City Submittal

**PROFESSIONAL SIGNATURE**  
 I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.  
 Date 4/5/16

**NOT FOR CONSTRUCTION**  
 45861

**QUALITY CONTROL**  
 Loucks Project No. 15597  
 Project Lead NMM  
 Drawn By JDK  
 Checked By NMM  
 Review Date

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**SWPPP**  
**C3-2**

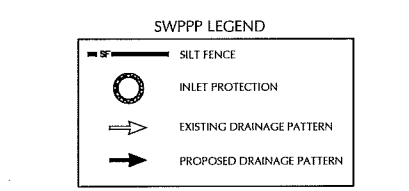
**GENERAL NOTES**

- MINNESOTA STATE STATUTE REQUIRES NOTIFICATION PER "GOPHER STATE ONE CALL" PRIOR TO COMMENCING ANY GRADING, EXCAVATION OR UNDERGROUND WORK.
- THE CONTRACTOR SHALL FIELD VERIFY LOCATIONS AND ELEVATIONS OF EXISTING UTILITIES AND TOPOGRAPHIC FEATURES PRIOR TO COMMENCEMENT OF CONSTRUCTION ACTIVITY. THE CONTRACTOR SHALL NOTIFY THE ENGINEER OF ANY DISCREPANCIES OR VARIATIONS FROM THE PLANS.
- THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS NECESSARY TO AVOID PROPERTY DAMAGE TO ADJACENT PROPERTIES DURING THE CONSTRUCTION PHASE OF THIS PROJECT. THE CONTRACTOR WILL BE HELD RESPONSIBLE FOR ANY DAMAGES TO ADJACENT PROPERTIES OCCURRING DURING THE CONSTRUCTION PHASE OF THIS PROJECT.
- THE CONTRACTOR WILL BE RESPONSIBLE FOR PROVIDING AND MAINTAINING TRAFFIC CONTROL DEVICES SUCH AS BARRICADES, WARNING SIGNS, DIRECTIONAL SIGNS, FLAGMEN AND LIGHTS TO CONTROL THE MOVEMENT OF TRAFFIC WHERE NECESSARY. PLACEMENT OF THESE DEVICES SHALL BE APPROVED BY THE ENGINEER PRIOR TO PLACEMENT. TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE APPROPRIATE MINNESOTA DEPARTMENT OF TRANSPORTATION STANDARDS.
- IN ACCORDANCE WITH GENERALLY ACCEPTED CONSTRUCTION PRACTICES, THE CONTRACTOR WILL BE SOLELY AND COMPLETELY RESPONSIBLE FOR CONDITIONS ON THE JOB SITE, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY DURING THE PERFORMANCE OF THE WORK. THIS REQUIREMENT WILL APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS.
- THE DUTY OF THE ENGINEER OR THE DEVELOPER TO CONDUCT CONSTRUCTION REVIEW OF THE CONTRACTORS PERFORMANCE IS NOT INTENDED TO INCLUDE REVIEW OF THE ADEQUACY OF THE CONTRACTORS SAFETY MEASURES IN, OR NEAR THE CONSTRUCTION SITE.
- BEFORE BEGINNING CONSTRUCTION THE CONTRACTOR SHALL INSTALL EROSION AND SEDIMENTATION CONTROL MEASURES IN ACCORDANCE WITH NPDES PERMIT REQUIREMENTS, BEST MANAGEMENT PRACTICES, STATE AND LOCAL REQUIREMENTS AND THE DETAILS SHOWN ON THE DETAIL SHEETS) OF THE PROJECT PLANS.
- ALL CONSTRUCTION PERMITS, APPLICATIONS AND FEES ARE THE RESPONSIBILITY OF THE CONTRACTOR.
- ALL ENTRANCES AND CONNECTIONS TO CITY STREETS SHALL BE CONSTRUCTED PER THE REQUIREMENTS OF THE STATE AND LOCAL JURISDICTIONS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL PERMITS AND NOTIFICATIONS AS REQUIRED.
- ALL STREET REPAIRS AND PATCHING SHALL BE PERFORMED PER THE REQUIREMENTS OF THE CITY. ALL TRAFFIC CONTROL SHALL BE PROVIDED BY THE CONTRACTOR AND SHALL BE ESTABLISHED PER THE REQUIREMENTS OF THE MINNESOTA MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) AND THE CITY. THIS SHALL INCLUDE ALL SIGNAGE, BARRICADES, FLASHERS AND FLAGGERS AS NEEDED. ALL PUBLIC STREETS SHALL BE OPEN TO TRAFFIC AT ALL TIMES.
- ADJUST ALL EXISTING STRUCTURES, BOTH PUBLIC AND PRIVATE TO THE PROPOSED GRADES WHERE DISTURBED AND COMPLY WITH ALL REQUIREMENTS OF THE UTILITY OWNERS. STRUCTURES BEING RESET TO PAVED AREAS MUST MEET OWNERS REQUIREMENTS FOR TRAFFIC LOADING.
- EXISTING TOPOGRAPHY PROVIDED BY LOUCKS, ALTA/ACSM LAND TITLE SURVEY, DATED MARCH 2, 2016.

**CITY OF ST. PAUL NOTES**

- SIGNS REGULATING PARKING AND/OR TRAFFIC ON PRIVATE PROPERTY SHALL BE INSTALLED BY THE PROPERTY OWNER OR CONTRACTOR OUTSIDE OF THE PUBLIC RIGHT-OF-WAY. SIGNS APPROVED BY PUBLIC WORKS TRAFFIC ENGINEERING REGULATING PARKING AND/OR TRAFFIC IN THE PUBLIC RIGHT-OF-WAY FOR THIS DEVELOPMENT SHALL BE INSTALLED BY CITY FORCES AT THE EXPENSE OF THE DEVELOPMENT. CONTACT TRAFFIC ENGINEERING 651-266-6200 SIX WEEKS IN ADVANCE OF NEEDED SIGNS).
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- NO PRIVATE FACILITIES IN THE RIGHT OF WAY: THE DEVELOPER IS STRICTLY PROHIBITED FROM INSTALLING PRIVATE ELECTRICAL WIRING, CONDUIT, RECEPTACLES AND/OR LIGHTING IN THE CITY'S RIGHT OF WAY. THIS INCLUDES STUBBING CONDUIT OR CABLE INTO THE PUBLIC RIGHT OF WAY TO ACCOMMODATE UTILITY FEEDS TO THE SITE. COORDINATE WITH EACH UTILITY PRIOR TO CONSTRUCTION TO DETERMINE FEED POINTS INTO THE PROPERTY. UTILITIES ARE RESPONSIBLE FOR SECURING EXCAVATION PERMITS TO RUN THEIR SERVICE INTO A SITE, AND (WHERE REQUIRED) SUBMITTING PLANS FOR REVIEW BY THE PUBLIC WORKS UTILITY REVIEW COMMITTEE.
- ANY SIGN OF FIXTURES REMOVED WITHIN THE RIGHT OF WAY OR AS PART OF THE SITE WORK SHALL BE REPLACED BY THE CITY. THE CONTRACTOR SHALL CONTACT MIKE MILLER IN TRAFFIC OPERATIONS AT 651-266-9726.
- AN OBSTRUCTION PERMIT MUST BE OBTAINED FROM PUBLIC WORKS (651-266-6151) IF THERE WILL BE ANY EXCAVATION IN KELLOGG BLVD OR WABASHA ST. OR IF TRUCKS/EQUIPMENT WILL BE DRIVING OVER CURBS OR IF CONSTRUCTION WILL BLOCK CITY STREETS, OR SIDEWALKS.
- ALL UNUSED EXISTING WATER SERVICES TO BE CUT OFF BY SAINT PAUL REGIONAL WATER SERVICE, WORK SCHEDULED BY OWNER'S CONTRACTOR.

**CALL BEFORE YOU DIG**  
**Gopher State One Call**  
 TWIN CITY AREA: 651-454-0002  
 TOLL FREE: 1-800-252-1166



**ESTIMATED QUANTITIES**

| DESCRIPTION                          | UNIT | QUANTITY |
|--------------------------------------|------|----------|
| TEMPORARY ROCK CONSTRUCTION ENTRANCE | EA   | 1        |
| PREFABRICATED CONCRETE WASHOUT       | EA   | 1        |
| SILT FENCE (STANDARD)                | LF   | 170      |
| BIOROLL LOGS                         | LF   | 200      |
| INLET PROTECTION                     | EA   | 18       |
| SEEDING                              | AC   | 0.XX     |

Project: 02\_23\_2017 1:34 PM W:\30151\5897\CADD\DATA\CIVIL\Job Sheet Files\C3-2 SWPPP



**SWPPP NOTES**

- THE NATURE OF THIS PROJECT WILL CONSIST OF CONSTRUCTING A 128 UNIT APARTMENT BUILDING, SURFACE PAVEMENTS, UTILITIES AND UNDERGROUND FILTRATION STORMWATER FACILITIES.
- THE INTENDED SEQUENCING OF MAJOR CONSTRUCTION ACTIVITIES ARE AS FOLLOWS:
  - INSTALL VEHICLE TRACKING BMP
  - CONTACT CITY FOR INSPECTION OF SILT FENCE STAKING LOCATIONS
  - INSTALL SILT FENCE AROUND SITE
  - CLEAR AND GRUB SITE
  - STRIP AND STOCKPILE TOPSOIL
  - REMOVE EXISTING BUILDINGS
  - REMOVE PAVEMENTS AND UTILITIES
  - ROUGH GRADE SITE
  - IMPORT CLEAN FILL FOR REPLACEMENT AND BALANCE
  - INSTALL UTILITIES
  - INSTALL BUILDING FOUNDATIONS
  - INSTALL CURB AND GUTTER
  - INSTALL PAVEMENTS AND WALKS
  - FINAL GRADE SITE
  - REMOVE ACCUMULATED SEDIMENT FROM STORMWATER STRUCTURES
  - SEED AND MULCH
  - WHEN ALL CONSTRUCTION ACTIVITY IS COMPLETE AND THE SITE IS STABILIZED, REMOVE SILT FENCE AND RESEED ANY AREAS DISTURBED BY THE REMOVAL.
- SITE DATA:
 

|                                    |         |
|------------------------------------|---------|
| AREA OF DISTURBANCE:               | 1.40 AC |
| PRE-CONSTRUCTION IMPERVIOUS AREA:  | 1.35 AC |
| POST-CONSTRUCTION IMPERVIOUS AREA: | 1.35 AC |

GENERAL SOIL TYPE: SEE GEOTECHNICAL EVALUATION REPORT
- THE LOCATION OF AREAS NOT TO BE DISTURBED MUST BE IDENTIFIED WITH FLAGS, STAKES, SIGNS, SILT FENCE, ETC. BEFORE CONSTRUCTION BEGINS.
- ALL DISTURBED GROUND LEFT INACTIVE FOR FOURTEEN (14) OR MORE DAYS SHALL BE STABILIZED BY SEEDING OR MULCHING ONLY AVAILABLE PRIOR TO SEPTEMBER 15) OR BY MULCHING OR COVERING OR OTHER EQUIVALENT CONTROL MEASURE.
- ON SLOPES 3:1 MAINTAIN SHEET FLOW AND MINIMIZE RILLS AND/OR GULLIES. SLOPE LENGTHS CAN NOT BE GREATER THAN 75 FEET.
 

██████ DENOTES SLOPES GREATER THAN 3:1. ALL 3:1 SLOPES TO BE STABILIZED WITH EROSION CONTROL BLANKET
- ALL STORM DRAINS AND INLETS MUST BE PROTECTED UNTIL ALL SOURCES OF POTENTIAL DISCHARGE ARE STABILIZED.
- TEMPORARY SOIL STOCKPILES MUST HAVE EFFECTIVE SEDIMENT CONTROL AND CAN NOT BE PLACED IN SURFACE WATERS OR STORM CONVEYANCE SYSTEMS. TEMPORARY STOCKPILES WITHOUT SIGNIFICANT AMOUNT OF SILT, CLAY, OR ORGANIC COMPOUNDS ARE EXEMPT EX: CLEAN AGGREGATE STOCK PILES, DEMOLITION CONCRETE STOCKPILES, SAND STOCKPILES.
- SEDIMENT LADEN WATER MUST BE DISCHARGED TO A SEDIMENTATION BASIN WHENEVER POSSIBLE. IF NOT POSSIBLE, IT MUST BE TREATED WITH THE APPROPRIATE BMP'S.
- SOLID WASTE MUST BE DISPOSED OF PROPERLY AND MUST COMPLY WITH MPCA DISPOSAL REQUIREMENTS.
- EXTERNAL WASHING OF CONSTRUCTION VEHICLES MUST BE LIMITED TO A DEFINED AREA OF THE SITE. RUNOFF MUST BE PROPERLY CONTAINED.
- NO ENGINE DEGREASING IS ALLOWED ON SITE.
- THE OWNER WHO SIGNS THE NPDES PERMIT APPLICATION IS A PERMITTEE AND IS RESPONSIBLE FOR COMPLIANCE WITH ALL TERMS AND CONDITIONS OF THE PERMIT. THE OPERATOR (CONTRACTOR) WHO SIGNS THE NPDES PERMIT APPLICATION IS A PERMITTEE FOR PARTS II.B., PART II.C., PART II.B.-F., PART V, PART IV AND APPLICABLE CONSTRUCTION ACTIVITY REQUIREMENTS FOUND IN APPENDIX A, PART C. OF THE NPDES PERMIT AND IS JOINTLY RESPONSIBLE WITH THE OWNER FOR COMPLIANCE WITH THOSE PORTIONS OF THE PERMIT.
- TERMINATION OF COVERAGE-PERMITTEES WISHING TO TERMINATE COVERAGE MUST SUBMIT A NOTICE OF TERMINATION (NOT) TO THE MPCA. ALL PERMITTEES MUST SUBMIT A NOT WITHIN 30 DAYS AFTER ONE OR MORE OF THE FOLLOWING CONDITIONS HAVE BEEN MET:
  - FINAL STABILIZATION, PER NPDES PERMIT PART IV.G. HAS BEEN ACHIEVED ON ALL PORTIONS OF THE SITE FOR WHICH THE PERMITTEE IS RESPONSIBLE.
  - TRANSFER OF OWNERSHIP AS DESCRIBED IN THE PERMIT.
- INSPECTIONS
  - INITIAL INSPECTION FOLLOWING SILT FENCE STAKING BY CITY REPRESENTATIVE IS REQUIRED.
  - INITIAL INSPECTION FOLLOWING SILT FENCE INSTALLATION BY CITY REPRESENTATIVE IS REQUIRED.
  - EXPOSED SOIL AREAS: ONCE EVERY 7 DAYS AND WITHIN 24 HOURS FOLLOWING A 0.5" OVER 24 HOUR RAIN EVENT.
  - STABILIZED AREAS: ONCE EVERY 30 DAYS
  - FROZEN GROUND: AS SOON AS RUNOFF OCCURS OR PRIOR TO RESUMING CONSTRUCTION.
  - INSPECTION AND MAINTENANCE RECORDS MUST BE RETAINED FOR 3 YEARS AFTER FILING OF THE NOTICE OF TERMINATION AND MUST INCLUDE: DATE AND TIME OF ACTION, NAME OF PERSON(S) CONDUCTING WORK, FINDING OF INSPECTIONS AND RECOMMENDATIONS FOR CORRECTIVE ACTION, DATE AND AMOUNT OF RAINFALL EVENTS GREATER THAN 0.5 INCHES IN A 24 HOUR PERIOD.
- MINIMUM MAINTENANCE
  - SILT FENCE TO BE REPAIRED, REPLACED, SUPPLEMENTED WHEN NONFUNCTIONAL, OR 1/3 FULL; WITHIN 24 HOURS
  - SEDIMENT BASINS DRAINED AND SEDIMENT REMOVED WHEN REACHES 1/2 STORAGE VOLUME. REMOVAL MUST BE COMPLETE WITHIN 72 HOURS OF DISCOVERY.
  - SEDIMENT REMOVED FROM SURFACE WATERS WITHIN 7 SEVEN DAYS
  - CONSTRUCTION SITE EXITS INSPECTED, TRACKED SEDIMENT REMOVED WITH 24 HOURS.
  - PROVIDE COPIES OF EROSION INSPECTION RESULTS TO CITY ENGINEER FOR ALL EVENTS GREATER THAN 1/2" IN 24 HOURS
- THE SWPPP, INCLUDING ALL CHANGES TO IT, AND INSPECTIONS AND MAINTENANCE RECORDS MUST BE KEPT AT THE SITE DURING CONSTRUCTION ACTIVITY BY THE PERMITTEES WHO HAVE OPERATIONAL CONTROL OF THE SITE.
- OWNER MUST KEEP RECORDS OF ALL PERMITS REQUIRED FOR THE PROJECT, THE SWPPP, ALL INSPECTIONS AND MAINTENANCE, PERMANENT OPERATION AND MAINTENANCE AGREEMENTS, AND REQUIRED CALCULATIONS FOR TEMPORARY AND PERMANENT STORM WATER MANAGEMENT SYSTEMS. THESE RECORDS MUST BE RETAINED FOR THREE YEARS AFTER FILING NPDES NOTICE OF TERMINATION.
- SWPPP MUST BE AMENDED WHEN:
  - THERE IS A CHANGE IN DESIGN, OPERATION, MAINTENANCE, WEATHER OR SEASONAL CONDITIONS THAT HAS A SIGNIFICANT EFFECT ON DISCHARGE
  - INSPECTIONS INDICATE THAT THE SWPPP IS NOT EFFECTIVE AND DISCHARGE IS EXCEEDING WATER QUALITY STANDARDS.
  - THE BMP'S IN THE SWPPP ARE NOT CONTROLLING POLLUTANTS IN DISCHARGES OR IS NOT CONSISTENT WITH THE TERMS AND CONDITIONS OF THE PERMIT.
- CONCRETE WASHOUT AREA
  - CONTRACTOR TO PROVIDE PREFABRICATED CONCRETE WASH-OUT CONTAINER WITH RAIN PROTECTION PER PLAN.
  - CONCRETE WASH-OUT TO BE IDENTIFIED WITH SIGNAGE STATING "CONCRETE WASHOUT AREA DO NOT OVERFILL"
  - CONCRETE WASHOUT WATER NEEDS TO BE PUMPED WITHIN 24 HOURS OF STANDING WATER IN WASHOUT AREA.
- IN THE EVENT OF ENCOUNTERING A WELL OR SPRING DURING CONSTRUCTION CONTRACTOR TO CEASE CONSTRUCTION ACTIVITY AND NOTIFY ENGINEER.
- PIPE OULETS MUST BE PROVIDED WITH TEMPORARY OR PERMANENT ENERGY DISSIPATION WITHIN 24 HOURS AFTER CONNECTION TO A SURFACE WATER.
- FINAL STABILIZATION
 

FINAL STABILIZATION REQUIRES THAT ALL SOIL DISTURBING ACTIVITIES HAVE BEEN COMPLETED AND THAT DISTURBED AREAS ARE STABILIZED BY A UNIFORM PERENNIAL VEGETATIVE COVER WITH 70% OF THE EXPECTED FINAL DENSITY, AND THAT ALL PERMANENT PAVEMENTS HAVE BEEN INSTALLED. ALL TEMPORARY BMP'S SHALL BE REMOVED, DITCHES STABILIZED, AND SEDIMENT SHALL BE REMOVED FROM PERMANENT CONVEYANCES AND SEDIMENTATION BASINS IN ORDER TO RETURN THE POND TO DESIGN CAPACITY.

- RESPONSIBILITIES
  - THE OWNER MUST IDENTIFY A PERSON WHO WILL OVERSEE THE SWPPP IMPLEMENTATION AND THE PERSON RESPONSIBLE FOR INSPECTION AND MAINTENANCE:
 

CONTACT: \_\_\_\_\_

COMPANY: \_\_\_\_\_

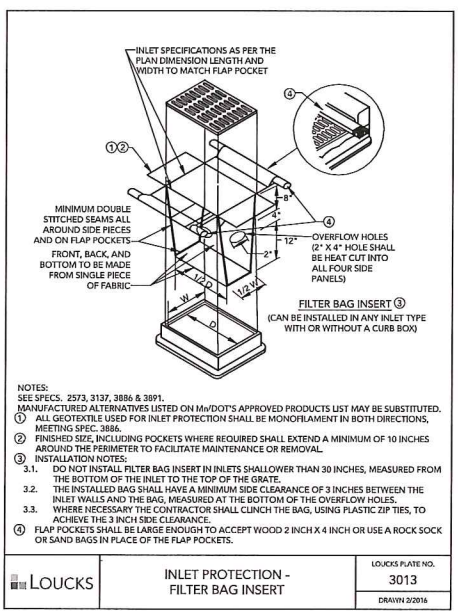
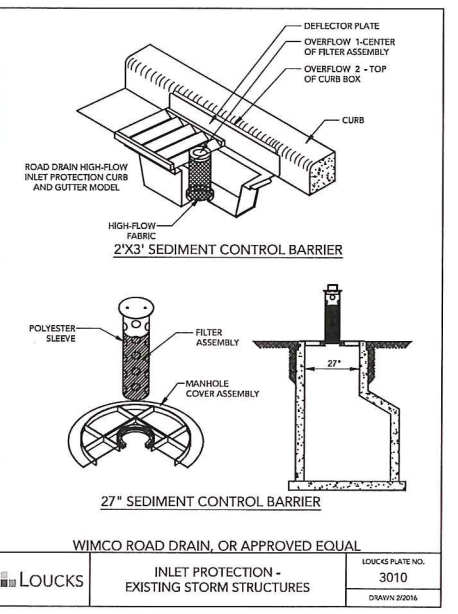
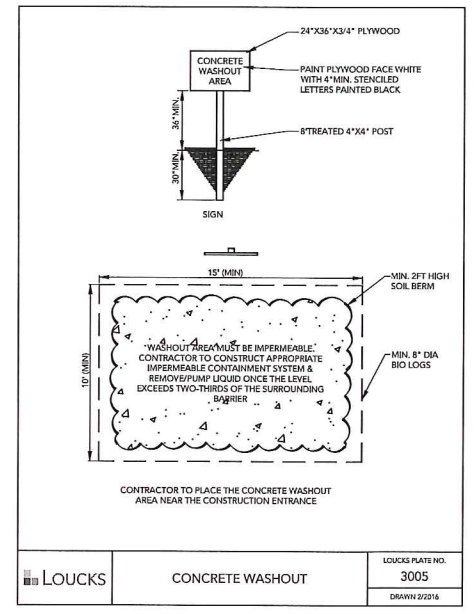
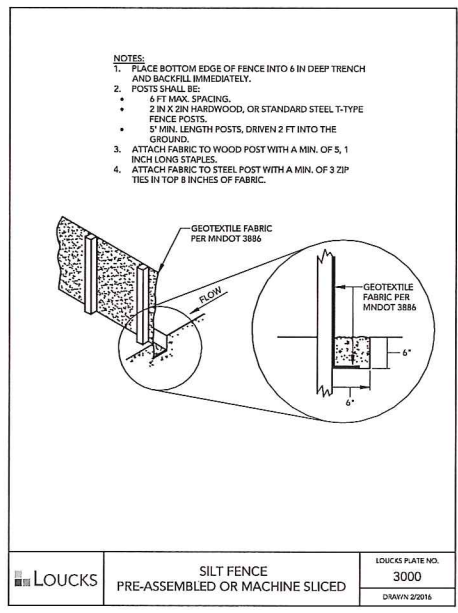
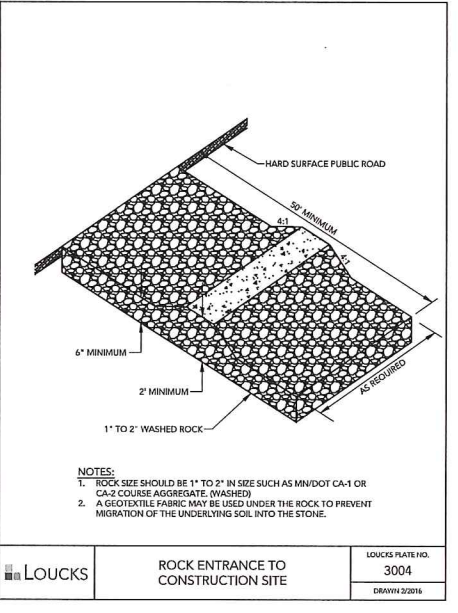
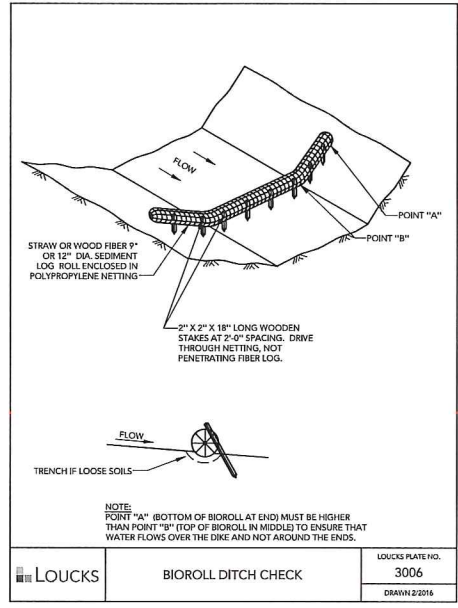
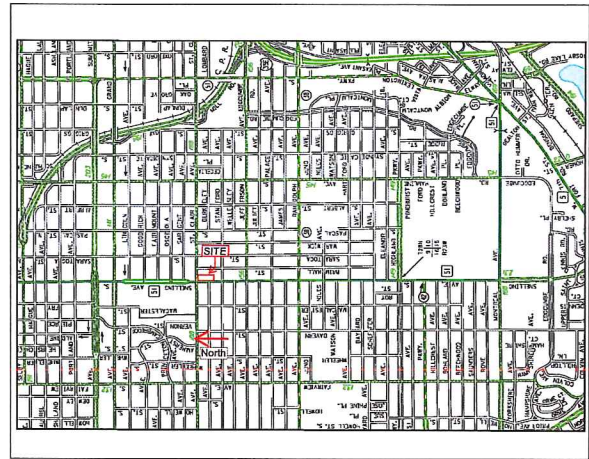
PHONE: \_\_\_\_\_
  - THE OWNER MUST IDENTIFY A PERSON WHO WILL BE RESPONSIBLE FOR LONG TERM OPERATIONS AND MAINTENANCE OF THE PERMANENT STORMWATER MANAGEMENT SYSTEM:
 

CONTACT: \_\_\_\_\_

COMPANY: \_\_\_\_\_

PHONE: \_\_\_\_\_
- THE WATERSHED DISTRICT OR THE CITY MAY HAVE REQUIREMENTS FOR INSPECTIONS OR AS-BUILT DRAWINGS VERIFYING PROPER CONSTRUCTION OF THE BMP'S.

**VICINITY MAP**



**SNELLING MULTI-FAMILY**  
ST. PAUL, MN

**LECESSE DEVELOPMENT**

**LOUCKS**  
PLANNING  
CIVIL ENGINEERING  
LAND SURVEYING  
LANDSCAPE ARCHITECTURE  
ENVIRONMENTAL  
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Maple Grove, MN 55369  
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6/17/2016 Check Set  
2/23/2017 City Submittal

**PROFESSIONAL SIGNATURE**  
I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.  
Date: \_\_\_\_\_  
45861

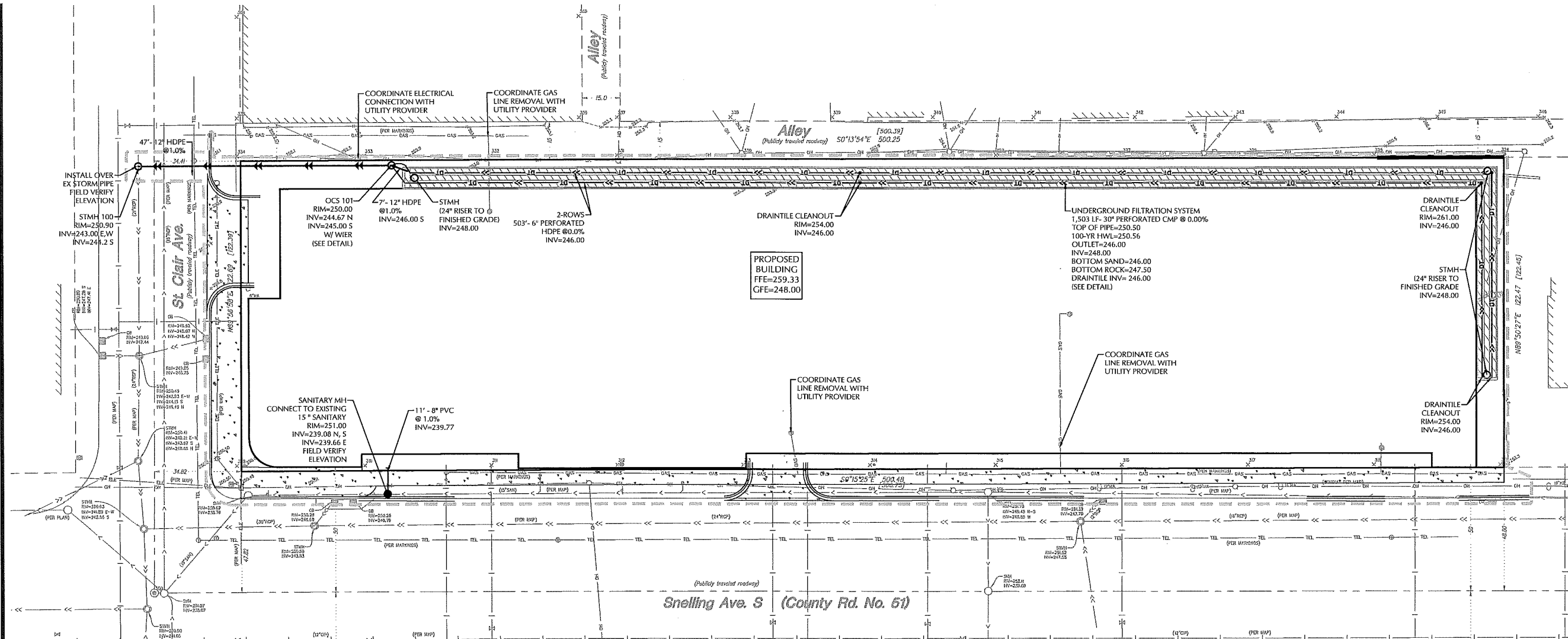
**QUALITY CONTROL**  
Loucks Project No. 15597  
Project Lead NMM  
Drawn By JJK  
Checked By NMM  
Review Date \_\_\_\_\_

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**SWPPP NOTES & DETAILS**  
**C3-3**





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  - INSPECTION CONTACT: THE DEVELOPER SHALL CONTACT THE RIGHT OF WAY INSPECTOR, JIM HEHN, AT 651-485-0417 (ONE WEEK PRIOR TO BEGINNING WORK) TO DISCUSS SAFETY ISSUES SUCH AS TRAFFIC CONTROL AND PEDESTRIAN-SAFE WALKWAYS. OTHER ISSUES THAT MAY AFFECT THE ROW SUCH AS EROSION CONTROL AND COORDINATION WITH OTHER WORK NEARBY THAT THE DEVELOPER MAY NOT BE AWARE OF SHOULD BE DISCUSSED AS WELL.
  - SAFE WORK SITE REQUIREMENTS: THE CONTRACTOR SHALL PROVIDE A CONTINUOUS, ACCESSIBLE AND SAFE PEDESTRIAN WALKWAY THAT MEETS ADA AND MN MUTCD STANDARDS IF WORKING IN A SIDEWALK AREA, AND TRAFFIC CONTROL PER MN MUTCD REQUIREMENTS FOR WORK IN THE PUBLIC RIGHT OF WAY.
  - NO PRIVATE FACILITIES IN THE RIGHT OF WAY: THE DEVELOPER IS STRICTLY PROHIBITED FROM INSTALLING PRIVATE ELECTRICAL WIRING, CONDUIT, RECEPTACLES AND/OR LIGHTING IN THE CITY'S RIGHT OF WAY. THIS INCLUDES STUBBING CONDUIT OR CABLE INTO THE PUBLIC RIGHT OF WAY TO ACCOMMODATE UTILITY FEEDS TO THE SITE. COORDINATE WITH EACH UTILITY PRIOR TO CONSTRUCTION TO DETERMINE FEED POINTS INTO THE PROPERTY. UTILITIES ARE RESPONSIBLE FOR SECURING EXCAVATION PERMITS TO RUN THEIR SERVICE INTO A SITE, AND (WHERE REQUIRED) SUBMITTING PLANS FOR REVIEW BY THE PUBLIC WORKS UTILITY REVIEW COMMITTEE.
  - ANY SIGN OF FIXTURES REMOVED WITH IN THE RIGHT OF WAY OR AS PART OF THE SITE WORK SHALL BE REPLACED BY THE CITY. THE CONTRACTOR SHALL CONTACT MIKE MILLER IN TRAFFIC OPERATIONS AT 651-266-9726.
  - AN OBSTRUCTION PERMIT MUST BE OBTAINED FROM PUBLIC WORKS (651-266-6151) IF THERE WILL BE ANY EXCAVATION IN KELLOGG BLVD OR WABASHA ST. OR IF TRUCKS/EQUIPMENT WILL BE DRIVING OVER CURBS OR IF CONSTRUCTION WILL BLOCK CITY STREETS, OR SIDEWALKS.
  - ALL UNUSED EXISTING WATER SERVICES TO BE CUT OFF BY SAINT PAUL REGIONAL WATER SERVICE, WORK SCHEDULED BY OWNER'S CONTRACTOR.

- UTILITY NOTES**
- ALL SANITARY SEWER, STORM SEWER AND WATERMAIN UTILITIES SHALL BE FURNISHED AND INSTALLED PER THE REQUIREMENTS OF THE SPECIFICATIONS, THE MINNESOTA PLUMBING CODE, THE LOCAL GOVERNING UNIT, AND THE STANDARD UTILITIES SPECIFICATION OF THE CITY ENGINEERS ASSOCIATION OF MINNESOTA (CEAM), 2013 EDITION.
  - ALL UTILITY PIPE BEDDING SHALL BE COMPACTED SAND OR FINE GRANULAR MATERIAL. ALL COMPACTON SHALL BE PERFORMED PER THE REQUIREMENTS OF THE CEAM SPECIFICATION AND THE GEOTECHNICAL REPORT.
  - ALL CONNECTIONS TO EXISTING UTILITIES SHALL BE PERFORMED PER THE REQUIREMENTS OF THE STATE AND LOCAL JURISDICTIONS. THE CITY DEPARTMENT OF ENGINEERING AND BUILDING INSPECTIONS DEPARTMENT AND THE CONSTRUCTION ENGINEER MUST BE NOTIFIED AT LEAST 48 HOURS PRIOR TO ANY WORK WITHIN THE PUBLIC RIGHT OF WAY, OR WORK IMPACTING PUBLIC UTILITIES.
  - ALL STORM SEWER, SANITARY SEWER AND WATER SERVICES SHALL TERMINATE 5' FROM THE BUILDING FACE UNLESS OTHERWISE NOTED.
  - A MINIMUM OF 18 INCHES OF VERTICAL SEPARATION AND 10 FEET OF HORIZONTAL SEPARATION IS REQUIRED FOR ALL UTILITIES UNLESS OTHERWISE NOTED.
  - ALL NEW WATERMAIN AND SERVICES MUST HAVE A MINIMUM OF 8.0 FEET OF COVER. EXTRA DEPTH MAY BE REQUIRED TO MAINTAIN A MINIMUM 18" VERTICAL SEPARATION TO SANITARY OR STORM SEWER LINES. THE CONTRACTOR SHALL FIELD ADJUST WATERMAIN TO AVOID CONFLICTS WITH SANITARY SEWER, STORM SEWER, AND SERVICES AS REQUIRED. INSULATION OF WATER AND SANITARY SEWER LINES SHALL BE PROVIDED WHERE 8.0 FEET MINIMUM DEPTH CAN NOT BE ATTAINED.
  - ALL FIRE HYDRANTS SHALL BE LOCATED 5 FEET BEHIND BACK OF CURB OR EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
  - PROPOSED PIPE MATERIALS:  
WATERMAIN DIP 6" DIAMETER  
SANITARY SEWER PVC SDR 35 6" DIAMETER  
STORM SEWER DUAL WALL HDPE 12" TO 30" DIAMETER  
STORM SEWER PVC SDR 35 6" DIAMETER
  - ALL SANITARY SEWER WYES, TEES AND SERVICES SHALL BE 90° PVC SDR 35.
  - ALL PORTIONS OF THE STORM SEWER SYSTEM, INCLUDING CATCH BASINS, LOCATED WITHIN 10 FEET OF THE BUILDING OR WATER SERVICE LINE MUST BE TESTED ACCORDANCE WITH MINNESOTA RULES, PART 4715.2820.
  - ALL JOINTS AND CONNECTIONS IN THE STORM SEWER SYSTEM SHALL BE CASTIGHT OR WATERTIGHT (SEE MINNESOTA RULES, PART 4715.0700). APPROVED RESILIENT RUBBER JOINTS MUST BE USED TO MAKE WATERTIGHT CONNECTIONS TO MANHOLES, CATCHBASINS, AND OTHER STRUCTURES.
  - HIGH-DENSITY POLYETHYLENE (HDPE) STORM DRAINS MUST COMPLY WITH MINNESOTA RULES, PART 4715.0540:  
a. PIPES 4-INCH TO 10-INCH IN SIZE MUST COMPLY WITH AASHTO M252.  
b. PIPES 12-INCH TO 60-INCH IN SIZE MUST COMPLY WITH ASTM F2306.  
c. ALL FITTINGS MUST COMPLY WITH ASTM D3212.  
d. WATER-TIGHT JOINTS MUST BE USED AT ALL CONNECTIONS INCLUDING STRUCTURES.



**WARNING:**  
THE CONTRACTOR SHALL BE RESPONSIBLE FOR CALLING FOR LOCATIONS OF ALL EXISTING UTILITIES. THEY SHALL COOPERATE WITH ALL UTILITY COMPANIES IN MAINTAINING THEIR SERVICE AND / OR RELOCATION OF LINES.  
THE CONTRACTOR SHALL CONTACT GOPHER STATE ONE CALL AT 651-454-0002 AT LEAST 48 HOURS IN ADVANCE FOR THE LOCATIONS OF ALL UNDERGROUND WIRES, CABLES, CONDUITS, PIPES, MANHOLES, VALVES OR OTHER BURIED STRUCTURES BEFORE DIGGING. THE CONTRACTOR SHALL REPAIR OR REPLACE THE ABOVE WHEN DAMAGED DURING CONSTRUCTION AT NO COST TO THE OWNER.

**BENCHMARK**  
BENCHMARK #1: TOP OF TOP NUT OF HYDRANT IN THE SE QUAD OF SARATOGA ST. S. AND ST. CLAIRE AVE. ELEVATION = 258.69 FEET (ST. PAUL DATUM)  
BENCHMARK #2: CAST IRON MONUMENT AT THE NW CORNER OF SECTION 10, TOWNSHIP 28, RANGE 23. ELEVATION = 250.66 FEET (ST. PAUL DATUM)  
ELEVATIONS SHOWN ARE BASED ON CITY OF ST. PAUL DATUM.  
CONVERSION FACTOR TO 1929 NGVD, ADD 694.10.

- CITY OF ST. PAUL PERMIT REQUIREMENTS**
- ORDERING OBSTRUCTION AND EXCAVATION PERMITS: CONTACT PUBLIC WORKS RIGHT OF WAY SERVICE DESK AT (651) 266-6151. IT IS STRONGLY RECOMMENDED THAT CONTRACTORS CALL FOR COST ESTIMATES PRIOR TO BIDDING TO OBTAIN ACCURATE COST ESTIMATES.
  - OBSTRUCTION PERMITS: THE CONTRACTOR MUST OBTAIN AN OBSTRUCTION PERMIT IF CONSTRUCTION (INCLUDING SILT FENCES) WILL BLOCK CITY STREETS, SIDEWALKS OR ALLEYS, OR IF DRIVING OVER CURBS.
  - EXCAVATION PERMITS: ALL DIGGING IN THE PUBLIC RIGHT OF WAY REQUIRES AN EXCAVATION PERMIT. IF THE PROPOSED BUILDING IS CLOSE TO THE RIGHT OF WAY, AND EXCAVATING INTO THE RIGHT OF WAY IS NEEDED TO FACILITATE CONSTRUCTION, CONTACT THE UTILITY INSPECTOR.
  - FAILURE TO SECURE PERMITS: FAILURE TO SECURE OBSTRUCTION PERMITS OR EXCAVATION PERMITS WILL RESULT IN A DOUBLE-PERMIT FEE AND OTHER FEES REQUIRED UNDER CITY OF ST. PAUL LEGISLATIVE CODES.
  - REQUIREMENTS TO WORK IN THE PUBLIC RIGHT OF WAY: ALL UTILITIES AND CONTRACTORS WORKING IN THE PUBLIC RIGHT OF WAY MUST BE REGISTERED, INSURED AND BONDED, AS RECOGNIZED BY THE PUBLIC WORKS SERVICE DESK. (651-266-6151)

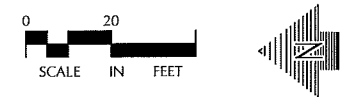
**PROFESSIONAL SIGNATURE**  
I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.  
Date: \_\_\_\_\_ 45861

**QUALITY CONTROL**

|                   |       |
|-------------------|-------|
| Locks Project No. | 15597 |
| Project Lead      | NMM   |
| Drawn By          | JDK   |
| Checked By        | NMM   |
| Review Date       |       |

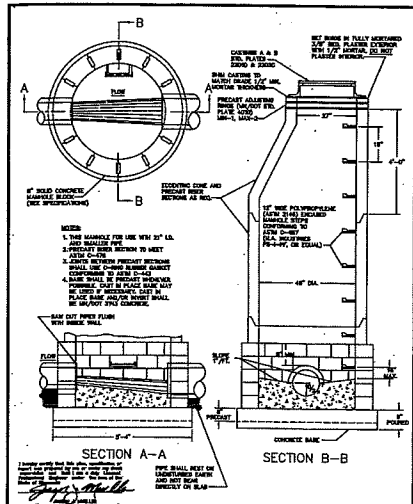
**SHEET INDEX**

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| C3-1 | GRADING PLAN    |
| C3-2 | SWPPP           |
| C3-3 | SWPPP NOTES     |
| C4-1 | UTILITY PLAN    |
| C8-1 | DETAILS         |
| C8-2 | DETAILS         |





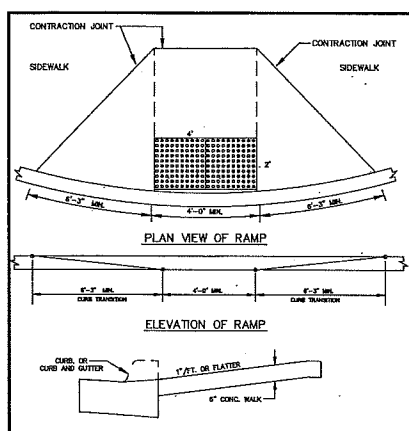
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1304C



APPROVED: [Signature] CITY ENGINEER  
CITY OF ST. PAUL  
DEPARTMENT OF PUBLIC WORKS  
MANHOLE, DESIGN TYPE II  
Date of Revision: JULY 2002  
Standard Plate No. 2102C

**CITY OF SAINT PAUL GUIDE  
for  
CONSTRUCTION OF PEDESTRIAN CURB RAMPS FOR THE HANDICAPPED**

1. Pedestrian Curb Ramps will be constructed as per City of Saint Paul, Department of Public Works Standard Plate No. 3003, 3003D, or Standard Plate No. 3003D.
2. The degree of slope specified on the City of Saint Paul, Department of Public Works Standard Plate No. 3003, 3003D, and 3003D is 1" per foot or flatter. This specification will be met whenever possible, even through areas existing sidewalk, whenever required. The exception being when the removal will cause damage to the adjacent property.
3. Surface treatment shall consist of ADA required Truncated Domes tile inserts. Truncated Domes tile inserts shall be used from end to end and shall be provided by Howard-Francis, Inc. or approved equal.
4. The City will act, as a matter of policy, to install Pedestrian Curb Ramps in front of private residences. Requests for a ramp in front of commercial enterprises that serve the handicapped will be considered on an individual basis. The final decision for each individual request will be based on the following:
  - a. Volume of the existing potential users.
  - b. Cost and practicality of construction. (Existing walls that are encumbered with awnings or loading systems are very costly)
  - c. Evaluate the benefits of ramp installation against the liability increase resulting from grade changes.
  - d. Feasibility to housing specifically designed for the handicapped.
5. Pedestrian Curb Ramps will be located as per City of Saint Paul, Department of Public Works Standard Plate No. 3003, with some exceptions. For example, the specific problems of individual quarters will have to be considered. Utility overhead will affect the location of some Pedestrian Curb Ramps.
6. In project areas of partial street construction (e.g., street overlays, sewer trenching and restoration, sidewalk construction only in areas of existing curb, and curb construction only in areas of existing walk), the removal of existing sidewalk or curb will be required to accommodate construction of a Pedestrian Curb Ramp.
7. The Planning and Design Section will coordinate this guide during the plan specification preparatory phase of the project. Plans and specifications for all concrete shall specify the location of required Pedestrian Curb Ramps for the handicapped.

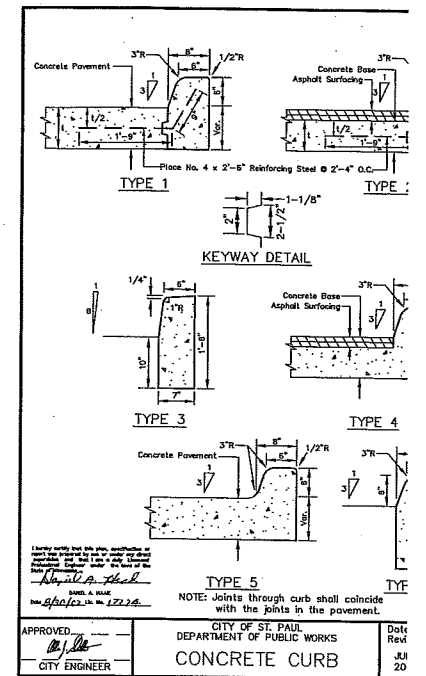


APPROVED: [Signature] CITY ENGINEER  
CITY OF ST. PAUL  
DEPARTMENT OF PUBLIC WORKS  
PEDESTRIAN CURB RAMP  
FOR THE HANDICAPPED  
Date of Revision: FEB 2007  
Standard Plate No. 3005D

**SNELLING  
MULTI-FAMILY**  
ST. PAUL, MN.

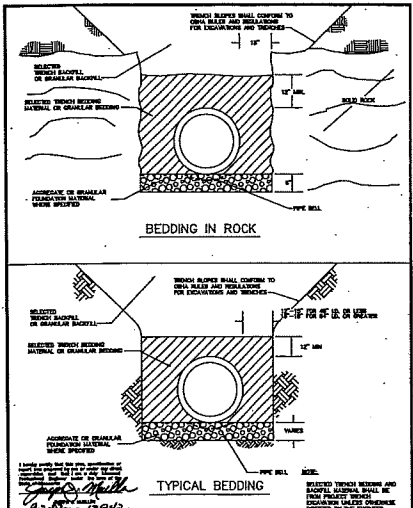
**LECESSE  
DEVELOPMENT**

**LOUCKS**  
PLANNING  
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LAND SURVEYING  
LANDSCAPE ARCHITECTURE  
ENVIRONMENTAL  
7200 Hemlock Lane, Suite 300  
Maple Grove, MN 55369  
763.424.5505  
www.loucksin.com

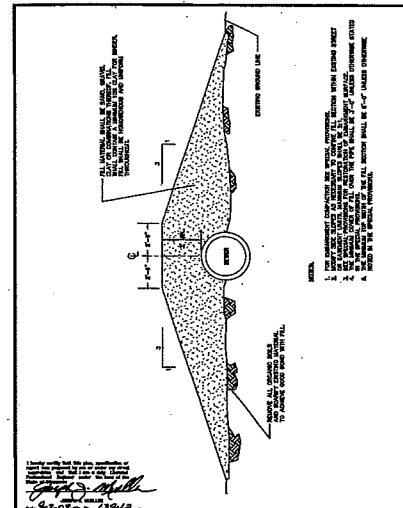


APPROVED: [Signature] CITY ENGINEER  
CITY OF ST. PAUL  
DEPARTMENT OF PUBLIC WORKS  
CONCRETE CURB  
Date of Revision: JUL 20

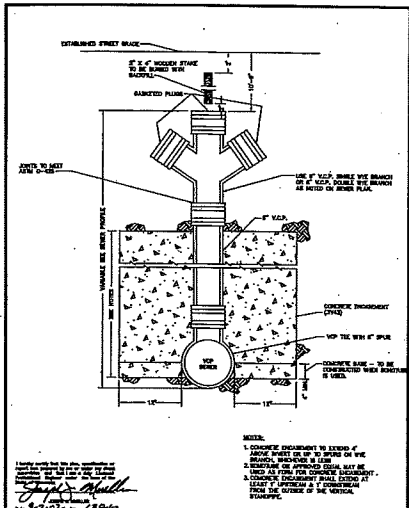
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APPROVED: [Signature] CITY ENGINEER  
CITY OF ST. PAUL  
DEPARTMENT OF PUBLIC WORKS  
CLASS 'D' PIPE BEDDING  
Date of Revision: JULY 2002  
Standard Plate No. 2317C



APPROVED: [Signature] CITY ENGINEER  
CITY OF ST. PAUL  
DEPARTMENT OF PUBLIC WORKS  
TYPICAL FILL SECTION  
Date of Revision: JULY 2002  
Standard Plate No. 2310C



APPROVED: [Signature] CITY ENGINEER  
CITY OF ST. PAUL  
DEPARTMENT OF PUBLIC WORKS  
STANDPIPE  
FOR V.C.P. SEWER  
Date of Revision: JULY 2002  
Standard Plate No. 2303B

**CADD QUALIFICATION**  
CADD files created by the Consultant for this project are instruments of the Consultant professional services for use solely with respect to this project. These CADD files shall not be used in other projects, for additions to this project, or for completion of this project by others without written approval by the Consultant. Upon the Consultant's approval, users may be permitted to obtain copies of the CADD drawings files for information and reference only. All mechanical or architectural modifications, additions, or deletions to these CADD files shall be made at the full risk of the user making such modifications, additions or deletions and that Loucks will not be responsible for any errors or omissions that may result from such modifications, additions or deletions from any CADD files created, modified, or deleted by the Consultant from any CADD files created, modified, or deleted by the Consultant.

**SUBMITTAL/REVISIONS**  
6/17/2016 Check Set  
2/23/2017 City Submittal

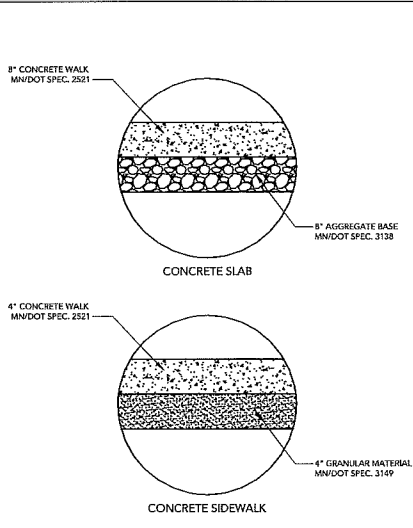
**PROFESSIONAL SIGNATURE**  
I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.  
Date: 4/5/16

**QUALITY CONTROL**  
Loucks Project No. 15597  
Project Lead NMM  
Drawn By JDK  
Checked By NMM  
Review Date

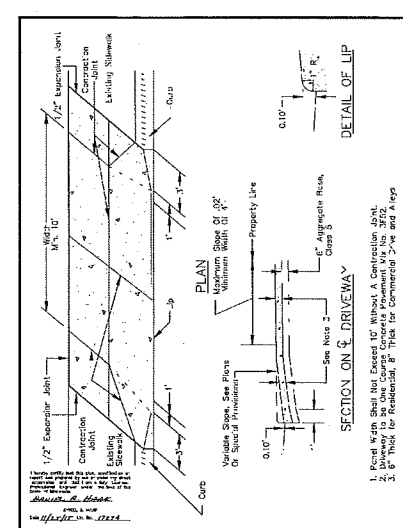
**SHEET INDEX**

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- C2-1 SITE PLAN
- C3-1 GRADING PLAN
- C3-2 SWPPP
- C3-3 SWPPP NOTES
- C4-1 UTILITY PLAN
- C8-1 DETAILS
- C8-2 DETAILS

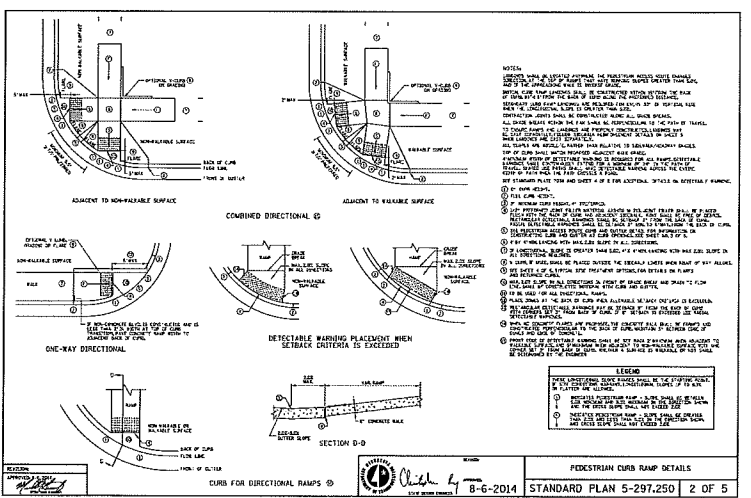
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400B



APPROVED: [Signature] CITY ENGINEER  
CITY OF ST. PAUL  
DEPARTMENT OF PUBLIC WORKS  
CONCRETE SIDEWALK /  
SLAB SECTIONS  
Date of Revision: MAY 2016  
Standard Plate No. 2036



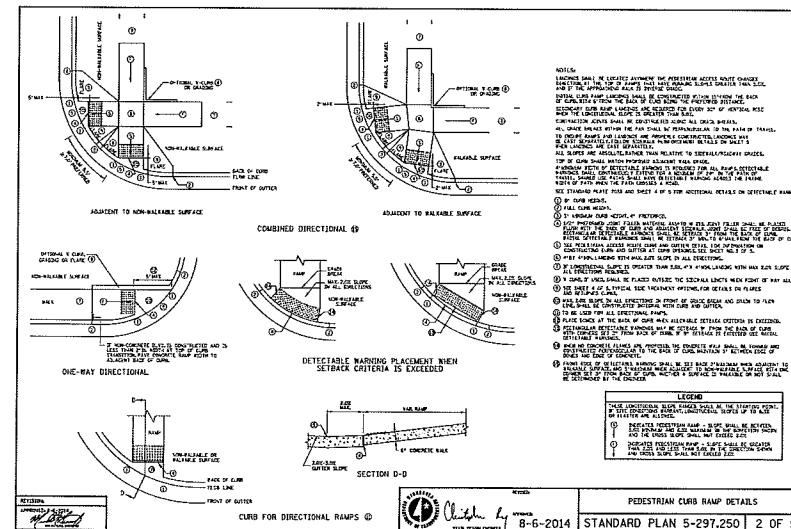
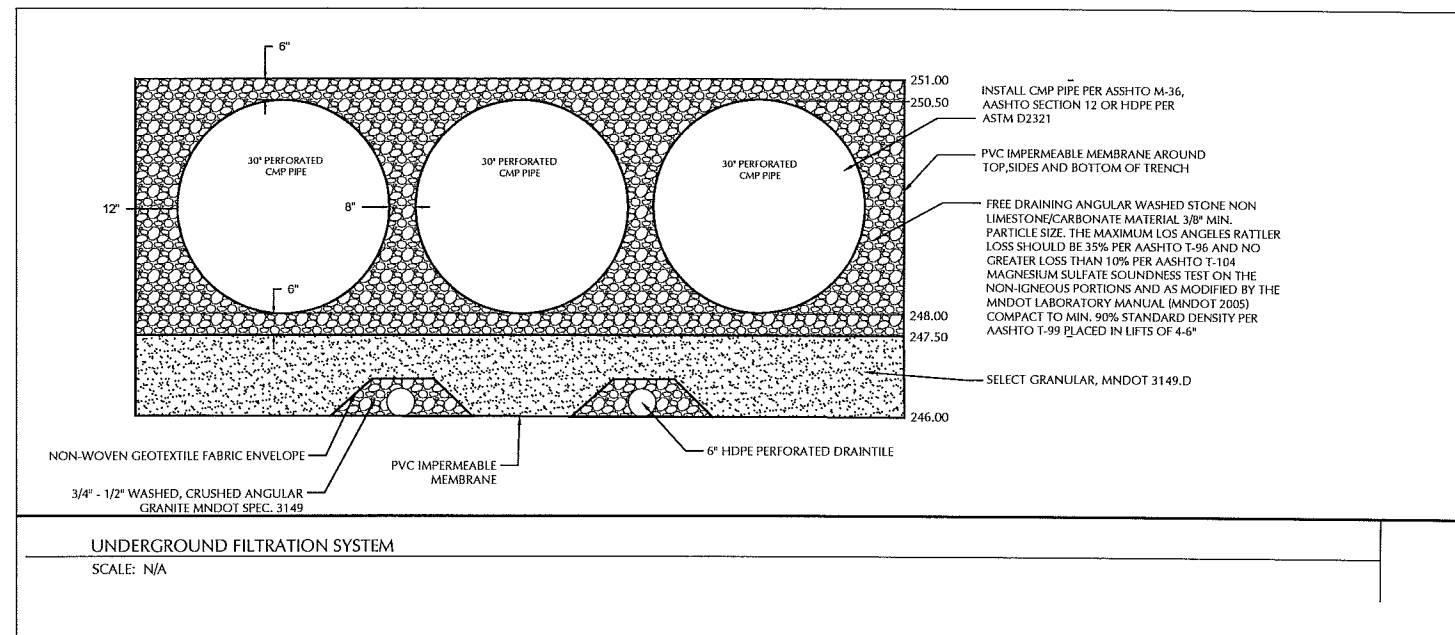
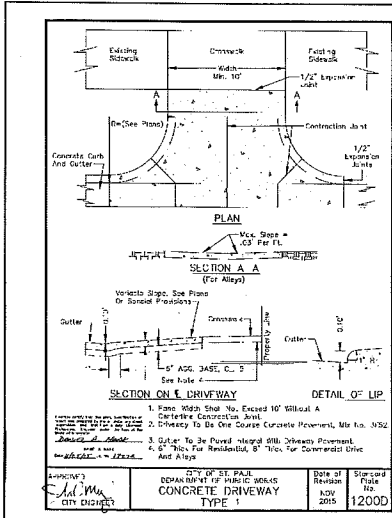
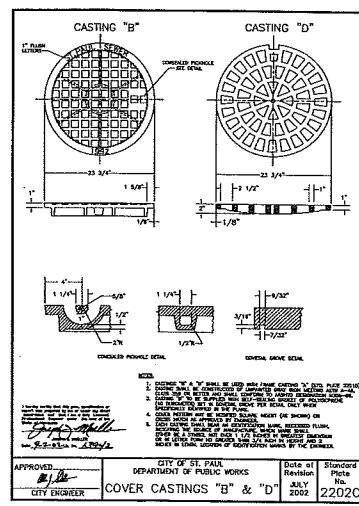
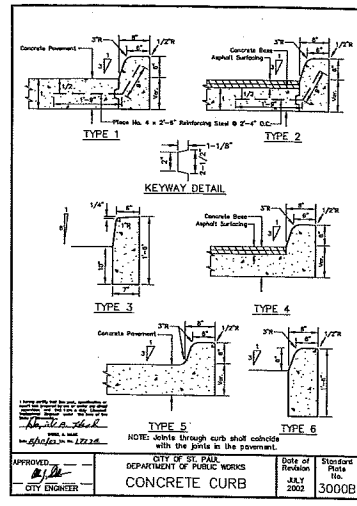
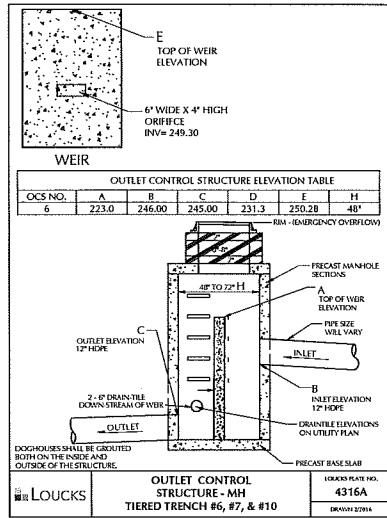
APPROVED: [Signature] CITY ENGINEER  
CITY OF ST. PAUL  
DEPARTMENT OF PUBLIC WORKS  
CONCRETE DRIVEWAY  
TYPE 7  
Date of Revision: MAY 2016  
Standard Plate No. 1207



APPROVED: [Signature] CITY ENGINEER  
CITY OF ST. PAUL  
DEPARTMENT OF PUBLIC WORKS  
PEDESTRIAN CURB RAMP DETAILS  
Date of Revision: 8-6-2014  
Standard Plan 5-297.250 2 OF 5

**DETAILS  
C8-1**

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Substituting for Invalid reference



**SNELLING  
MULTI-FAMILY**  
ST. PAUL, MN

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DEVELOPMENT**

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ENVIRONMENTAL  
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763.424.5505  
www.loucksinc.com

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**SUBMITTAL/REVISIONS**

|           |                |
|-----------|----------------|
| 6/17/2016 | Check Set      |
| 2/23/2017 | City Submittal |

**PROFESSIONAL SIGNATURE**  
I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.

**NOT FOR CONSTRUCTION**

Date: 4/8/2014

**QUALITY CONTROL**

|                    |       |
|--------------------|-------|
| Loucks Project No. | 15597 |
| Project Lead       | NMM   |
| Drawn By           | JDK   |
| Checked By         | NMM   |
| Review Date        |       |

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| C8-2 | DETAILS         |

**DETAILS  
C8-2**