

ZONING COMMITTEE STAFF REPORT

1. **FILE NAME:** CP Rail - Car Department Building **FILE #** 17-065-566
 2. **APPLICANT:** Canadian Pacific Railway **HEARING DATE:** August 17, 2017
 3. **TYPE OF APPLICATION:** Conditional Use Permit
 4. **LOCATION:** 1213 Pigs Eye Lake Road, NE of R/R roundhouse
 5. **PIN & LEGAL DESCRIPTION:** 042822410004, Section 4 Town 28 Range 22 Ex Area In Lease 82316 The Fol; Part Of Govt Lot 4 Sw Of C M St P & P Rr R/w & Ely Of St Paul Bridge & Terminal Rr R/w (subj To Rd Esmt) In Sec 4 Tn 28 Rn 22
 6. **PLANNING DISTRICT:** 1 **PRESENT ZONING:** I2
 7. **ZONING CODE REFERENCE:** §61.501, §72.73, §72.74
 8. **STAFF REPORT DATE:** August 6, 2017 **BY:** Josh Williams
 9. **DATE RECEIVED:** August 1, 2017 **60-DAY DEADLINE FOR ACTION:** September 30, 2017
-

- A. **PURPOSE:** Conditional use permit for construction of a building in the flood fringe (FF) elevated on an alternative to fill
- B. **PARCEL SIZE:** 69,000 sq. ft. (1.58 acres)
- C. **EXISTING LAND USE:** Railyard (I2)
- D. **SURROUNDING LAND USE:**
North, East, West, and South: Industrial and railroad (I2)
- E. **ZONING CODE CITATION:** §72.73 states that any structure in the FF flood fringe district not elevated on fill requires a conditional use permit; §72.74 lists standards for conditional uses in the FF flood fringe district; §61.501 lists general conditions that must be met by all conditional uses.
- F. **HISTORY/DISCUSSION:** Multiple conditional use permits have been granted for structures within the railyard, which is comprised of multiple parcels. The most recent CUP was for an office building in 2010.
- G. **PARKING:** The gross floor area (GFA) of the proposed building is approximately 1,700 square feet, and requires a minimum of five off-street parking spaces (the zoning code requires one off-street parking space per 400 sq. ft. GFA for office uses). The plans provided do not indicate how space in the attached garage will be allocated. Staff estimate that the garage would likely accommodate no more than two vehicles. However, the proposed building is part of a larger facility where ample off-street surface parking is available. Staff recommend that identification of parking on the site sufficient to meet the required minimum for the building be handled as part of the required site plan review, at the discretion of site plan review staff.
- H. **DISTRICT COUNCIL RECOMMENDATION:** As of the date of this staff report, the District 1 Council had not provided a recommendation.
- I. **FINDINGS:**
 1. The applicant proposes to construct a new office building for Car Department (rail car maintenance) operations. The new building will have approximately 1,700 finished square feet elevated to the Regulatory Flood Protection Elevation (RFPE, elevation of 708.8') and an approximately 795 square foot garage at grade (702.8'). The Base Flood Elevation (BFE, or 100-year flood) for the site has been calculated at 706.8'.
 2. The garage and exterior foundation walls will be constructed to the FP-3 or FP-4 floodproofing standards. The garage is designed to flood internally, and required automatic openings are provided to allow movement of water and equalization of hydrostatic pressure. The remainder of the building will be elevated on fill within the foundation walls.
 3. §72.74 lists standards for conditional uses in the FF flood fringe district. Subsections (a) through (d) are applicable to the proposed project:
 - (a) *Alternative elevation methods other than the use of fill may be utilized to elevate a structure's lowest floor above the regulatory flood protection elevation. These alternative methods may*

include the use of stilts, pilings, parallel walls or above grade, enclosed areas such as crawl spaces or tuck-under garages. The base or floor of an enclosed area shall be considered above grade and not a structure's basement or lowest floor if: 1) the enclosed area is above grade on at least one (1) side of the structure; 2) is designed to internally flood and is constructed with flood-resistant materials; and 3) is used solely for parking of vehicles, building access or storage. The above-noted alternative elevation methods are subject to the following additional standards:

- (1) Design and certification. The structure's design and as-built condition must be certified by a registered professional engineer or architect as being in compliance with the general design standards of the Minnesota State Building Code and, specifically, that all electrical, heating, ventilation, plumbing and air conditioning equipment and other service facilities must be at or above the regulatory flood protection elevation or be designed to prevent floodwater from entering or accumulating within these components during times of flooding.*
 - (2) Specific standards for above grade, enclosed areas. Above grade, fully enclosed areas such as crawl spaces or tuck-under garages must be designed to internally flood and the design plans must stipulate:
 - a. A minimum area of "automatic" openings in the walls where internal flooding is to be used as a floodproofing technique. There shall be a minimum of two (2) openings on at least two (2) sides of the structure and the bottom of all openings shall be no higher than one (1) foot above grade. The automatic openings shall have a minimum net area of not less than one (1) square inch for every square foot of enclosed area subject to flooding unless a registered professional engineer or architect certifies that a smaller net area would suffice. The automatic openings may be equipped with screens, louvers, valves or other coverings or devices, provided that they permit the automatic entry and exit of floodwaters without any form of intervention.*
 - b. That the enclosed area will be designed of flood-resistant materials in accordance with the FP-3 or FP-4 classifications in the Minnesota State Building Code and shall be used solely for building access, parking of vehicles or storage.**
- (b) Basements, as defined in §72.14, shall be subject to the following:*
- (1) Residential basement construction shall not be allowed below the regulatory flood protection elevation except as authorized in subsection (e) of this section.*
 - (2) Nonresidential basements may be allowed below the regulatory flood-protection elevation, provided the basement is protected in accordance with subsection (c) or (e) of this section.*
- (c) All areas of nonresidential structures including basements to be placed below the regulatory flood protection elevation shall be structurally dry floodproofed in accordance with the FP-1 or FP-2 floodproofing classifications in the Minnesota State Building Code. This shall require making the structure watertight, with the walls substantially impermeable to the passage of water and with structural components having the capability of resisting hydrostatic and hydrodynamic loads and the effects of buoyancy. Structures floodproofed to the FP-3 or FP-4 classification shall not be permitted.*
- (d) The storage or processing of materials that are, in times of flooding, flammable, explosive or potentially injurious to human, animal or plant life is prohibited. Storage of other materials or equipment may be allowed if readily removable from the area within the time available after a flood warning and in accordance with a plan approved by the planning commission, or if elevated above the regulatory flood protection elevation by alternative methods which meet the requirements of subsection (a) above. Storage of bulk materials may be allowed provided an erosion/sedimentation control plan is submitted which clearly specifies methods to be used to stabilize the materials on site for a regional flood event. The plan must be prepared and certified by a registered professional engineer or other qualified individual acceptable to the planning commission.*

(e) *When the Federal Emergency Management Agency has issued a letter of map revision-fill (LOMR-F) for vacant parcels of land elevated by fill to the one (1) percent chance flood elevation, the area elevated by fill remains subject to the provisions of this chapter. A structure may be placed on the area elevated by fill with the lowest floor below the regulatory flood protection elevation provided the structure meets the following provisions:*

- (1) *No floor level or portion of a structure that is below the regulatory flood protection elevation shall be used as habitable space or for storage of any property, materials, or equipment that might constitute a safety hazard when contacted by floodwaters. Habitable space shall be defined as any space in a structure used for living, sleeping, eating or cooking. Bathrooms, toilet compartments, closets, halls, storage rooms, laundry or utility space, and similar areas are not considered habitable space.*
- (2) *For residential and nonresidential structures, the basement floor may be placed below the regulatory flood protection elevation subject to the following standards:*
 - a. *The top of the immediate floor above any basement area shall be placed at or above the regulatory flood protection elevation.*
 - b. *Any area of the structure placed below the regulatory flood protection elevation shall meet the "reasonably safe from flooding" standards in the Federal Emergency Management Agency (FEMA) publication entitled "Ensuring that Structures Built on Fill In or Near Special Flood Hazard Areas Are Reasonably Safe From Flooding," Technical Bulletin 10-01, a copy of which is hereby adopted by reference and made part of this chapter. In accordance with the provisions of this chapter, and specifically section 72.33(g), the applicant shall submit documentation that the structure is designed and built in accordance with either the "Simplified Approach" or "Engineered Basement Option" found in FEMA Technical Bulletin 10-01.*
 - c. *If the ground surrounding the lowest adjacent grade to the structure is not at or above the regulatory flood protection elevation, then any portion of the structure that is below the regulatory flood protection elevation must be floodproofed consistent with any of the FP-1 through FP-4 floodproofing classifications found in the Minnesota State Building Code.*

These standards can be met. The applicant has proposed a building consistent with the requirements of this section. As a condition of approval, the applicant should provide building and foundation plans and record of as-built condition for the building signed by a registered professional engineer or architect and verifying consistency with the applicable requirements of §72.74 of the Saint Paul code, the Minnesota State Building Code, and FEMA Technical Bulletin 10-01. Storage may be allowed in the garage of the proposed building, provided all stored materials are removed in times of flooding. Review and acceptance by the Department of Safety and Inspections of an updated flood response plan for the CP Rail Pig's Eye yard that incorporates the proposed building should be a condition of approval.

4. §72.32 lists thirteen (13) factors to be considered in evaluating applications for conditional use permits in the FF flood fringe district:
 - (a) *The relationship of the proposed use to the comprehensive plan and floodplain management program for the city.* Subject to meeting the standards listed in §72.74, this proposed use is in compliance with the Saint Paul Comprehensive Plan and the City's floodplain management program. Policy 5.1.3 of the river corridor chapter of the comprehensive plan supports continuation of and additions to industrial uses in the Childs Road industrial area if said additions will not have significant adverse impacts on air or water quality nor impair river valley views. The proposed additions are to an existing facility located in a large industrial area, and will not significantly alter river valley views. The proposed building is replacing an existing building that will be demolished, and will not result in air or water quality impacts.
 - (b) *The importance of the services provided by the proposed facility to the community.* This finding is not applicable. The proposed building will be part of an existing facility.
 - (c) *The ability of the existing topography, soils, and geology to support and accommodate the*

proposed use. The proposed use is a new building within an existing railyard facility. The area is characterized by flat topography. While soils and geology of the area have long supported railyard operations and associated structures, the proposed building is to be constructed on helical piles to ensure sufficient foundational support.

- (d) *The compatibility of the proposed use with existing characteristics of biologic and other natural communities.* The proposed building is to be located in an existing railyard; the area is industrial in character, and does not contain significant biological communities. Impacts of the proposed building will not extend beyond the immediate area.
- (e) *The proposed water supply and sanitation systems and the ability of those to prevent disease, contamination, and unsanitary conditions.* The area is already served by adequate water supply and sanitation systems. The proposed building will replace an existing building, which is to be demolished, and will not create significant additional demand for water supply or sanitation capability.
- (f) *The requirements of the facility for a river-dependent location, if applicable.* The proposed building is part of an existing railyard facility that is located within the river corridor.
- (g) *The safety of access to the property for ordinary vehicles.* Safe access to the site is available via Childs Road and Shop Road.
- (h) *The susceptibility of the proposed facility and its contents to flood damage and the effect of such damage on the individual owner.* The proposed structure will be built to FP-3 or FP-4 wet floodproofing standards. In times of flooding, the building will be evacuated per the applicant's flood response plan. The applicant is self-insured.
- (i) *The dangers to life and property due to increased flood heights or velocities caused by encroachments.* The proposed encroachments are of limited footprint and located in the flood fringe where impacts on flood flows are negligible.
- (j) *The expected heights, velocity, duration, rate of rise, and sediment transport of the floodwaters expected at the site.* The proposed building is located in the flood fringe, where the velocity of flood flow is generally minimal.
- (k) *The danger that materials may be swept onto other lands or downstream to the injury of others.* The proposed building will be constructed of floodproof materials, and any items stored below the RFPE will be removed in times of flooding. The proposed building will also be located in the flood fringe, where velocity of flood flows is generally minimal.
- (l) *The availability of alternative locations or configurations for the proposed use.* The proposed building is part of an existing facility which is located within the flood fringe.
- (m) *Such other factors as are relevant to the purposes of this chapter.* The factors and findings enumerated and described herein adequately evaluate the proposed use for the purposes of this chapter.

5. §61.501 lists five standards that all conditional uses must satisfy:

- (a) *The extent, location and intensity of the use will be in substantial compliance with the Saint Paul Comprehensive Plan and any applicable subarea plans which were approved by the city council.* Subject to meeting the standards listed in §72.74, this proposed use is in compliance with the Saint Paul Comprehensive Plan and the City's floodplain management program. Policy 5.1.3 of the river corridor chapter of the comprehensive plan supports continuation of and additions to industrial uses in the Childs Road industrial area if said additions will not have significant adverse impacts on air or water quality nor impair river valley views. The proposed additions are to an existing facility located in a large industrial area, and will not significantly alter river valley views. The proposed building is replacing an existing building that will be demolished, and will not result in air or water quality impacts.
- (b) *The use will provide adequate ingress and egress to minimize traffic congestion in the public streets.* This condition is met. The proposed facility will be served by Childs and Shop Roads. The use is not expected to generate additional traffic.

- (c) *The use will not be detrimental to the existing character of the development in the immediate neighborhood or endanger the public health, safety and general welfare.* This condition is met. The proposed facility is consistent with the existing industrial character of the immediate neighborhood.
- (d) *The use will not impede the normal and orderly development and improvement of the surrounding property for uses permitted in the district.* This condition is met. The use is industrial in nature, and will not impeded improvement of surrounding properties for allowed uses.
- (e) *The use shall, in all other respects, conform to the applicable regulations of the district in which it is located.* This condition can be met. Subject to the acceptance by the Department of Safety and Inspections of a flood response plan for the proposed building and certification of plans by a registered engineer or architect, the use conforms to all applicable regulations of the I2 general industrial district, RC2 river corridor district, and the FF flood fringe district.

J. **STAFF RECOMMENDATION:** Based on the above findings, staff recommends approval of the conditional use permit for construction of a building in the (FF) flood fringe on an alternative to fill subject to the following additional condition(s):

1. Site plan approval. Final plans approved by the Zoning Administrator for this use shall be in substantial compliance with the plans submitted and approved as part of this application.
2. A flood response plan including the proposed building shall be accepted by the Department of Safety and Inspections, and the applicant shall conduct operations consistent with said plan.
3. At or prior to building permit review, the plans for the proposed structure must be certified by a registered engineer or architect as consistent with the applicable requirements of §72.74 of the Saint Paul code, the Minnesota State Building Code, and FEMA Technical Bulletin 10-01..
4. After construction, the applicant shall submit to the zoning administrator the required elevation certification certifying the as-built elevation of the proposed building, and as-built plans certified by a registered professional engineer or architect as consistent with the applicable requirements of §72.74 of the Saint Paul code, the Minnesota State Building Code, and FEMA Technical Bulletin 10-01.



CONDITIONAL USE PERMIT APPLICATION

Department of Planning and Economic Development
Zoning Section
1400 City Hall Annex
25 West Fourth Street
Saint Paul, MN 55102-1634
(651) 266-6589

Zoning office use only	
File #	17-065566
Case #	
Preliminary Hearing Date	8-17-17

PD=1

042822410004

APPLICANT

Name	Patrick Mooney		
Address	126 South 6th Street #900		
City	Minneapolis	st.	MN Zip 55402
Daytime Phone	612-904-5996		
Name of Owner (if different)	Canadian Pacific Railway		
Contact Person (if different)		Phone	

PROPERTY LOCATION

Address / Location	1000 Shop Road		
Legal Description			
Current Zoning	Industrial I2		
(attach additional sheet if necessary)			

TYPE OF PERMIT:

Application is hereby made for a Conditional Use Permit under provisions of Chapter 72, Section 74, Paragraph a of the Zoning Code.

SUPPORTING INFORMATION: Explain how the use will meet all of the applicable standards and conditions. If you are requesting modification of any special conditions or standards for a conditional use, explain why the modification is needed and how it meets the requirements for modification of special conditions in Section 61.502 of the Zoning Code. Attach additional sheets if necessary.

The building structure will be elevated on piles with surrounding foundation wall at perimeter base. Finished floor elevation will be 2' above FEMA base flood elevation with encroachment. The Structural Engineer is working on calculation to verify resistance against uplift and hydro pressures.

Required site plan is attached

Applicant's Signature

[Handwritten Signature]

Date

7/12/17

City Agent

[Handwritten Signature]

7-12-17



City of Saint Paul Department of Safety & Inspections, 375 Jackson Street, Suite 220, Saint Paul MN 55101

SITE PLAN REVIEW APPLICATION

Date Application Received:

Staff Use Only
SPR File #
Application Fee \$
Staff Meeting Date:
City Agent:

Project Name: Canadian Pacific Railway Car Department Building	
Site Address: 1213 Pigs Eye Lake Road	Property Identification Number: 042822410004
Project Description: Construction of a new stand alone building for yard personnel.	

Provide (5) five Paper Copies 11x17 and an electronic PDF version (11x17 print format) of the complete Site Plan package including **certificate of survey, civil site plan, exterior architectural plan, and landscape plan.**

Project Summary

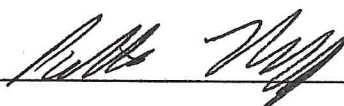
Est. Project Cost: \$ <i>(exclusive of land value)</i>	TBD	Est. Construction Start	August 2017	Proposed Land Use:
Parcel Area [sq. ft.]	744,440.4 sf	Disturbed Area [sq. ft.]	9,500 sf	<input type="checkbox"/> Residential <input type="checkbox"/> Institutional <input type="checkbox"/> Parking <input type="checkbox"/> Commercial <input checked="" type="checkbox"/> Industrial Only <input type="checkbox"/> Mixed-Use <input type="checkbox"/> Other
Floor Area Ratio	.0045	Building Gross Floor Area	3,360 sf	# Off-Street Parking Spaces
<input type="checkbox"/> Historic District/Property		<input checked="" type="checkbox"/> Flood Plain Property		<input type="checkbox"/> Steep Slope (>12%)

Residential Project Details

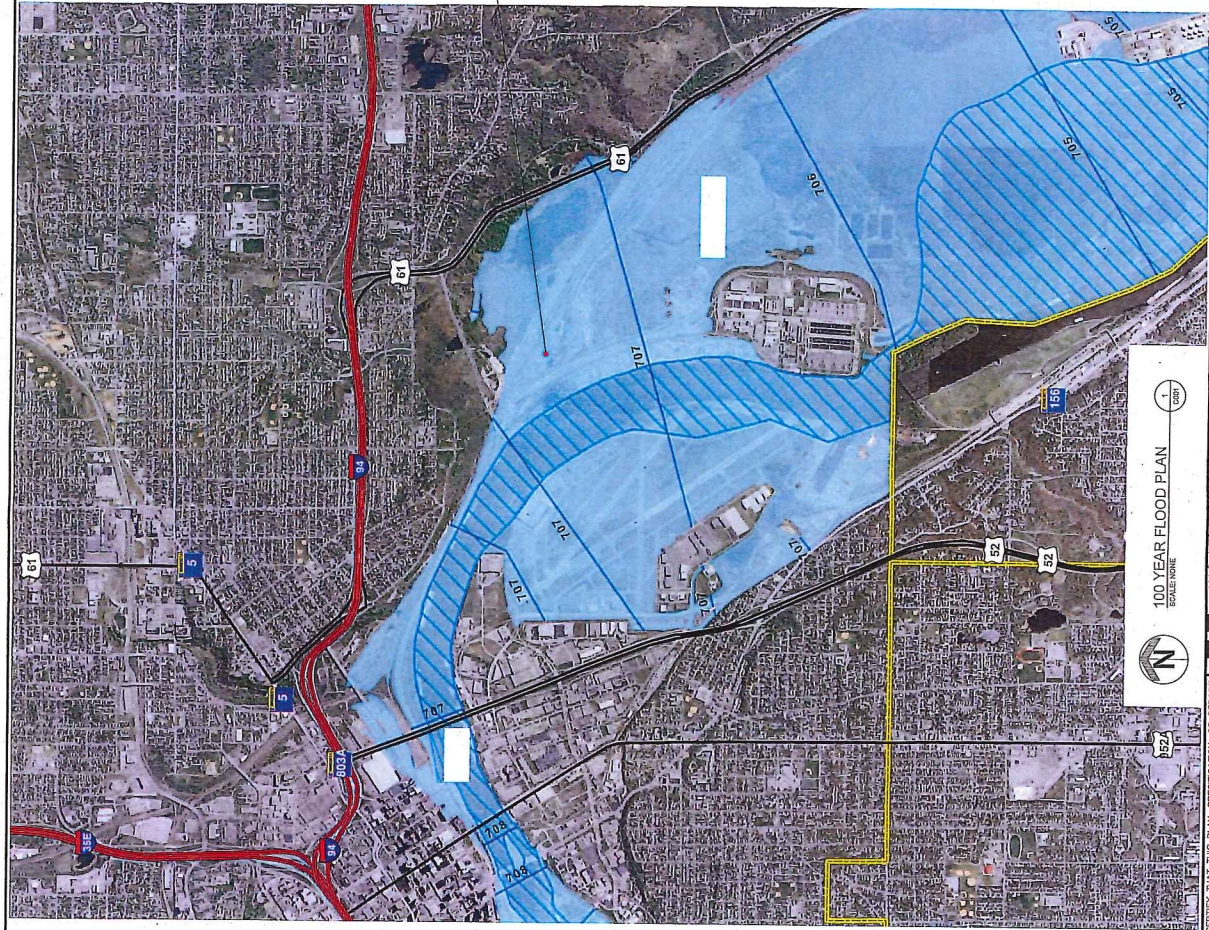
# Residential Units	NA	# Affordable	NA	% AMI for Affordable	NA
---------------------	----	--------------	----	----------------------	----

Applicant Information [Name, company, address, phone, e-mail]

Developer or Property Owner	Project Contact [PM, architect]	Construction Contact
Pat Mooney Canadian Pacific Railway 120 South 6th Street #900 Minneapolis, MN 55402	Brian Gadiant Momentum Design Group 765 North Hampden Ave #180 St. Paul, MN 55114	Mike Johnson MP Johnson Construction, Inc. 50 South 6th Street #1413 Minneapolis, MN 55402

Signature  952 583 9788 Date 7/27/17

Staff Use Only			
Zoning District	Overlay Zoning District	District Council	
Ward	Watershed District	MnDOT or County	
<input type="checkbox"/> Parkland Dedication	<input type="checkbox"/> TDMP	<input type="checkbox"/> CUP Required	Previous SPR



NOTE:
 THIS PLAN IS A DESIGN DEVELOPMENT PLAN FOR THE
 EMERGENCY MANAGEMENT AGENCY (EMMA) TO BE USED IN
 RESPONSE TO A FLOOD EVENT IN MINNESOTA. IT IS NOT A
 FLOOD HAZARD MAP OR A FLOOD INSURANCE RISK ASSESSMENT.

DESIGNER		TKDA
DRAWN		TKDA
CHECKED		TKDA
NO.	DATE	DESCRIPTION OF REVISIONS
B.	7/27/17	MANUSCRIPT REVIEW
A.	8/21/17	MANUSCRIPT REVIEW

444 Cedar Street, Suite 1500
 Minneapolis, MN 55401
 612.282.4400
 tkda.com

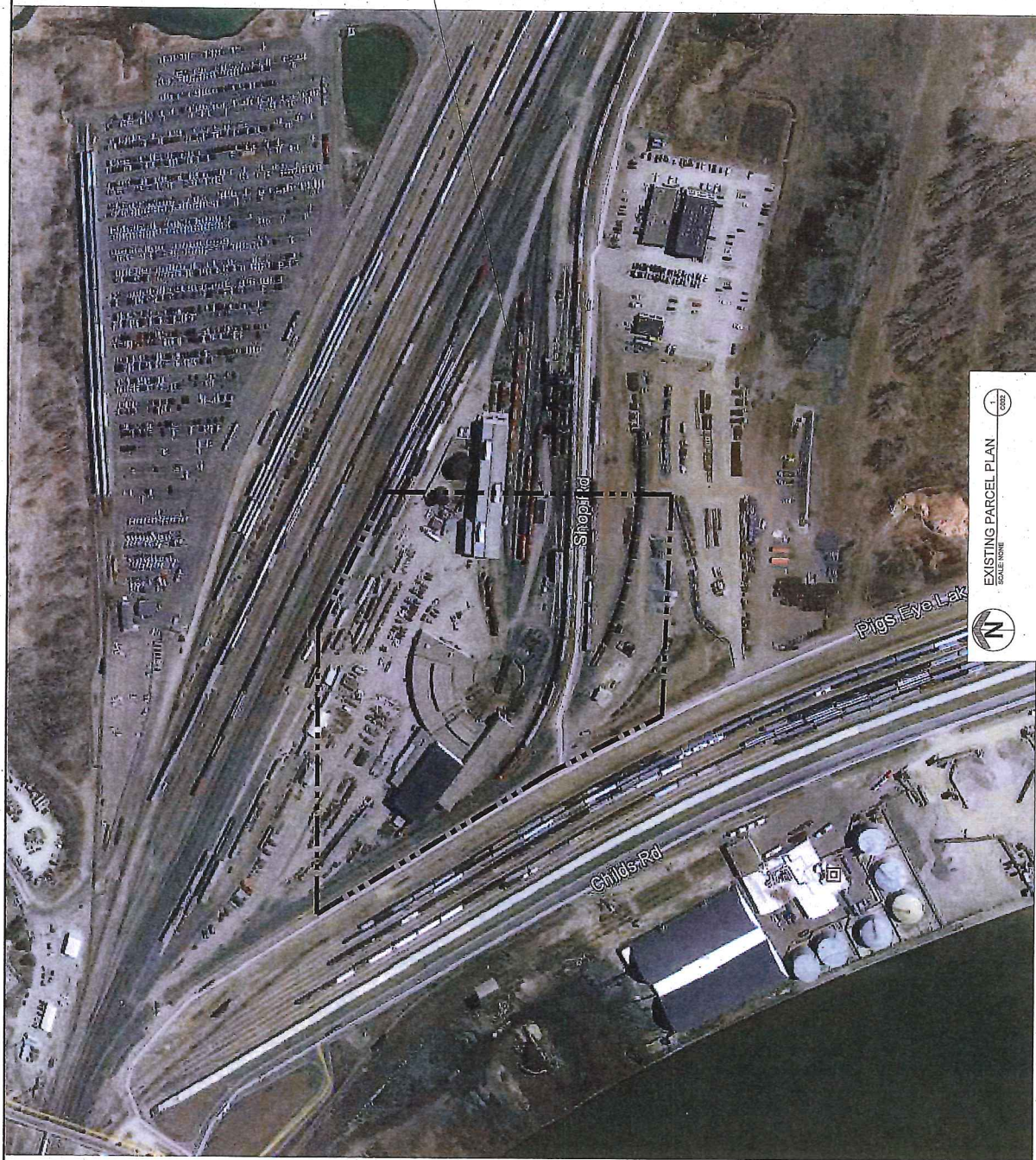
ST. PAUL
 YARD
 CANADIAN PACIFIC
 RAILWAY

100 YEAR FLOOD PLAN

CSMA NO. 15265.000
 DRAWING NO. C001

DESIGNED BY: [REDACTED]
 DRAWN BY: [REDACTED]
 CHECKED BY: [REDACTED]
 I HEREBY CERTIFY THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE AUTHORITY OF THE STATE OF MINNESOTA.
 SIGNATURE: [REDACTED] DATE: 7-27-17
 PRINTED NAME: ANDREW J. MAGSTROM LIC. NO. 52208

PLOT DATE: 04/27/2017 12:08pm
 FILENAME: K:\VFP\hwy\15265000\04_Production\01_CAD\02_Sheet\C001.dwg



PARCEL ID # 04822410004
1213 PIGS EYE LAKE ROAD



EXISTING PARCEL PLAN
SCALE: NONE

CONTRACT NO. 12685.000
DATE: 08-21-17
DRAWING NO. C002

EXISTING PARCEL PLAN

CANADIAN PACIFIC
RAILWAY

ST. PAUL
YARD

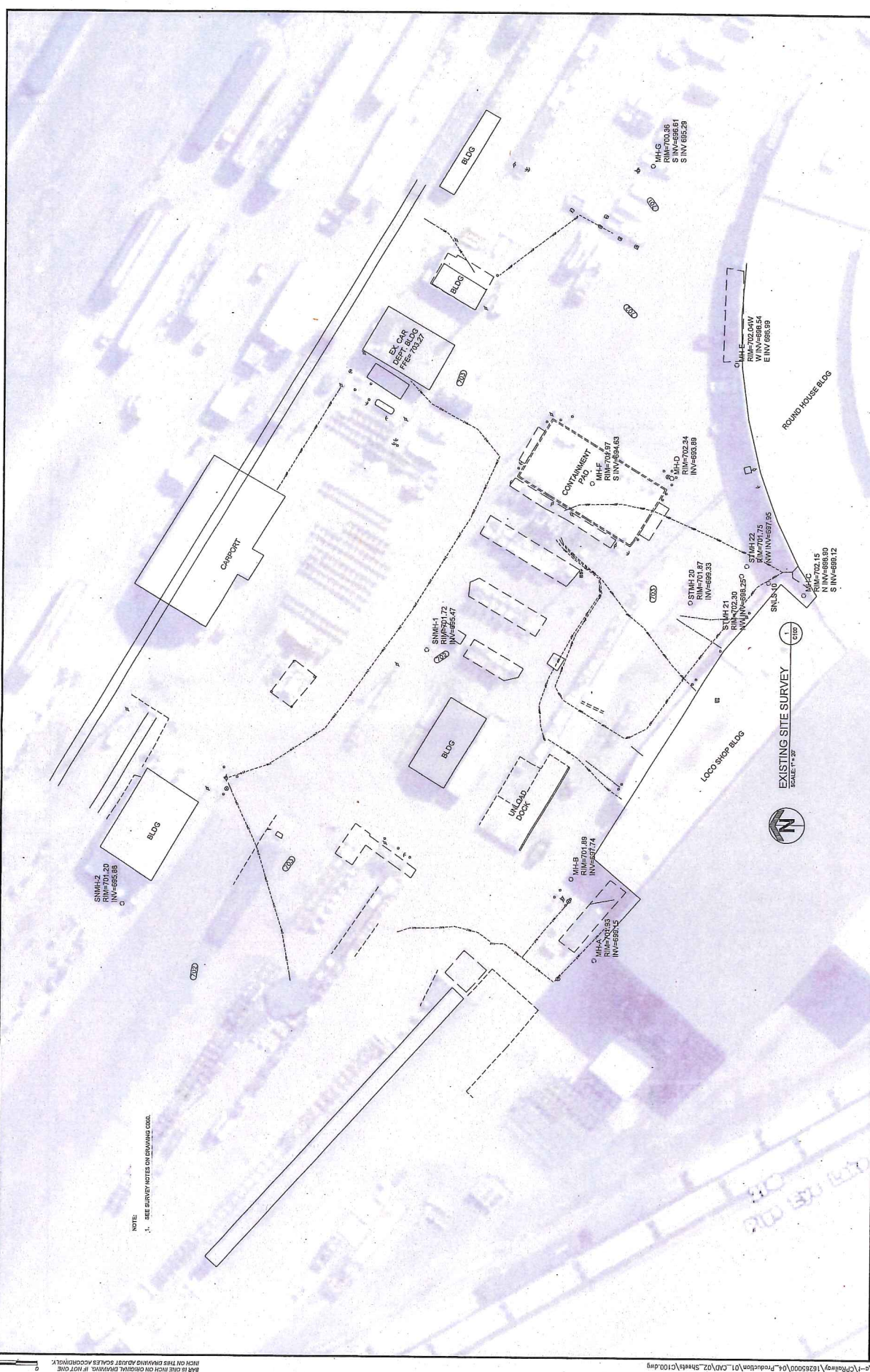
444 Cedar Street, Suite 1500
St. Paul, MN 55101
851.292.4400
hdb.com



DESIGNED BY: [Redacted]
DRAWN BY: [Redacted]
CHECKED BY: [Redacted]

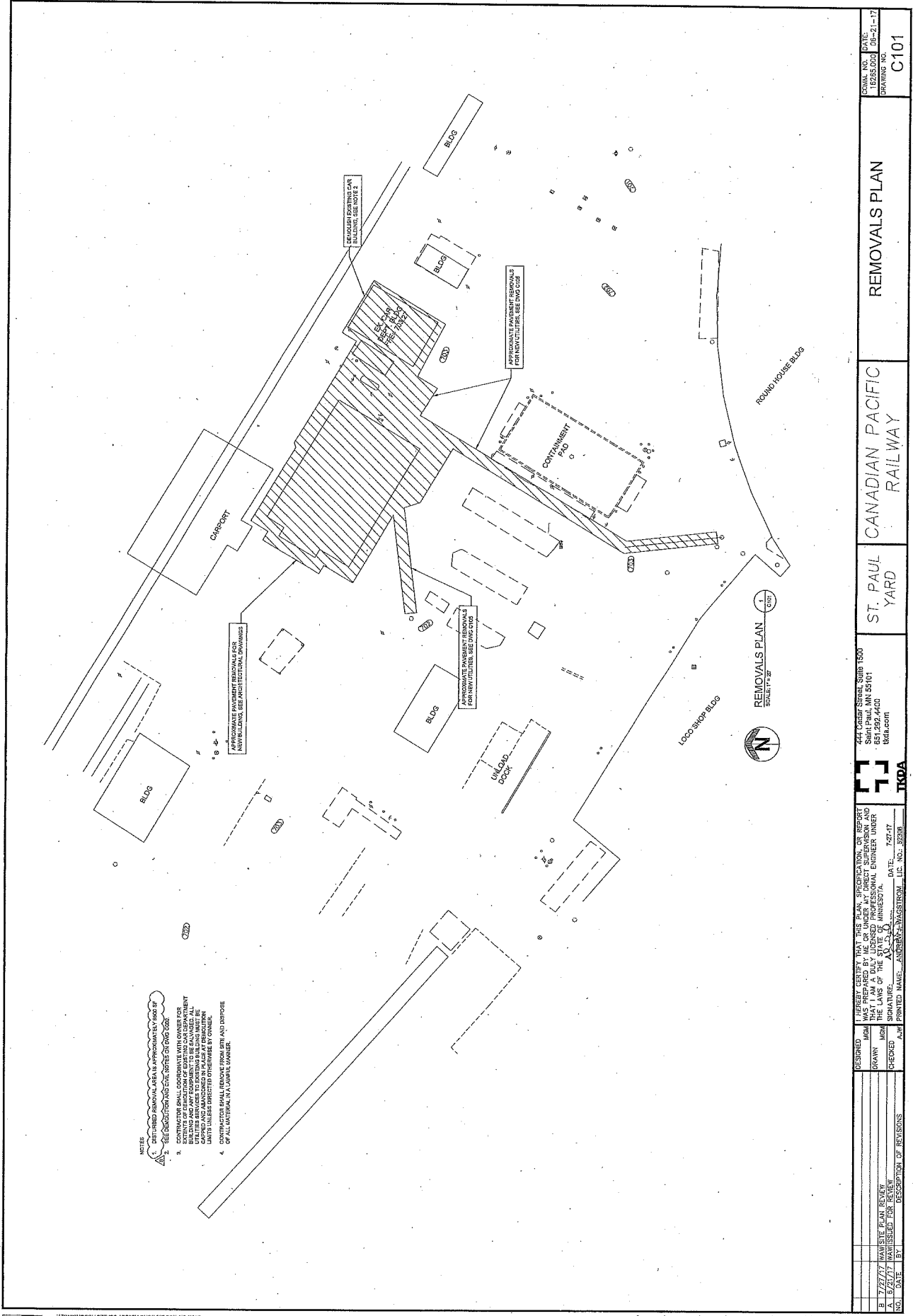
THIS REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION, OR REPORT THAT I AM A FULLY LICENSED PROFESSIONAL ENGINEER UNDER THE STATUTES OF THE STATE OF MINNESOTA. DATE: 7-29-17
PRINTED NAME: ANDREW J. MAGSTROM LIC. NO.: 82808

NO.	DATE	BY	DESCRIPTION OF REVISIONS



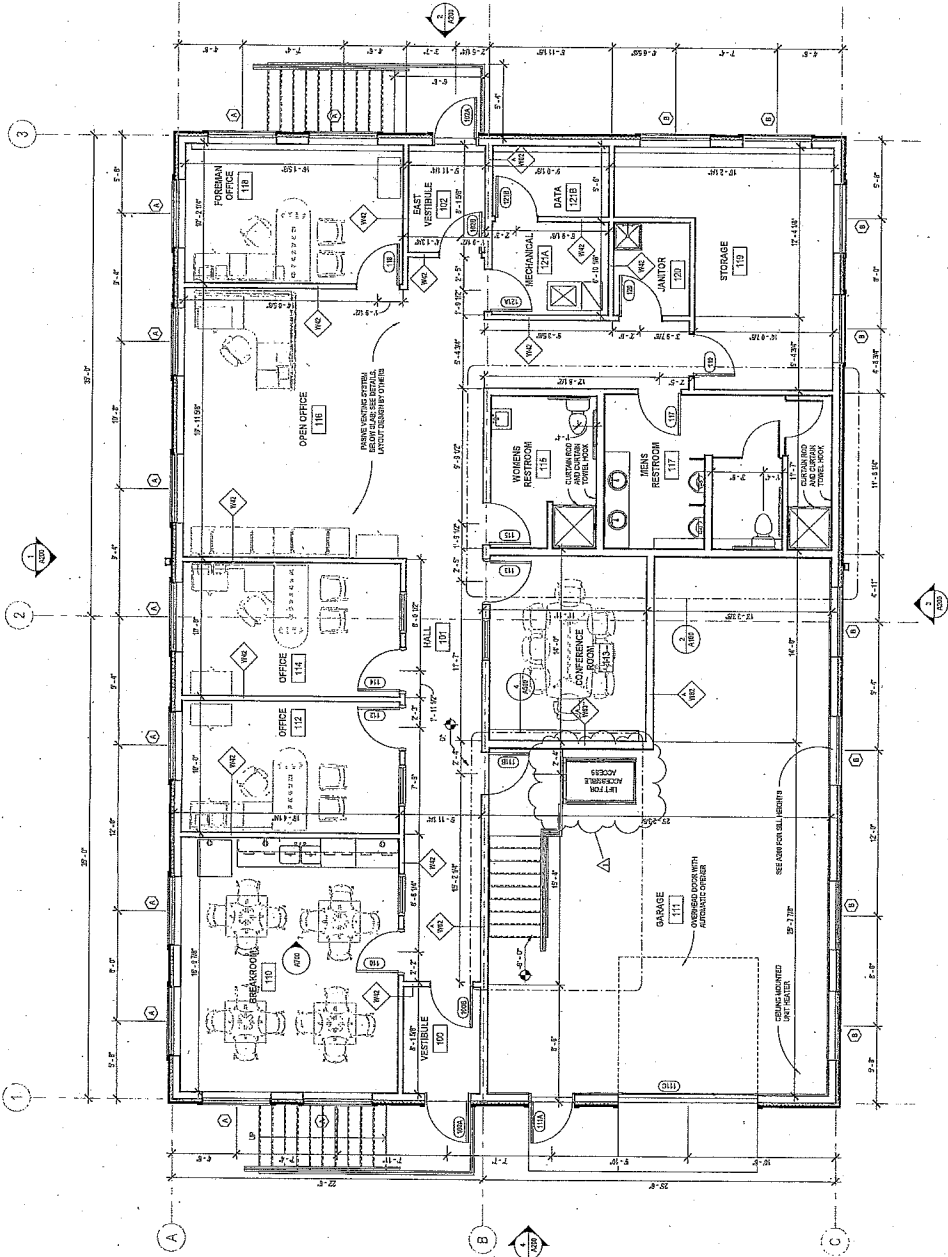
NOTE:
1. SEE SURVEY NOTES ON DRAWING COG.

DESIGNER: [Blank] DRAWN: [Blank] CHECKED: [Blank]		I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY CLOSE PERSONAL SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA. DATE: 3.27.17 PRINTED NAME: ANDREW J. WASTROM, LIC. NO.: 62306		444 Cedar Street, Suite 1500 Minneapolis, MN 55101 612.338.4400 tbd.com		TKDA		ST. PAUL YARD		CANADIAN PACIFIC RAILWAY		EXISTING SITE SURVEY		CSMA NO. 16265.009 DATE: 08-21-17 DRAWING NO. C100	
NO.	DATE	BY	DESCRIPTION OF REVISIONS												
1	3/27/17	AWASTROM	ISSUED FOR REVIEW												
2	3/27/17	AWASTROM	ISSUED FOR REVIEW												

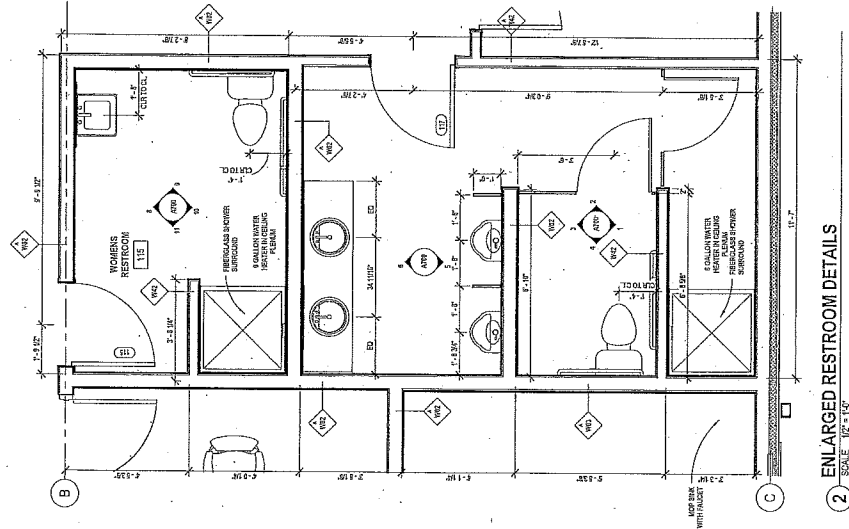


- NOTES
1. SEE DETAIL DRAWINGS FOR REMOVALS.
 2. EXTENTS OF REMOVALS OF EXISTING CONCRETE SHALL BE DETERMINED BY THE CONTRACTOR. ALL EXISTING CONCRETE SHALL BE REMOVED TO THE UNFINISHED FLOOR SLAB OR TO THE UNFINISHED CEILING SLAB, WHICHEVER IS DEEPER. ALL EXISTING CONCRETE SHALL BE REMOVED TO THE UNFINISHED FLOOR SLAB OR TO THE UNFINISHED CEILING SLAB, WHICHEVER IS DEEPER.
 3. CONTRACTOR SHALL REMOVE FROM SITE AND DISPOSE OF ALL MATERIAL IN A LAWFUL MANNER.
 4. CONTRACTOR SHALL REMOVE FROM SITE AND DISPOSE OF ALL MATERIAL IN A LAWFUL MANNER.

PROJECT #	5539	DATE	02/22/17	DRAWN	MM/2017
PROJECT NAME	ST. PAUL YARD				
PROJECT NUMBER	15014				
PROJECT SHEET DATE	02/22/17				
PROJECT ARCHITECT	MCDERMOTT DESIGN GROUP				
DATE	02/22/17				



1 FIRST FLOOR PLAN
 SCALE: 1/8" = 1'-0"

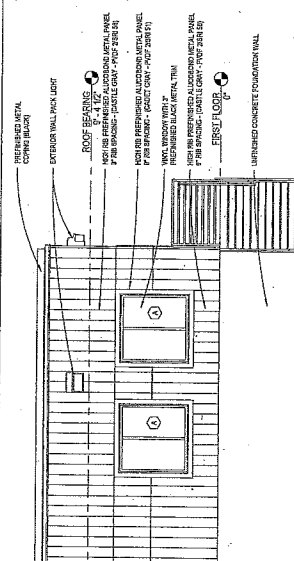


2 ENLARGED RESTROOM DETAILS
 SCALE: 1/2" = 1'-0"

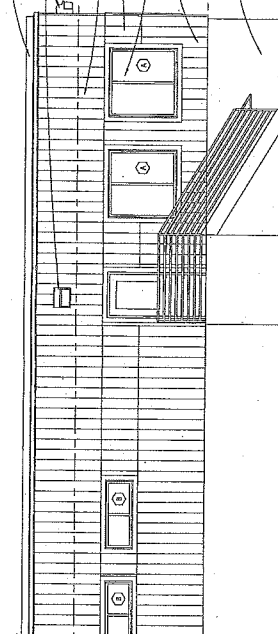
- FINISH SCHEDULE:**
1. FLOOR FINISH: POLISHED CONCRETE
 2. WALL FINISH: 1/2" GYPSUM BOARD WITH MEDIUM GRADE PAPER
 3. CEILING FINISH: 2' X 4' ACoustical TILE
 4. DOOR FINISH: 1 1/2" SOLID CORE

- NOTES:**
1. ALL FINISHES TO BE IN ACCORDANCE WITH THE FINISH SCHEDULE.
 2. ALL FINISHES TO BE IN ACCORDANCE WITH THE FINISH SCHEDULE.
 3. ALL FINISHES TO BE IN ACCORDANCE WITH THE FINISH SCHEDULE.
 4. ALL FINISHES TO BE IN ACCORDANCE WITH THE FINISH SCHEDULE.
 5. ALL FINISHES TO BE IN ACCORDANCE WITH THE FINISH SCHEDULE.
 6. ALL FINISHES TO BE IN ACCORDANCE WITH THE FINISH SCHEDULE.
 7. ALL FINISHES TO BE IN ACCORDANCE WITH THE FINISH SCHEDULE.
 8. ALL FINISHES TO BE IN ACCORDANCE WITH THE FINISH SCHEDULE.
 9. ALL FINISHES TO BE IN ACCORDANCE WITH THE FINISH SCHEDULE.
 10. ALL FINISHES TO BE IN ACCORDANCE WITH THE FINISH SCHEDULE.
 11. ALL FINISHES TO BE IN ACCORDANCE WITH THE FINISH SCHEDULE.

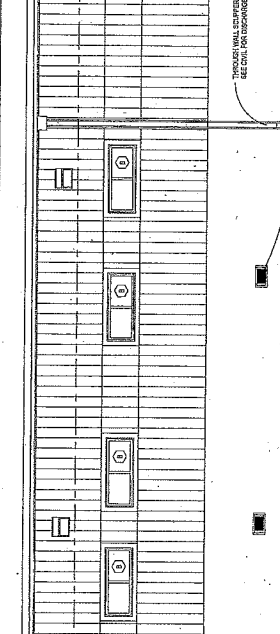
- GENERAL NOTES:**
1. ALL WORK SHALL BE IN ACCORDANCE WITH THE CANADIAN PACIFIC STANDARD SPECIFICATIONS FOR CONSTRUCTION.
 2. ALL WORK SHALL BE IN ACCORDANCE WITH THE CANADIAN PACIFIC STANDARD SPECIFICATIONS FOR CONSTRUCTION.
 3. ALL WORK SHALL BE IN ACCORDANCE WITH THE CANADIAN PACIFIC STANDARD SPECIFICATIONS FOR CONSTRUCTION.
 4. ALL WORK SHALL BE IN ACCORDANCE WITH THE CANADIAN PACIFIC STANDARD SPECIFICATIONS FOR CONSTRUCTION.
 5. ALL WORK SHALL BE IN ACCORDANCE WITH THE CANADIAN PACIFIC STANDARD SPECIFICATIONS FOR CONSTRUCTION.
 6. ALL WORK SHALL BE IN ACCORDANCE WITH THE CANADIAN PACIFIC STANDARD SPECIFICATIONS FOR CONSTRUCTION.
 7. ALL WORK SHALL BE IN ACCORDANCE WITH THE CANADIAN PACIFIC STANDARD SPECIFICATIONS FOR CONSTRUCTION.
 8. ALL WORK SHALL BE IN ACCORDANCE WITH THE CANADIAN PACIFIC STANDARD SPECIFICATIONS FOR CONSTRUCTION.
 9. ALL WORK SHALL BE IN ACCORDANCE WITH THE CANADIAN PACIFIC STANDARD SPECIFICATIONS FOR CONSTRUCTION.
 10. ALL WORK SHALL BE IN ACCORDANCE WITH THE CANADIAN PACIFIC STANDARD SPECIFICATIONS FOR CONSTRUCTION.
 11. ALL WORK SHALL BE IN ACCORDANCE WITH THE CANADIAN PACIFIC STANDARD SPECIFICATIONS FOR CONSTRUCTION.



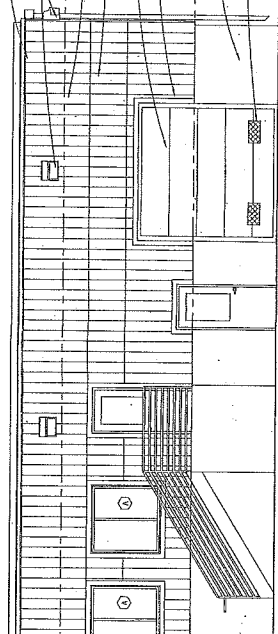
1 NORTH ELEVATION
 SCALE: 1/8" = 1'-0"



2 EAST ELEVATION
 SCALE: 1/8" = 1'-0"

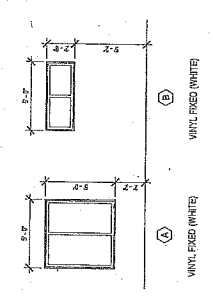


3 SOUTH ELEVATIONS
 SCALE: 1/8" = 1'-0"

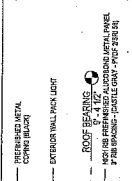


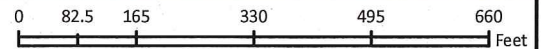
4 WEST ELEVATION
 SCALE: 1/8" = 1'-0"

EXTERIOR GENERAL NOTES:
 1. ALL WORK TO BE DONE IN ACCORDANCE WITH THE 2015 CANADIAN NATIONAL BUILDING CODE.
 2. ALL CONCRETE SHALL BE 3000 PSI STRENGTH AND CLEAR CONCRETE SHALL BE USED.
 3. ALL TRIM AT PERIMETER OF WINDOWS SHALL BE CALLED.
 4. ALL WINDOW UNITS TO HAVE CONCRETE AND MASONRY WATER RESISTANT.



5 WINDOW TYPES
 SCALE: 1/8" = 1'-0"






FILE NAME: CP Rail-Car Department Building

Aerial

APPLICATION TYPE: CUP

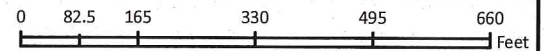
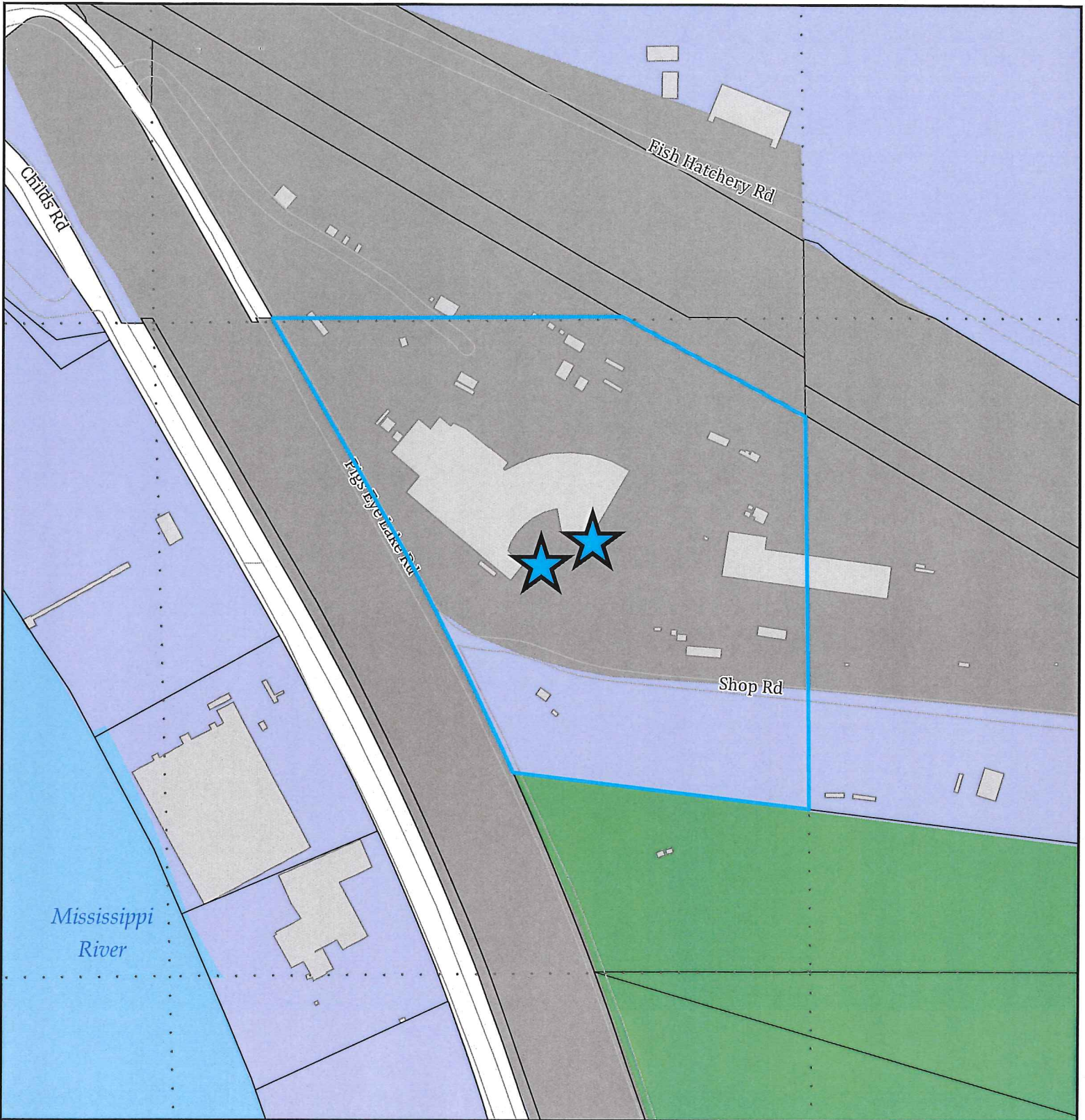
 Subject Parcels

FILE #: 17-065566 DATE: 8/1/2017

PLANNING DISTRICT: 1

ZONING PANEL: 17





FILE NAME: CP Rail-Car Department Building

APPLICATION TYPE: CUP

FILE #: 17-065566 DATE: 8/1/2017

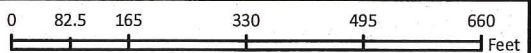
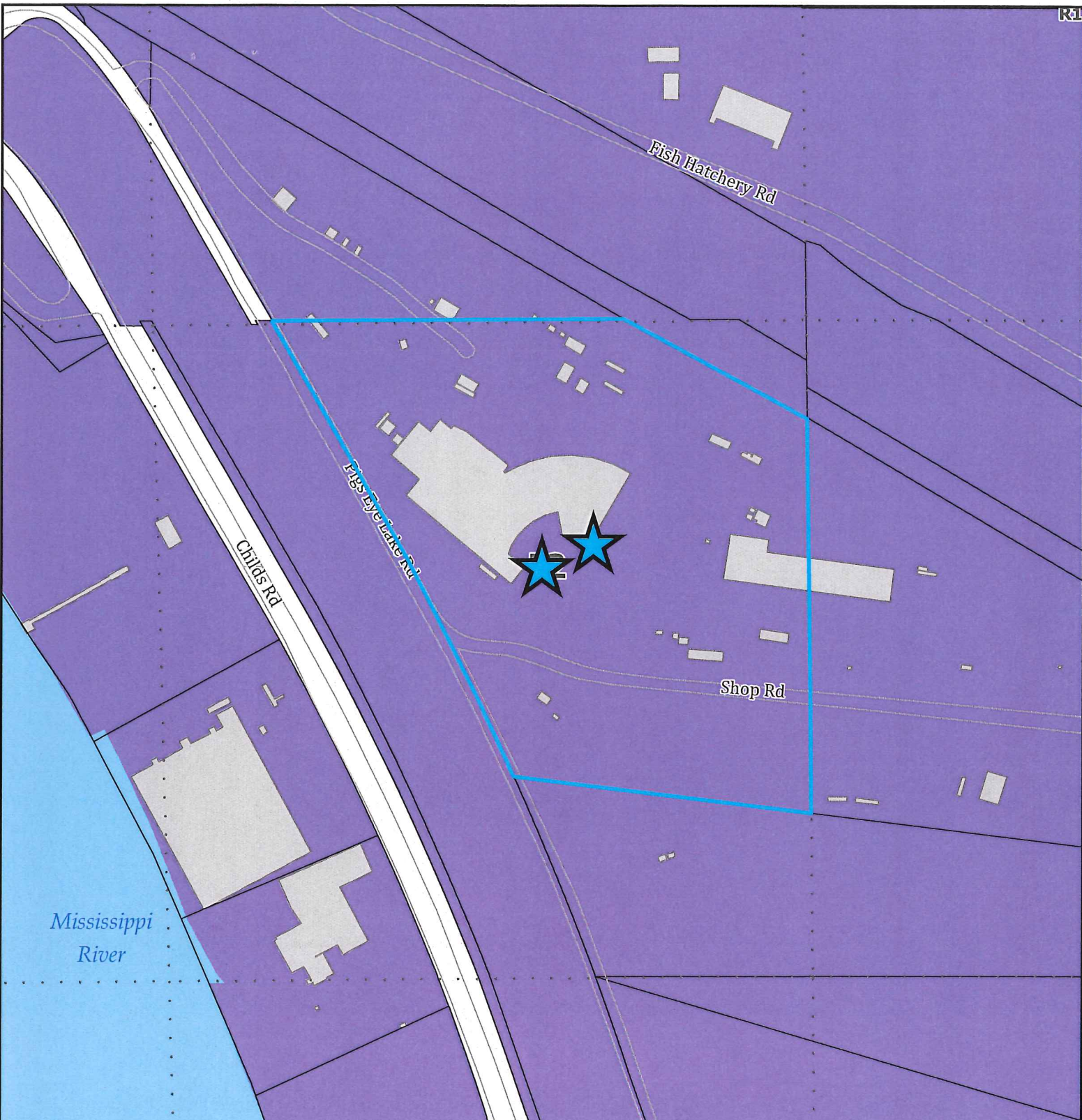
PLANNING DISTRICT: 1

ZONING PANEL: 17

Land Use

- Industrial and Utility
- Park, Recreational or Preserve
- Railway
- Water
- Subject Parcels
- Section Lines









FILE NAME: CP Rail-Car Department Building

APPLICATION TYPE: CUP

FILE #: 17-065566 DATE: 8/1/2017

PLANNING DISTRICT: 1

ZONING PANEL: 17

- Zoning**
-  Subject Parcels
 -  Section Lines
 -  R1 One-Family
 -  I2 General Industrial

