



CITY OF SAINT PAUL
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Date: July 18, 2019
To: Saint Paul Planning Commission
From: Comprehensive and Neighborhood Planning Committee (CNPC)
Subject: Design Standards Amendment to the *Ford Site Zoning and Public Realm Master Plan*
– Recommendations Based on Public Testimony and Staff Comment

BACKGROUND

In 2017, the City Council directed the creation of design standards when it adopted the *Ford Site Zoning and Public Realm Master Plan (Master Plan)* (Ord 17-40). Design standards support the development of a walkable, pedestrian and bike-friendly built environment, and encourage a sense of place. They are intended to address things like how the faces of buildings meet the street, what exterior building materials should be used, and landscaping. The standards will be added to the *Master Plan*, with Zoning Code amendments necessary for consistency.

Ryan Companies (Ryan) was awarded development rights to the Ford Site as the master developer, and requested amendments to the *Master Plan*. After recommendations from the Planning Commission, a version of those amendments was adopted by the City Council in April of 2019. While most of those changes were to existing *Master Plan* content, the design standards being considered now would be new material.

PROCESS

A consultant team led by LHB, Inc. and supported by PlaceMakers and Forecast Public Art began working on the design standards in late fall of 2018. An inter-departmental City staff working group was created and met regularly to advise the consultant throughout the process. A focus group comprised of varied professionals and neighborhood representatives familiar with the area around the Ford Site was also assembled. The focus group met twice – once early in the process to provide guidance on overall approach and an outline of the standards, and once towards the end of the process to react to draft materials. Ryan was invited to, and participated in, both the working and focus groups.

City staff and the consultant presented at the Highland District Council's January 15, 2019 Community Development Committee meeting to provide a briefing on scope and schedule, and to answer questions. City staff hosted a public open house on April 23, 2019 at Gloria Dei Lutheran Church to update interested parties on the work, provide draft materials, and listen to feedback.

As an amendment to the *Master Plan* and per Sec 66.951 of the Zoning Code, the draft *Master Plan* and zoning text amendments are reviewed by the Planning Commission and City Council. A public hearing was held at the Planning Commission on June 28, 2019. One person spoke at the public hearing and submitted written comments. In addition to that, written comments were received from two individuals and a resolution was submitted by the Community Development Committee of the Highland District Council.

SUMMARY OF PUBLIC COMMENT

The majority of the comments were neither in support nor opposition to the draft, but consisted of suggestions or concerns that people felt the Commission should consider. There were several specific comments that were beyond the scope of the design standards and are not addressed in the recommended changes. All public comments are attached. The main points made by the public were as follows:

- The human scale and relationship between public and private space is important (fence/wall height, setback specificity, floor elevations)
- Proper function of the ROW is important (snow storage, pedestrian safety)
- Access to private development is important (safety at vehicular ingress/egress)
- Consider expanding material choices for both buildings and pavement
- Beware of inconsistencies between draft standards and other regulations

COMMENTS FROM OTHER CITY DEPARTMENTS

As noted at the May 29 meeting of the CNPC, the nature of the design standards has required frequent coordination with the Departments of Public Works, Parks and Recreation, Safety and Inspections, and the City Attorney's Office. That coordination continued through the public review period and informed the updated draft design standards.

RECOMMENDED CHANGES

Due to the large volume of feedback from the Committee, the public and other City departments, significant changes to the draft were recommended. Most of the changes were to eliminate duplication of content already in the *Master Plan* or Zoning Code (such as height of boulevard plantings) and streamline material that addressed similar topics (such as instances of exterior building material descriptions and the building opening requirements). In addition, there was some material proposed to move to other areas of the *Master Plan* which is summarized in Attachment 3. There were, however, recommendations to substantially change or eliminate some content in the draft, such as:

- Reducing the specificity of setback requirements.
 - The public hearing draft included locations where setbacks were required to be more specific than the range outlined in the *Master Plan*, found in the Central Open Space Frontage standards (old S12) and the Gateway East Frontage standards (old S49). For building types allowed in the F3 district, setback distances are a range of 10'-20' for Townhouse, Multi-Family Low, and Multi-Family Medium building types; 5'-20' for Live/Work building type; 5'-15' for Mixed Residential & Commercial, Civic & Institutional, and Commercial &

Employment building types. Limiting the maximum to 10' in the Central Open Space Frontage sets a definite build-to line for the first set and significantly restricts it for the second. Market pressure will likely push it to the minimum setback anyway, but if not, the differing depths of buildings would help to create more visual interest and varied space on what will be an open space edge. The Gateway East Frontage type sits in the context of proposed baseball fields to the north and apartment buildings with large setbacks east of Cleveland Avenue. A range of setbacks is required in the *Master Plan* to ensure a relationship with the street, as well as proposed standard G14 to require orientation to the corner. Considering the context and the other siting regulations, a more specific range seemed unnecessary.

- Eliminating a standard that prohibited different architectural styles and materials on townhouses.
 - It is likely and appropriate that different materials will be used on the exterior of buildings. The standards in general do not address architectural style.
- Eliminated a standard that townhouse facades needed to be a minimum of 85% brick.
 - Based on feedback received during the comment period, requiring such a high percentage of a single material could lead to a monotonous swath of townhouses on the western half of the site.
- Redefining how the border of the Civic Square would be treated.
 - The public hearing draft included a “green planting area” that was difficult to define. The language was adjusted to be more specific and performance based, saying that trees will define the perimeter of the square and planted close to the property line.
- Removing a requirement that gathering areas face public areas.
 - It is impossible to predict what the exact use, design, and context of future buildings will be, and specifying the internal organization of site-specific architectural elements without that information seemed inappropriate.

To bring the Zoning Code and proposed *Master Plan* amendments into alignment, Zoning Code text amendments are also necessary, such as:

- Adding F districts to Sec 60.301 Zoning districts established
- Adding F districts to Sec 64.503 re: signs
- Clarifying that site plan review addresses all development in F districts, including one- and two-family dwellings
- Adding a process for minor and major amendments

July 12, 2019

CNPC – Ford Design Standards, Post-Hearing Recommendations

RECOMMENDED ACTION

The CNPC recommends that the Planning Commission:

1. Forward the July 18, 2019 draft of the Ford Design Standards and related Zoning Code text amendments to the Mayor and City Council with a recommendation to approve the amendments to the *Ford Site Zoning and Public Realm Master Plan* and corresponding Zoning Code text amendments.

Attachments:

1. Draft Ford Design Standards Amendments to the *Ford Site Zoning and Public Realm Master Plan*
2. Draft Zoning Code Text Amendments related to the *Ford Site Zoning and Public Realm Master Plan*
3. List of draft updated material in the *Master Plan*
4. Written comment from June 28, 2019 public hearing
5. Draft resolution

Cc:

Dr. Bruce Corrie, PED Director
Ward 3 Office
City Attorney's Office
Highland District Council
Tony Barranco, Ryan Companies
Design Standards Working Group
Design Standards Focus Group

ATTACHMENT 1:

Draft Ford Design Standards Amendments to the *Ford Site Zoning and Public Realm Master Plan*

Introduction

The design standards are to be used in concert with the building type details and form requirements found in other chapters of the *Ford Site Zoning and Public Realm Master Plan* and the The Saint Paul Zoning Code, which acts as the backdrop of zoning and other building and site regulations for all elements not specifically referenced within this chapter. It will be used in tandem throughout the design and review process. The design standards that follow support and complete the walkable, pedestrian-and bike-friendly built environment of the Ford Site.

The design standards are organized by the three scales to which they apply: site-wide, by zoning district, and by frontage type. The Vehicle Access diagram graphically maps where the walkable pedestrian environment is given priority, and where sidewalks and paths should not be interrupted by driveways, garage or service curb cuts.

Design standards that apply by zoning district and frontage type are focused on private spaces, lots, yards, and buildings. Because each district includes unique areas and features, frontage types address how the public/private development complements the public realm. A total of nine (9) unique frontages are applicable in various districts.

Design Standards – Site-wide

Private Landscape Standards

(Refer also to requirements for vegetative, landscape and building lot coverage in Chapter 4).

Lawns and Gardens

G1. Plant materials shall provide visual, multi-seasonal color and a layered aesthetic with plant materials at various heights and textures (refer to pages 47-49 of the Master Plan).

Pavements and Surfaces

G2. Surfaces for walks shall be poured concrete, integral color concrete, stone, pavers, clay brick, and other long-lasting materials.

G3. Concrete shall be broom finished with a deliberate scoring pattern to prevent cracking.

G4. Paving materials shall coordinate with the overall design of the lot and building.

Walls, Fences, and Furnishings

G5. Site and retaining walls shall be of long-lasting quality materials, with preference given to natural stone and clay brick that coordinates with related building materials on nearby structures. Segmental block is permitted, but shall be selected for resistance to salt and weather and are appropriate in scale, finish and color to building materials.

G6. With the exception of walls required for infrastructure, walls in landscaped areas exceeding 4' in height shall be terraced to soften their appearance. Climbing and cascading plants should be used.

G7. All retaining walls designed to retain or protect roadway ROW shall be designed as cast-in-place cantilever walls per the MNDOT Roadway Design Manual.

G8. Fencing visible from public rights-of-way shall be masonry, ornamental metal or wood, or some combination of the three, and shall be resistant to impacts of salt and weather.

G9. Furnishings shall be of high quality and match the style of the building design.

G10. The use of chain link, plastic or wire fencing (or similar) shall not be permitted.

Building Standards

G11. The lower twenty-five (25) feet of buildings shall include elements that relates to the human scale at grade. These elements include doors and windows, texture, projections, awnings and canopies, ornament, etc.

G12. Porches, steps, roof overhangs, hooded front doors or similar architectural elements shall be used to define all primary residential entrances.

G13. A primary building entrance shall be located on the addressed side(s) of the building.

G14. New buildings on corner lots shall be oriented to the corner and both public streets.

G15. For new Live/Work, Mixed Residential and Commercial, Civic and Institutional, and Commercial & Employment buildings, windows and doors or openings shall comprise at least fifty (50) percent of the length and at least thirty (30) percent of the area of the ground floor along addressed sides of the building and sides that face open space.

G16. Windows shall be designed with punched and recessed openings or other window installations that create a strong rhythm of light and shadow. Glass on windows and doors shall be clear or slightly tinted, and allow views into and out of the interior.

G17. If an outdoor storage, service or loading area is visible from adjacent residential uses or a public street or walkway, it shall be screened by a decorative fence, wall or screen of plant material at least six (6) feet in height.

G18. Surface parking shall not be located facing a front street or within thirty (30) feet of a corner.

G19. Exterior building materials shall consist of high-quality materials such as brick, stone, tinted masonry or cast stone, stucco, glass, metal, and fiber cement cladding/siding. The following materials are not acceptable: unadorned plain or painted concrete block; tilt-up concrete panels; synthetic stucco products (EIFS); reflective glass; and vinyl, fiberglass, asphalt or fiberboard siding.

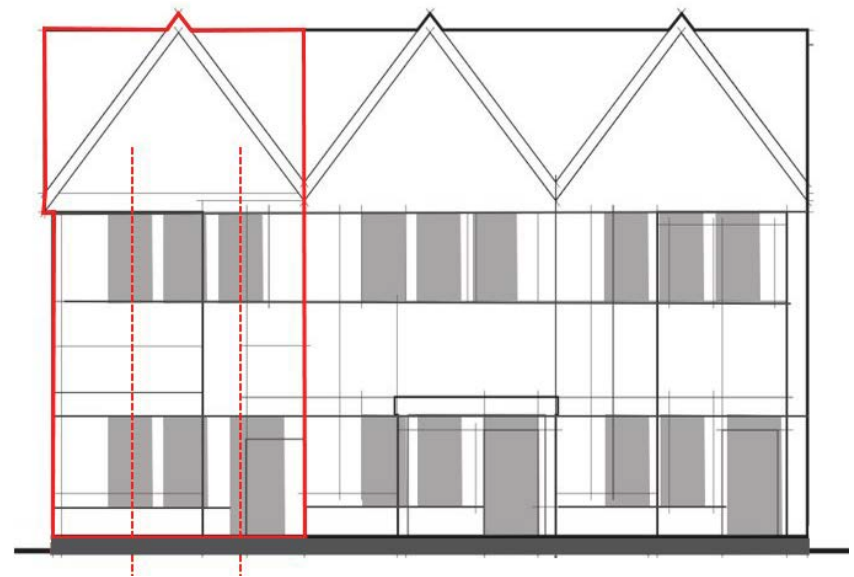
Standards specific to Multi-Unit and Townhouse Building Type

G20. Stoops may project into a required yard.

G21. Exterior stairs may be parallel or perpendicular to façade.

G22. Townhouse/rowhouse buildings shall exhibit vertical proportions; building elements including windows, balconies, doors, etc. shall reinforce the overall vertical proportions.

G23. All entrances within ten feet (10') of the front property line shall be raised above the average finished grade of the sidewalk a minimum of twelve inches (12") and a maximum of forty-eight inches (48") subject to ADA requirements.



Townhouse facade that exhibits vertical proportions

Standards specific to Structured Parking

G24. Entrance drives and garage doors for underground or structured parking shall be designed for pedestrian convenience and safety. Vehicular entrances to structured parking shall be minimized so that they do not dominate the access street frontage of the building. Design techniques shall include recessed entries; extending portions of the structure over the entry; using screening and landscaping to soften the appearance of the entry; using the smallest curb cut and driveway possible.

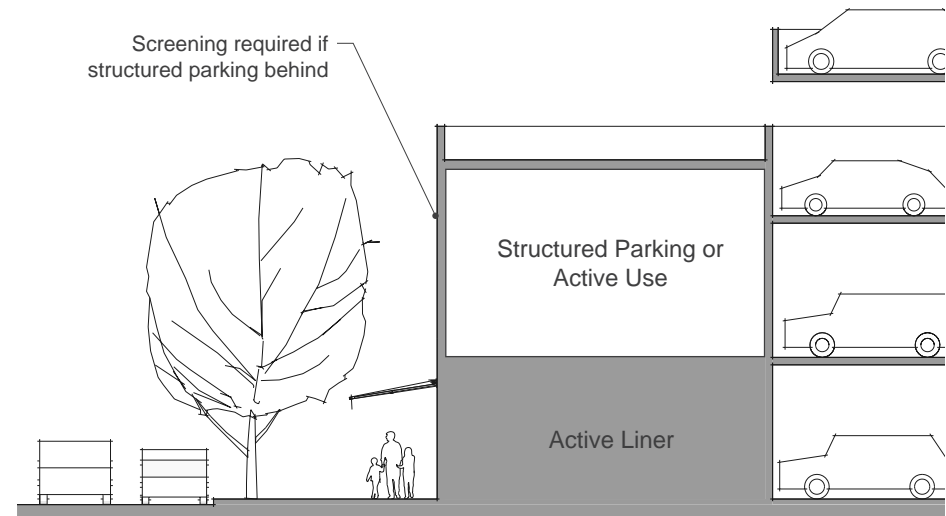
G25. New above-grade structured parking fronting on arterial and collector streets shall be lined with active commercial/retail uses at street level with direct access to the sidewalk.

G26. Any portion of structured parking that is not concealed behind active uses shall be designed with exterior wall treatments, ornamental metal, louvers, fenestration and materials that screen the view of vehicles and normal building and mechanical elements. Opaque glass used to cover structural beams shall not be permitted.

G27. Exposed ramping shall not face open space.

G28. Parking structure entrances shall be designed to be an integral part of the building façade, and to respond to the pedestrian experience and safety needs.

G29. Parking structures shall be finished with flat floor plates to allow for roof plantings and solar infrastructure.



Parking structure liner (ref G24 to G29)

Vehicle Access

The Master Plan is intended to create a walkable, mixed-use neighborhood with pedestrian access on all streets. However, vehicular access to parcels is a necessary requirement. The Vehicle Access diagram identifies locations where vehicular access to private lots should be located to minimize conflict with people on more pedestrian-focused streets. Due to site constraints, limited garage, service and loading access may be necessary on other streets.

Legend

||||| Vehicle Access Street

*Map not to scale



Design Standards by Zoning District and Frontage Type




Design standards also apply by zoning district and frontage type. Each district contains unique areas and features to which standards are applied to enhance the relationship between private development and public realm. A total of nine (9) frontages are applicable in various districts and are shown on the Frontage Map. “Frontage” is the combination of the building type/façade, yard/setback, public walk, and the relationship to the boulevard and street.



*Map not to scale



Frontage Types

-  *Urban Center*
-  *Urban Center/Diagonal*
-  *Urban Center/Residential*

The intent of the Urban Center frontage type is to ensure that the Urban Plaza, Diagonal Way and Civic Square spaces are vibrant and active, that building facades provide an edge to and define the public realm, and that public activity in the plaza and square does not negatively impact activity in the private development and vice-versa. Frontage types vary depending on whether the first-floor use is residential or non-residential.

Gateway

The intent of the Gateway frontage type is to activate Gateway Park based on the scale of the open space and the likely building types that will face it. The Gateway is a large open space that will feature an element to welcome people to the new neighborhood.

Central Open Space

The intent of the Central Open Space frontage type is to frame and activate this central defining feature of the new neighborhood. Given the anticipated amount of activity in this area, it will be especially important to provide a well-defined edge between the public and private realms along Falls Passage.

Pedestrian/Bike Street

The intent of the Pedestrian/Bike Street frontage type is to design and scale buildings in proportion to a narrower, limited-mode right-of-way. Given the planned scale of the buildings along these rights-of-way, designing the frontages to avoid a “canyon effect,” blank walls, garage and service access, and exposed building utilities will be critical to maintaining vibrancy.

Mississippi River Boulevard

The intent of the River Boulevard frontage type is to continue the physical character and relationship between street and building that already exists along Mississippi River Boulevard (MRB). Maintaining the unique arrangement of street, sidewalk, public landscaping, private landscaping and building façade will provide an edge to the new neighborhood consistent with the historic pattern along MRB.

Mississippi River Boulevard South

The intent of the River Boulevard South frontage type is to design and site residential structures so that there is a clear delineation between private property and the public space to the south. This frontage type must ensure that the open space is understood to be accessible to the public.

F1 River Residential

This district includes deep building setbacks from Mississippi River Boulevard and informal landscape consistent with the predominantly detached, single family residential character of the corridor.




Building types allowed by zoning: single family, multi-unit home and carriage house.

Private Landscape Standards *Trees*

S1. Trees within thirty feet (30') of Mississippi River Boulevard right-of-way shall be native species and planted in an informal pattern.



Legend

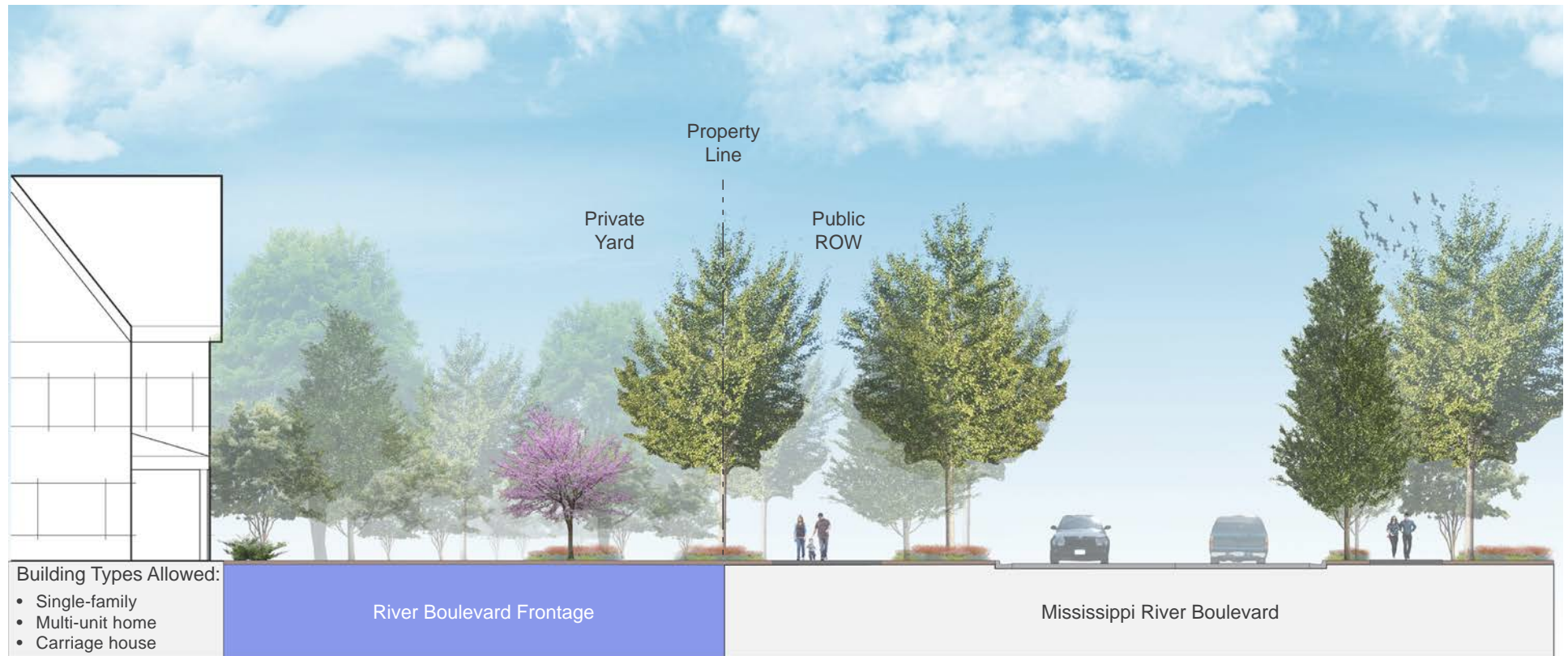
-  Mississippi River Boulevard Frontage
-  Mississippi River Boulevard South Frontage
-  F1 District Boundary

*Map not to scale

Mississippi River Boulevard Frontage Standards

S2. Private landscape shall reflect the existing the informal character of the Mississippi River Boulevard.

S3. The lot at the southeast corner of Mississippi River Boulevard and Bohland Avenue shall be setback at least fifteen feet (15') from Bohland Avenue.

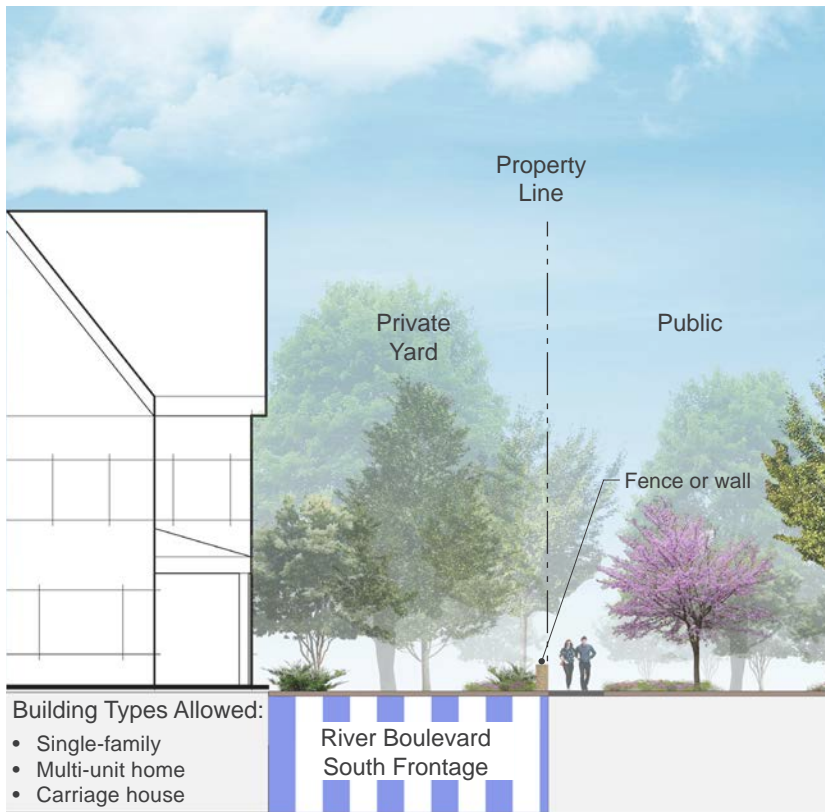


Illustrative section to indicate areas of public access. Does not represent correct scale or final design.

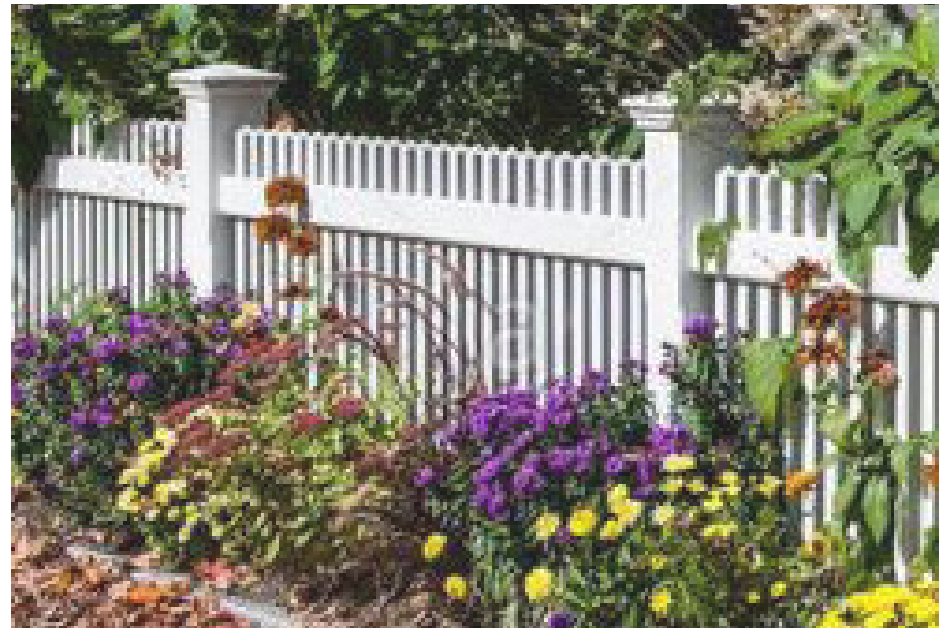
Mississippi River Boulevard South Frontage Standards

S4. The primary building face/facade must be oriented to the public open space.

S5. To differentiate the private lot from the public space, the southern property line shall be defined by a decorative fence, or wall with a maximum height of forty-two inches (42”), with breaks for pedestrian entry to units.



Illustrative section to indicate areas of public access.
Does not represent correct scale or final design.



F2 Residential Mixed Low District

The Residential Mixed Low District is intended to have an established residential neighborhood character. The neighborhood park is located in this district which will further reinforce a neighborhood character and scale.

Building types allowed by zoning: carriage house, townhouse, multi-family low, live/work, mixed residential/commercial, and commercial/employment.

Building Standards

S6. All residential entrances within fifteen feet (15') of the front property line shall be raised above the average finished grade of the sidewalk a minimum of eighteen inches (18") subject to ADA requirements.



Legend

— F2 District Boundary

*Map not to scale

F3 Residential Mixed Mid

This district allows a broad range of building types and straddles the central open space. The character and identity of this highly visible location will be dependent on the careful relationship between building facades/yards and the public open spaces. Specific frontage requirements apply to the central open space and the east-west oriented ped/bike streets.

Building types allowed by zoning: townhouse, multi-family low/mid, live/work, mixed residential/commercial, and commercial/employment.



Legend

- Central Open Space Frontage
- Pedestrian/Bike Street Frontage
- F3 District Boundary

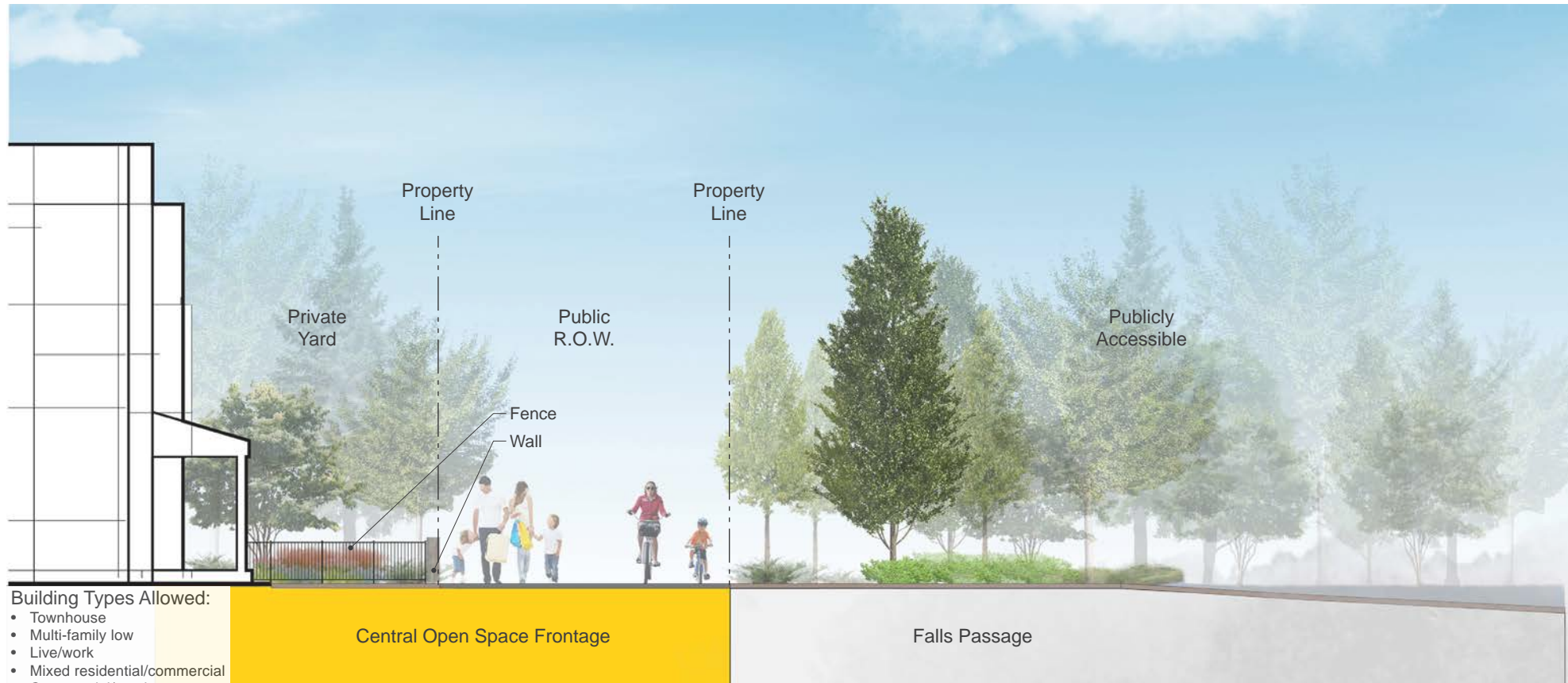
*Map not to scale

Central Open Space Frontage Standards

S7. A primary building entrance (stoop, porch or doorway façade) shall face the Central Open Space.

S8. A hedge, decorative stone, wood or metal fence (maximum 42' high) shall define the front property line, with breaks for pedestrian entry into units.

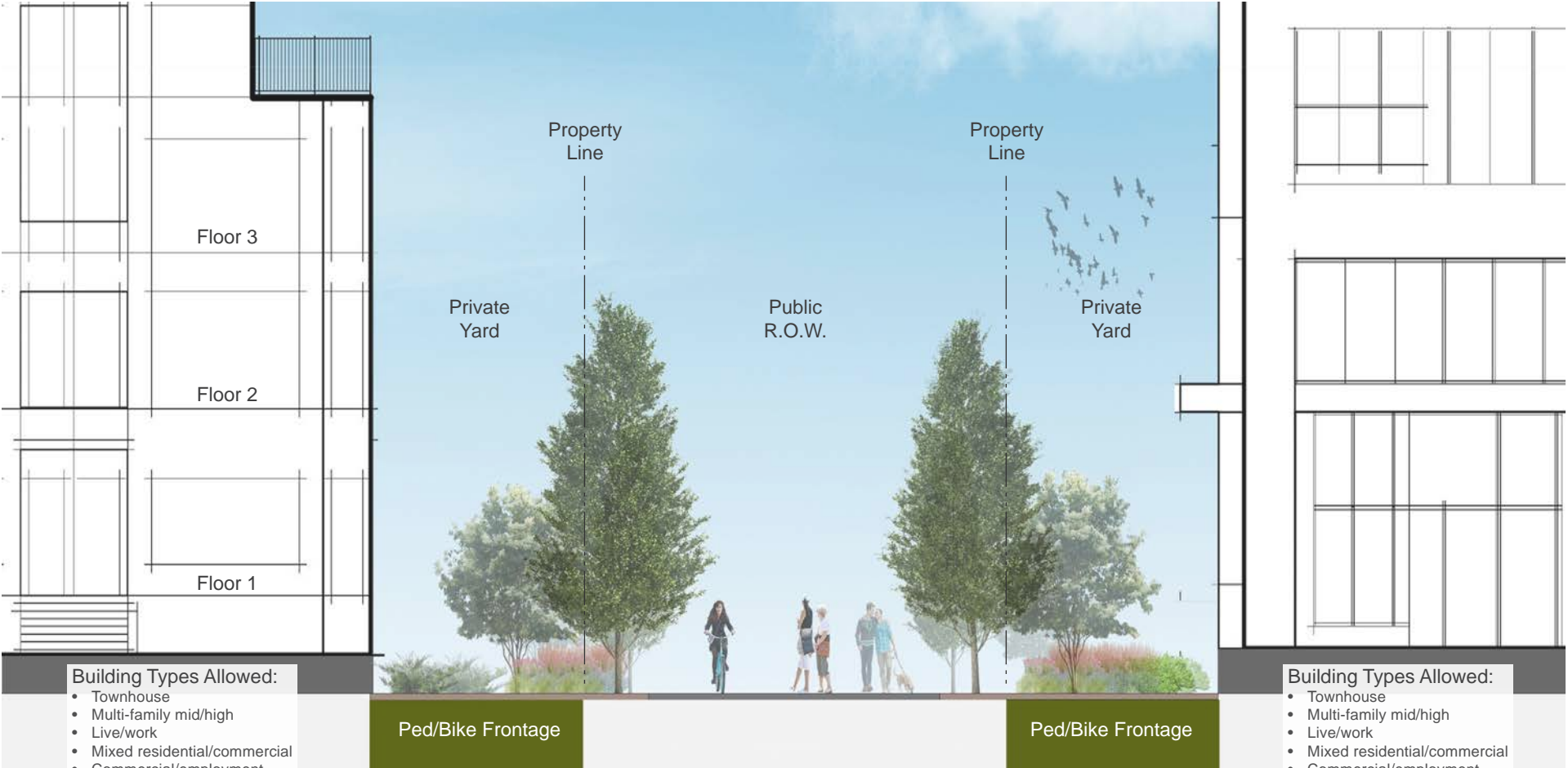
S9. Grass turf is not allowed in the yard closest to the central open space.



Illustrative section to indicate areas of public access. Does not represent correct scale or final design.

Ped/Bike Street Frontage Standards

- S10. Buildings shall step-back at least six feet (6') at the fourth floor or below if built to less than the ten-foot (10') setback.
- S11. Grass turf is not allowed in the private yard.



Illustrative section to indicate areas of public access. Does not represent correct scale or final design.



F4 Residential Mixed High District

This district will have an urban feel and character and contain a range of taller multi-family and mixed-use buildings. Streets and dedicated pedestrian/bike ways contribute to a more dense environment that will benefit from additional landscape and building setback requirements defined with frontage types.

Building types allowed by zoning: townhouse, rowhouse, multi-family medium, multi-family high, live/work, mixed residential/commercial, commercial & employment, civic/institutional and parking structures.

Pedestrian/Bike Street Frontage Standards

The Ped/Bike frontage type is described in the F3 District section of the design standards.



Legend

- Pedestrian/Bike Street Frontage
- F4 District Boundary

*Map not to scale

F5 Business Mixed District

This district will function as the commercial center of the site. The entrance and identity of the site hinges on the Urban Plaza. The Civic Square will act as the physical ‘heart’ of the neighborhood and will host a variety of seasonal activities. The diagonal way that connects the two will support a signature pedestrian-oriented destination.

Building types allowed by zoning: mixed residential & commercial, commercial/employment, civic/institutional and parking structures.



Legend

- Urban Center Frontage
- Urban Center Diagonal Frontage
- Urban Center Residential Frontage
- F5 District Boundary

*Map not to scale

Urban Center Frontage Standards

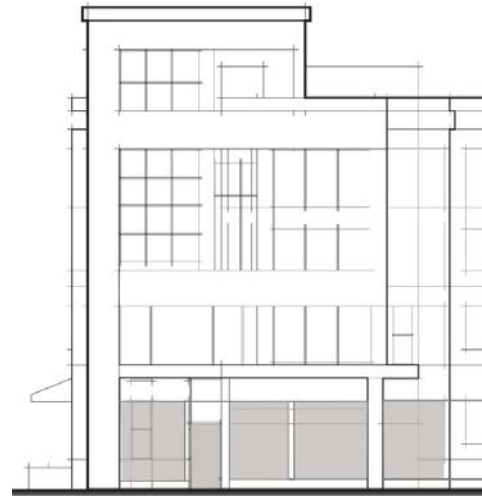
S12. The building face shall include a minimum of 65% transparent glazing in the bottom 12 feet of the building for portions of the ground floor not dedicated to residential units. See Urban Center/Residential for additional requirements for portions of the ground floor dedicated to residential units.

S13. Trees shall define the perimeter of the civic square and must be planted within 30' of the adjacent property line.

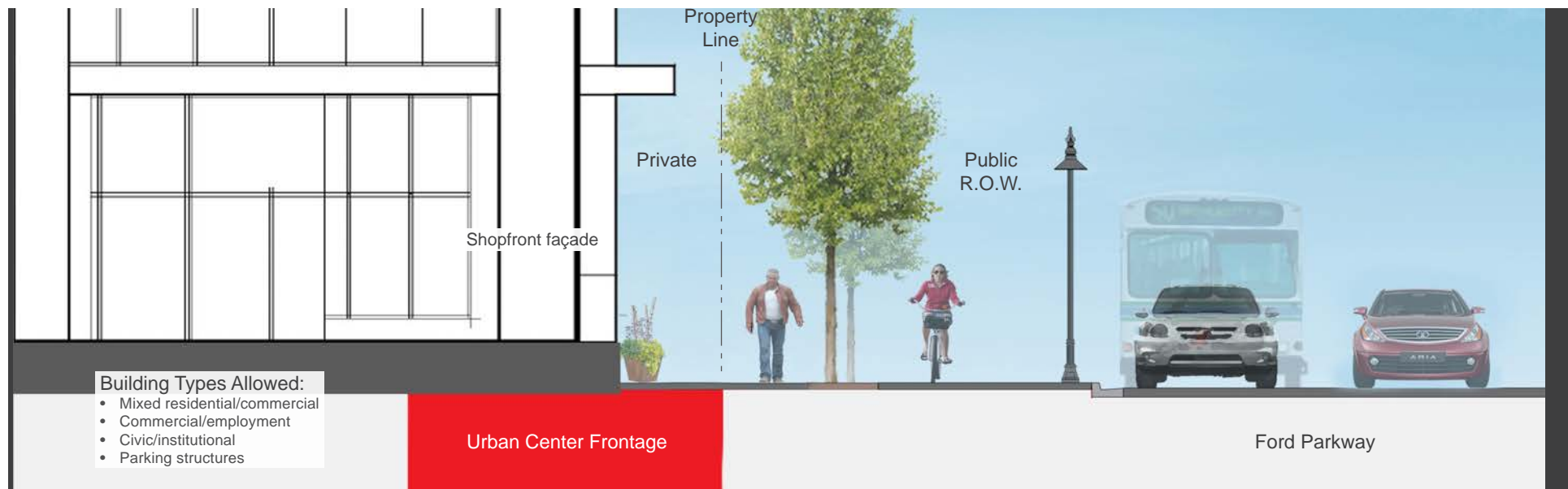
S14. Square and plaza surfaces shall be textured or integral colored poured concrete, stone, pavers, clay brick, or other long-lasting material.

S15. Building canopies/awnings to maintain a minimum nine-foot (9') clearance from ground plane.

S16. Building entries may be recessed from the façade up to six feet (6') in depth.



Facade with 65% glazing (S12)



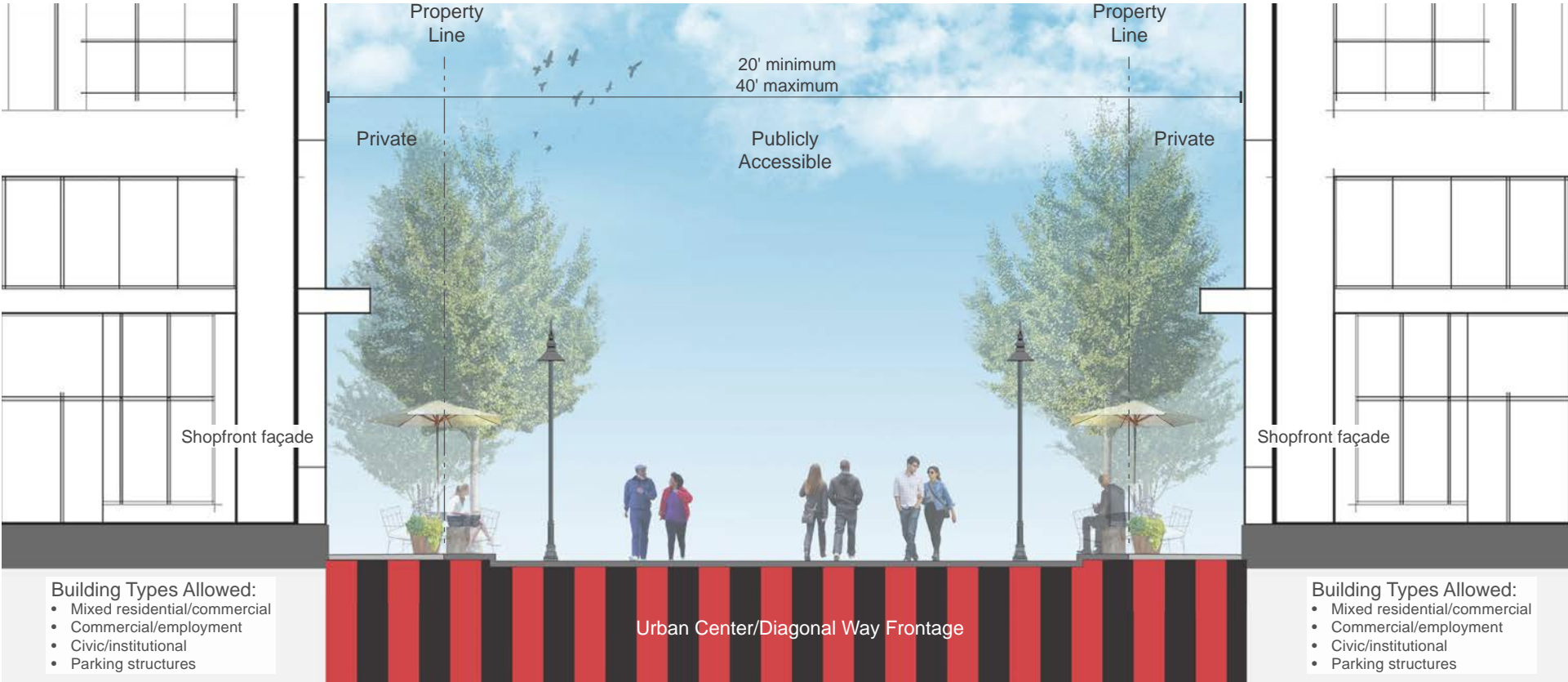
Illustrative section to indicate areas of public access. Does not represent correct scale or final design.

Urban Center/Diagonal Way Frontage Standards

All Urban Center Frontage standards shall apply.

S17. The building separation distance shall range from twenty (20') minimum to forty (40') maximum.

S18. The center of the Diagonal Way shall remain clear of obstructions to a minimum of twelve feet (12').



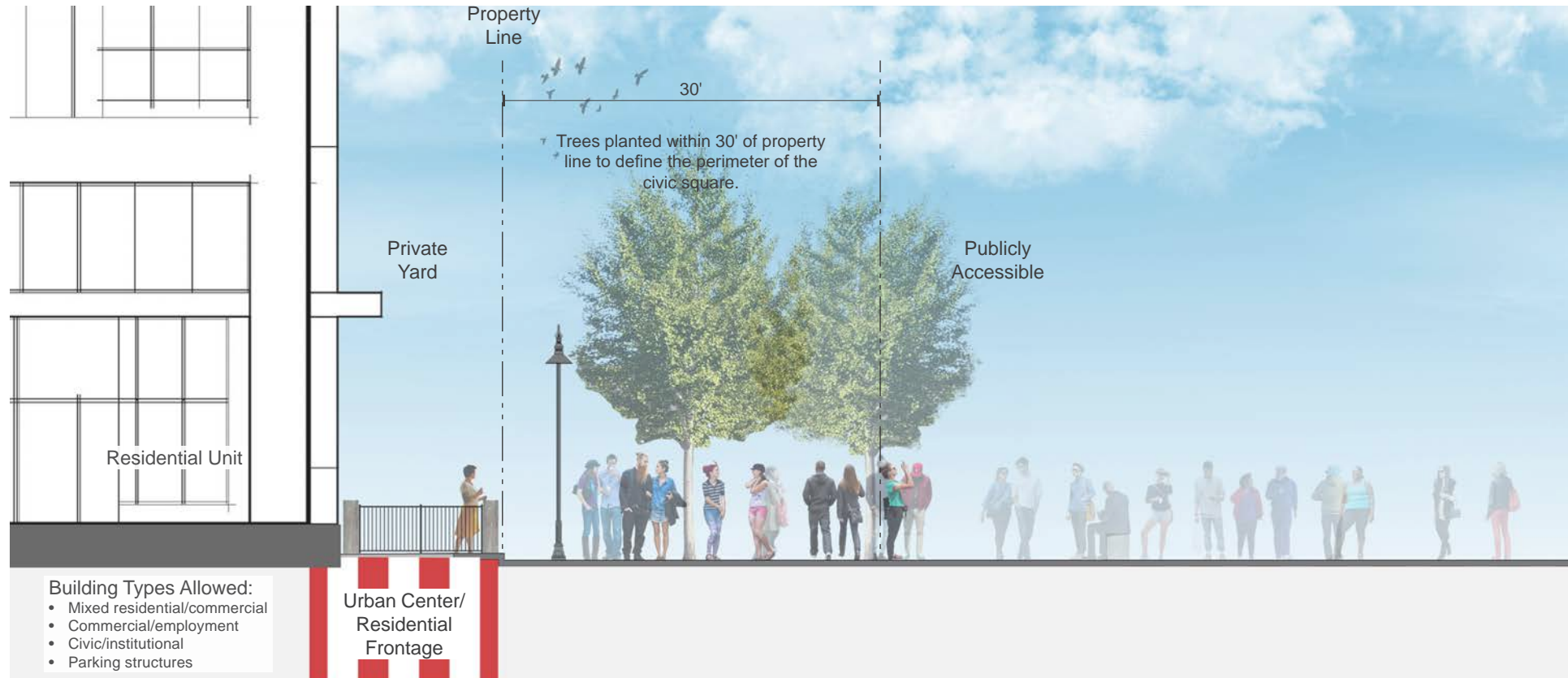
Illustrative section to indicate areas of public access. Does not represent correct scale or final design.

Urban Center/Residential Frontage Standards

All Urban Center Frontage standards shall apply.

S19. Any private patio/yard off of a residential unit shall be a minimum of twelve inches (12”) above ground plane of plaza/square surface.

S20. Residential units adjacent to the civic square shall be defined by a masonry or decorative metal railing (maximum forty-two inches (42”) high).



Illustrative section to indicate areas of public access. Does not represent correct scale or final design.

F6 Gateway

The gateway districts are highly visible corners of the site that will act as entry, identity and will also provide opportunities for business and employment uses. Applicable frontage types address the informal landscape character of the Mississippi River Boulevard and the more urban street relationship on the east side. Both locations represent unique opportunities for public art responses.

Building types allowed by zoning: commercial & employment, civic/institutional and parking structures.



Legend

- Gateway Frontage
- F6 District Boundary

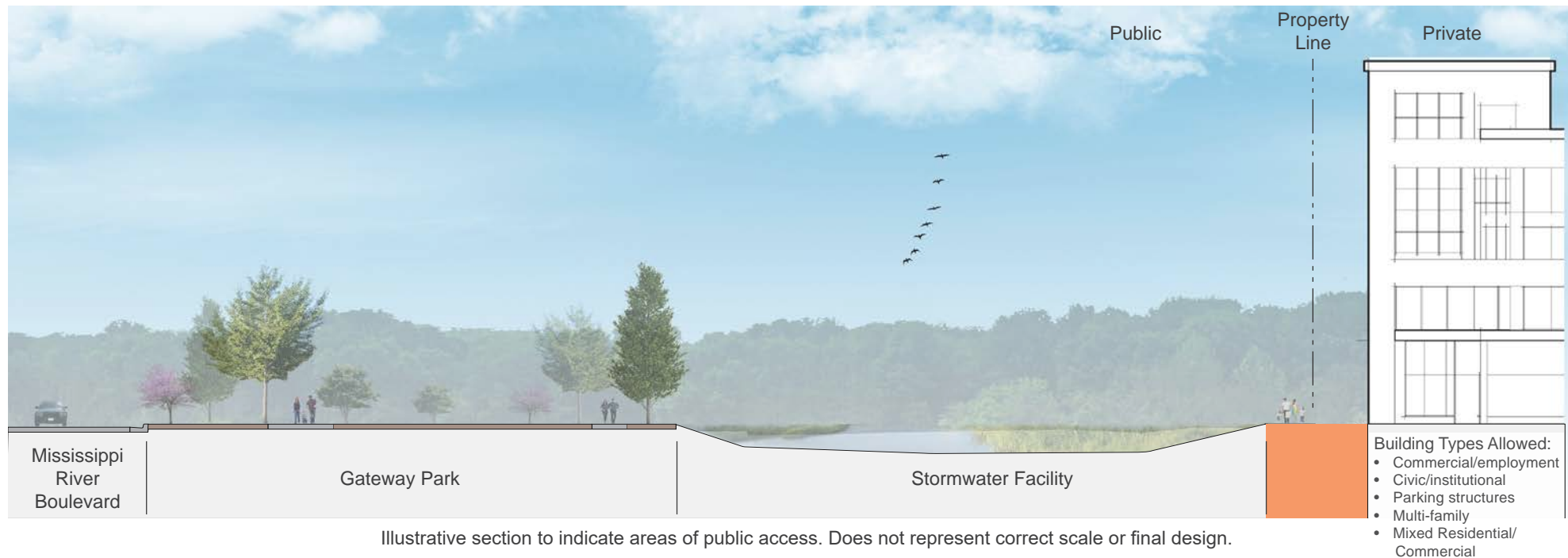
*Map not to scale

Gateway Frontage Standards

S21. Building setback fronting the Gateway park shall be a maximum of ten feet (10').

S22. The landscape design of the site and related stormwater facilities shall reflect the natural and informal character of the Boulevard.

S23. Retaining walls and other visual barriers facing the park shall be limited to twenty-four inches (24") high.





Appendix

Street Trees

- Refer to Legislative Code Title 8 Zoning Code, Chapters 66, 67 and Title 17, Chapters 175, 176, 177, 178, and Title 29, Chapter 362.
- For other boulevard treatments including rain gardens refer to applicable ordinances in Chapters 73, 105, 116, 121, 135 and 176 of the Saint Paul Legislative Code.

Building Standards

- Refer to Legislative Code Title 8 Zoning Code Article I. 63.100 General Provisions and Performance Standards

ATTACHMENT 2:

Draft Zoning Code Text Amendments related to the *Ford Site Zoning and Public Realm Master Plan*

DRAFT ZONING TEXT AMENDMENTS FOR FORD DESIGN STANDARDS (Revised 7/18/19)

The Zoning Code text amendments that follow are recommended to be addressed through a zoning study initiated by the Planning Commission to reflect the addition of proposed design standards. The existing text reflects adopted changes from April of 2019 (Ord 19-19, Ryan amendments).

Note: Only relevant sections that provide reference or are being amended are included.

Sec 60.301. - Zoning districts established.

For the purposes of this code, the city is hereby divided into the following zoning districts:

(a) Residential districts.

...

(f) Special districts.

VP vehicular parking district

PD planned development district

(g) Ford districts.

F1 river residential district

F2 residential mixed low district

F3 residential mixed mid district

F4 residential mixed high district

F5 business mixed district

F6 gateway district

~~(g)~~ (h) Overlay districts...

~~(h)~~ (i) River corridor overlay districts...

~~(i)-(j)~~ (j) Floodplain management overlay districts...

[Adds Ford Districts to list of zoning districts found in the City.]

Sec. 61.402. - Site plan review by the planning commission.

(a) *Plan to be submitted.* A site plan shall be submitted to and approved by the planning commission before a permit is issued for grading or the erection or enlargement of any building except one- and two-family dwellings, and including the following:

...

(4) Any development in a T or F district.

...

[There is currently no requirement for site plan review of single-family homes. Since there are specific standards proposed for the frontage along Mississippi River Boulevard, this amendment ensures review of the design standards in the event a one-family dwelling is proposed in that location. The exception for one- and two-family dwellings does not apply to (4) because (a) states that a site plan shall be submitted with those exceptions *and* including the following list. “Development” is defined in Sec. 60.205 and includes one- and two-family homes.]

Sec. 64.503. - T1-T4 traditional neighborhood, F1-F6 Ford and OS-BC business districts.

(a) *Business and identification signs...*

[Unless F districts are added here, 64.501 language prohibits signage on the site.]

ARTICLE IX. 66.900. FORD DISTRICTS

...

Division 3. 66.930. Ford District Dimensional Standards

Sec. 66.931. Ford district dimensional standards table.

Table 66.931, Ford district dimensional standards, sets forth density and dimensional standards that are specific to Ford districts. These standards are in addition to the provisions of chapter 63, regulations of general applicability. Where an existing building does not conform to the following requirements, the building may be expanded without fully meeting the requirements as long as the expansion does not increase the nonconformity.

Table 66.931. Ford District Dimensional Standards

Building Type by Zoning District (a)	Floor Area Ratio (Min.- Max)	Lot Width Min.(feet)	Building Width Max. (feet)	Building Height (feet)		Max. Lot Coverage by Buildings	Building Setback (feet) (e)	
				Min.	Max.		ROW (Min.- Max.)	Interior Min.
F1 river residential								
One-family dwelling	0.25 - 1.5	60	60	20	30 48	40%	10 - 40 (g)	10
Multi-unit home	0.25 - 1.5	80	60	20	48	40%	10 - 40 (g)	10
Carriage house	0.25 - 1.5	n/a	60	n/a	30	40%	10 - 20 (g)	6 (h)
F2 residential mixed low								
Townhouse, rowhouse	1.0 - 2.0	30	150	30	55	50%	10 - 20	6 (h)
Multifamily low	1.0 - 2.0	60	200	30	55	70%	10 - 20	6 (h)

DRAFT ZONING TEXT AMENDMENTS FOR FORD DESIGN STANDARDS (Revised 7/18/19)

Carriage house	1.0 - 2.0	n/a	60	n/a	30	per main building	10 - 20	6 (h)
Live/work	1.0 - 2.0	30	150	30	55	70%	5 - 20	6 (h)
Nonresidential or mixed	1.0 - 2.0	n/a	500	30	55	70%	5 - 15	6 (h)
F3 residential mixed mid								
Townhouse, rowhouse	1.0 - 4.0	30	150	30	65 (b)	50%	10 - 20	6 (h)
Multifamily	2.0 - 4.0	60	n/a	40	65 (b)	70%	10 - 20	6 (h)
Live/work	2.0 - 4.0	30	150	40	65 (b)	70%	5 - 20	6 (h)
Nonresidential or mixed	2.0 - 4.0	n/a	500	40	65 (b)	70%	5 - 15	6 (h)
F4 residential mixed high								
Townhouse, rowhouse	3.0 - 6.0	30	150	48	75 (c)	50%	10 - 20	6 (h)
Multifamily medium	3.0 - 6.0	n/a	n/a	48	75 (c)	70%	10 - 20	6 (h)
Live/work	3.0 - 6.0	30	150	48	75 (c)	70%	5 - 20	6 (h)
Nonresidential or mixed	3.0 - 6.0	n/a	500	48	75 (c)	70%	5 - 15	6 (h)
F5 business mixed								
Nonresidential or mixed	2.0 - 4.0	n/a	500	40	65 (d)	70%	5 - 15	6 (h)
F6 gateway								
Nonresidential or mixed	1.0 - 3.0	n/a	500	30	65	70%	5 - 15	6 (h)

Min. - Minimum Max. - Maximum ROW - Public Right-of-Way n/a - not applicable

Notes to table 66.331, Ford district dimensional standards:

(a) Building types are described and defined in Chapter 56 of the Ford Site Zoning and Public Realm Master Plan.

- (b) A maximum building height of seventy-five (75) feet may be permitted with a minimum ten (10) foot setback from all minimum setback lines for all portions of the building above a height of twenty-five (25) feet.
- (c) All portions of a building above a height of twenty-five (25) feet shall be stepped back a minimum of ten (10) feet from all minimum setback lines. The maximum building height may exceed seventy-five (75) feet, to a maximum of one hundred ten (110) feet, subject to the following conditions:
 - (1) A minimum of one (1) acre of buildable land in the F1, F2, F3, and/or F4 districts shall have been dedicated or conveyed to the city for public use for parks, playgrounds, recreation facilities, trails, or open space, in excess of the amount of land required to be dedicated for parkland at the time of platting. Such dedication of the additional parkland must be consistent with the criteria for parkland dedication in section 69.511, and is subject to city council approval.
 - (2) Maximum developable gross floor area of dedicated land from (c)(1), based on its underlying zoning, may be transferred and added to development allowed in an F4-zoned area, in compliance with other applicable requirements for the district or building, such as FAR, setbacks and open space coverage.
- (d) Building height may exceed sixty-five (65) feet, to a maximum of seventy-five (75) feet, with a minimum ten (10) foot setback from all minimum setback lines for all portions of the building above a height of thirty (30) feet, except for corner elements and portions of the building facing the civic square identified in the Ford Site Zoning and Public Realm Master Plan, Chapter ~~7~~8.
- (e) Building setback is the horizontal distance between a lot line and the nearest above-grade point of a building. An interior setback is measured from an interior lot line, which is a lot line separating a lot from another lot or lots. A public right-of-way (ROW) setback is measured from a lot line that is not an interior lot line: a lot line separating a lot from a street, alley, or public way.
- (f) Maximum building setback shall apply to at least sixty (60) percent of the building facade along the right-of-way.
- (g) Buildings shall be setback a minimum of thirty (30) feet, with no maximum setback, from a lot line separating a lot from Mississippi River Boulevard.

- (h) No setback is required for building walls containing no windows or other openings when the wall meets the fire resistance standards of the Minnesota State Building Code and there is a Common Interest Community (CIC) or recorded maintenance easement that covers the affected properties.

[Corrections to chapter numbers in the master plan and a transposition error from the April 2019 amendments.]

Division 4. 66.940. Ford District Development Standards

Sec. 66.941. Ford district accessory building standards.

In addition to the standards for accessory buildings in Section 63.501, accessory buildings in Ford districts shall be subject to the following regulations:

- (a) Accessory buildings shall meet required public right-of-way setback requirements for a carriage house in F1-F2 districts, and for the principal building on the lot in F3-F6 districts.
- (b) The Ford Site Zoning and Public Realm Master Plan, Chapter 56, regulates the number of accessory buildings permitted on a lot by building type.

Sec. 66.942. Ford district vehicle parking standards.

Off-street parking shall be provided as follows. These requirements supersede the parking requirements in section 63.207.

Table 66.942. Vehicle Parking Requirements by Use

<i>Land Use</i>	<i>Minimum Number of Parking Spaces</i>	<i>Maximum Number of Parking Spaces (a)</i>
Residential, dwellings	0.75 space per dwelling unit	2 spaces per dwelling unit
Residential, congregate living	0.25 space per bedroom	1 space per bedroom
Nonresidential	1 space per 600 square feet GFA	1 space per 200 square feet GFA

GFA – Gross Floor Area

The *Ford Site Zoning and Public Realm Master Plan*, Chapters ~~4.7~~ 4 and 5, sets vehicle parking facility standards that are in addition to the parking facility standards in chapter 63...

Sec. 66.945. Ford district general development standards.

- (a) ~~The design standards in section 66.343 for the T3 traditional neighborhood district apply in all Ford districts. The Ford Site Zoning and Public Realm Master Plan, Chapter 5, sets standards for building and public realm design.~~
- (b) The Ford Site Zoning and Public Realm Master Plan, Chapter ~~4.7~~, sets standards for vegetation and landscaping, lighting, solar energy, and roofing that are in addition to chapter 63 standards.
- (c) Site plans and other development proposals within the Ford zoning districts shall be consistent with the standards and requirements described in the Ford Site Zoning and Public Realm Master Plan unless the applicant can demonstrate that there are circumstances unique to the property that make compliance impractical or unreasonable.

Division 5. 66.950. Ford District Planning Requirements

Sec. 66.951. Ford Site Zoning and Public Realm Master Plan.

A Ford Site Zoning and Public Realm Master Plan, for use with this article to guide redevelopment of the Ford site, shall be adopted ~~and can be amended~~ by city council resolution after a public hearing and planning commission review and recommendation. Once approved, the Ford Site Zoning and Public Realm Master Plan may be amended as follows:

- (a) Minor amendment. Minor amendments to an approved master plan may be requested by the property owner or developer. The planning administrator shall cause the proposed request to be reviewed by the public works and parks and recreation departments and other affected city departments and may approve minor amendments, including changes of less than ten (10) percent in land area designated for public rights-of-way or parks, provided such changes are consistent with the intent of the master plan.
- (b) Major amendment. Major amendments to an approved master plan may be initiated by the city council, the planning commission, or any person having an ownership or leasehold interest (contingent included) in property that is the subject of the proposed modification. Major amendments include changes of ten (10) percent or more

in land area designated for public rights-of-way or parks; creation of a new public street or removal of a public street segment; rezoning; removal of a park or open space area; or addition or removal of an entire block. Major amendments may be approved by city council resolution following planning commission review, public hearing and recommendation.

[This language is based on the Traditional Neighborhood district planning requirements Section 66.344 to allow for adjustments during the master site plan, platting, and project site plan processes by establishing thresholds for administrative vs. council review. Proposed paragraph 66.945(c) also clarifies that all parts of the Master Plan shall be adhered to and reviewed during the site plan review process and pursuant to those rules (Sec. 61.400).]

ATTACHMENT 3:

List of draft updated material in the *Master Plan*

DRAFT list of changes to the *Ford Site Zoning and Public Realm Master Plan* not in proposed design standards chapter

7/18/19

1. Revise Table of Contents
2. Correct footer: Ford Site Zoning and Public Realm Master Plan
3. Correct chapter numbers and header titles to account for insertion of new Chapter 5: Design Standards
4. Remove references to forthcoming design standards

- Character & Site Organization

The public realm is intended to serve as the connective tissue within the site and to the neighborhood beyond. It is made up of the space between buildings - the right-of-way for streets and trails, the central stormwater spine, and the park spaces. ~~The private space between the right of way and buildings will be further defined by design standards to be added to the Ford site zoning districts.~~

- Where Regulations Apply

~~*Frontage Types to be addressed in forthcoming design standards (2018)~~

- General Standards

~~Design Standards~~

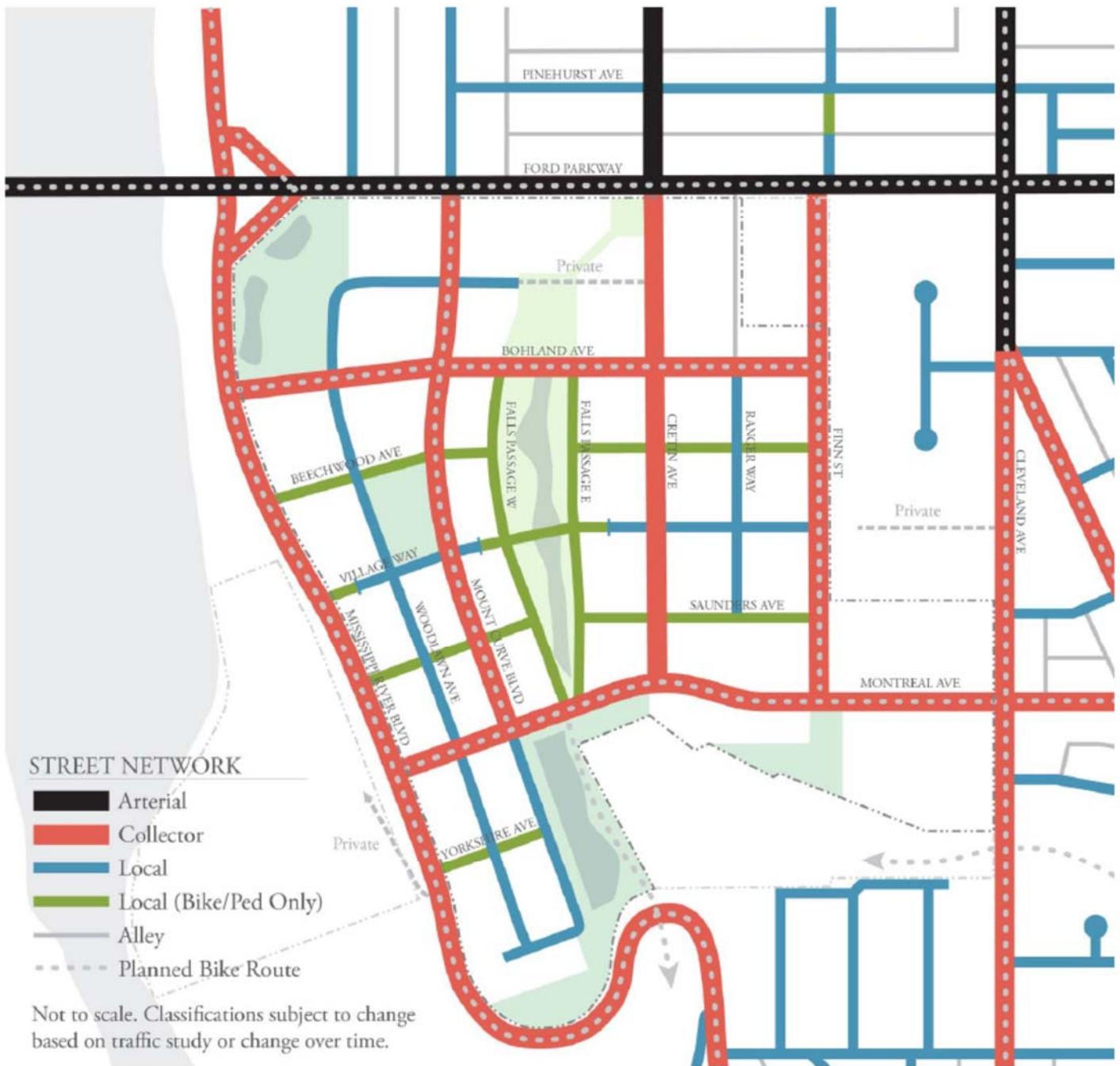
~~Design standards for buildings and public spaces on the Ford site redevelopment will be prepared for and added to this Ford Site Zoning and Master Plan in 2018. Until such time, the Traditional Neighborhood design standards for the T3 district shall apply, City Zoning Code Section 66.343.~~

5. Add row to Vegetative and Soil Requirements after “Street tree requirements” row:

	Unit	F1-F6
<u>Median Plantings</u>	<u>N/A</u>	<u>Trees are required in medians wider than eleven feet (11’), measured from the back of curb.</u>

	Unit	F1 River Residential	F2 Residential Mixed Low	F3 Residential Mixed Mid	F4 Residential Mixed High	F5 Business Mixed	F6 Gateway
Planting Size							
Trees - minimum planted size	(Caliper, Ht)	2.4 in; 6 feet					
Shrubs	Diameter	18 inches					
Vegetative Variety (minimum mix of species)							
Native overall	Minimum	85%	85%	75%	75%	75%	75%
Trees	Min Species Mix	6 per acre; 4 per block					
Shrubs	Min Species Mix	5 per acre					
Perennials	Min Species Mix	10 per acre					
Tree Canopy (measured as the % of the area)							
Tree canopy cover		50%	50%	30%	20%	20%	20%
Public Canopy Cover							
Civic space minimum	Area Covered	50% of non-built lot area			25% of non-built lot area		
Street tree requirements	Spacing	clustered		30' on center			
Private Canopy Cover							
Private lot minimum	Area Covered	1 per 7500 sf of lot or 12%			none		
Parking lot minimum	Area Covered	30%					
Healthy Tree Standards							
Minimum permeable surface per tree	Area	270 sf	270 sf	25 sf	25 sf	25 sf	25 sf
Structural soil per tree	Area	180 sf	180 sf	250 sf	250 sf	250 sf	250 sf
Soil Volume Standards for Tree Planting							
Soil volume		Minimum 2 cuft of soil per 1 sq ft of canopy, based on average mature tree size; or 400 cu ft for small trees, 800 cu ft for medium trees, or 1,200 cu ft for large trees.					
Soil volume type and location		Soil volume goals may be achieved through connected or combined soil beds or grouped tree planting. Use of structural soil under hardscapes, planting soil in open planting beds. Volume of structural soil/engineered soil structures to be determined by percentage of soil volume available.					

6. Update Street Network System map to clarify terminology of street hierarchy to match categories in zoning code and used by Public Works:



7. Reflect solar energy update from June 2019 in use table in Master Plan:

Use	F1	F2	F3	F4	F5	F6	Definition (d) Standards (s)
Public Services and Utilities							
Antenna, cellular telephone	P/C	P/C	P/C	P/C	P/C	P/C	(d), (s)
Electric transformer or gas regulator substation			P	P	P	P	(s)
Municipal building or use		P	P	P	P	P	(s)
Public utility heating or cooling plant		P	P	P	P	P	
<u>Solar energy generation facility, community</u>		<u>P/C</u>	<u>P/C</u>	<u>P/C</u>	<u>P/C</u>	<u>P/C</u>	<u>(d), (s)</u>
Utility or public service building	P	P	P	P	P	P	(d), (s)

8. Open Space diagram and descriptions

Urban Plaza and Civic Square

The urban plaza is the “front door” of the development and one of the most visible sites in the neighborhood. As a mostly paved, hardscape environment, the plaza will include features such as a fountain and public art, shopfronts that define the pedestrian space, and amenities such as tree plantings and a comfortable pedestrian environment.

Connected to the urban plaza via a narrow pedestrian connection, the a-civic square will serve as the focal point for community gathering throughout the day and year, for employees, residents, visitors and the Highland Community. The square will be located on the north end of the site near Ford Parkway, providing a link between the commerce and activity of Highland Village and the newly developed Ford site.

The ~~public~~ civic square will be lined with retail, service, residential and office uses, providing a critical mass of activity and people around the space. The square will be a pedestrian-only space, with vehicular access only for deliveries, cleaning, and emergency during restricted hours and from designated access points. Tenants choosing the office, residential and business frontages on the civic square, will be those that thrive in active, pedestrian environments, and seek a unique, place-based location that is rare to find in the region. All buildings lining the square will have vehicular access at the rear or in structured parking.

...

Community Green Space

The community green space will provide common areas to support future community gardens and other active and passive uses. Steep slopes are present on the south side of this park, and the green space will include well-defined landscaped edges and retaining walls as needed for stability and access.

[Also, update map and labels as needed to reflect the changes here and as amended in April 2019.]

9. Remove reference to T3 standards for structured parking that was adopted as part of Ryan amendments in April 2019; replaced with design standards language
10. Correct formatting errors as needed.

ATTACHMENT 4:

Written comment from June 28, 2019 public hearing

Richardson, Mike (CI-StPaul)

From: Nathaniel M Hood <nmhood@gmail.com>
Sent: Monday, June 17, 2019 2:41 PM
To: Richardson, Mike (CI-StPaul)
Cc: #CI-StPaul_Ward3; McMahon, Melanie (CI-StPaul); Tolbert, Chris (CI-StPaul)
Subject: Ford Site Design Standards | Planning Commission | Submitted Comments

Follow Up Flag: Follow up
Flag Status: Flagged

Think Before You Click: This email originated outside our organization.

Good afternoon Mike,

I am submitting comments for the City of St. Paul's Ford Site Design Standards – If possible, could you submit these comments to the Planning Commission for consideration for the June 28th public hearing?

I am generally supportive of the Ford Site Design Standards as written. I think city staff did a good job highlighting the major issues. There are only a few additional items I feel could be improved on. I have detailed those below. Thank you for your consideration -- and thank you in advance to passing along to the city's Planning Commission.

--

Page 2: Open Space Diagram –

- The City should aim to connect Village Way to the site to allow for pedestrian and bike traffic. This can be accomplished in a cost-effective way with a 10ft shared-use bituminous trail.
- The City should also aim to create a connection on the north of the Highland Ball fields (Open Space "H") to allow pedestrian traffic to connect through to Saunders Ave. This can be accomplished in a cost-effective way with a 10ft shared-use bituminous trail.
- The extension of Saunders Ave on the western section of the Ford Site is labeled as "Galaxie Ave". This may be confusing and I recommend being consistent throughout the corridor and staying with "Saunders Ave" (unless there is a historic reason / rational precedent for changing the name).

Page 3: Open Space Standards –

For "Open Space F" titled "Hidden Falls Headwaters Park" - While the the stormwater function is important, we should aim to make this space a great amenity for the neighborhood and include language to design it in a manner that doesn't make it simply a stormwater-run off retention wetland. The pond concept was one of the few non-controversial elements of the plan (it was widely supported) and we should aim to identify this space as something more than a "stormwater feature".

Page 4: Street Types and Descriptions –

Under "Vehicle Access Street", the City should aim to limit the size of curb cuts for parking garages/underground parking to 22 to 24ft. Smaller curb cuts lead to lower turning speeds and allow for more on-street parking opportunities. Slower speeds for vehicles leaving garages will lead to a safer pedestrian environment.

Page 7: Gateway East Definition

The paragraph states that "greenspace is *encouraged* to face the street". I believe we should change the language to "*should*". This area of the site will be the most dense residential district of the site and I believe that it's important we incorporate good, greenspace at the street level in this node. A well-design linear public greenspace can help make a dense residential district feel quiet, quaint and comfortable.

Page 8:

"G12" - "Where breaks occur, the street edge shall be continued through the use of fencing, low walls and/or landscaping". We should add that the fencing should aim to be decorative and the low walls be made of higher quality building material.

Page 9:

"G18" - This section should provide guidance on structured and underground parking access design, such as curb cut width (aim for 22ft to 24ft for residential, slightly larger for commercial) and to provide a design that allows for safe exiting with proper site views. Having abrupt exits from structured parking can create a safety issue for pedestrians (e.g.: *The Finn onto Highland Parkway as an example of what not to do*). Smaller curb cuts lead to lower turning speeds and allow for more on-street parking opportunities. Slower speeds for vehicles leaving garages will lead to a safer pedestrian environment.

Page 12:

"S7" – The maximum height for a decorative fence should be reduced to 36". This is an appropriate height to delineate private space, but will do so by not being as imposing as taller fencing/ walls. I believe making this modification will help make the pedestrian space along MRB better.

Page 14: F3 Residential Mixed Mid

The City should aim to include language regarding residential parking facilities. If too difficult / cost prohibitive to allow underground parking, the building should avoid the "parking podium" design which can create a blank space at the human-scaled pedestrian realm.

Page 23: F6 Gateway

"S43" – This outlines the building materials recommended. I agree with the building material standards on all other districts that favors a neo-traditional urban design. However, the Gateway is an opportunity to allow for a creative / landmark structure. While we should still aim for a high standard of material and design, I believe we should deviate from the norm of the site in this district to allow more architectural flexibility; such as glass buildings, create metal façade buildings, etc.). I do not have recommendations on what new language would look like, but this is the one district where we should explore something ambitious/creative.

Thanks again for the consideration,

- Nate Hood
Highland Park, St. Paul

1879 Montreal Ave.
St. Paul, MN 55116



HIGHLAND

DISTRICT COUNCIL

1978 Ford Parkway • Saint Paul MN 55116 • 651.695.4005 • HighlandDistrictCouncil.org

Resolution on Ford Site Design Standards, Public Grounds Maintenance, and Placement of Utilities

Whereas the Highland District Council's Community Development Committee met with the City of Saint Paul, and Mike Lamb from LHB Urban Design and Planning on June 18th to discuss the proposed Design Standards for the Ford Site; and

Whereas there was a question about how building codes were factored into the Design Standards and Master Plan and what happens if they are updated during this process; and

Whereas the committee was concerned that the maintenance of public grounds, and placement of utilities are not specified in the Design Standards; therefore

Be it Resolved, that the Community Development Committee of the Highland District Council requests that the City make sure there is some method of accountability, for updating building codes, public ground maintenance contracts, and placement of utilities.

Approved June 25, 2019

By the Community Development Committee of the Highland District Council

Butler, Sonja (CI-StPaul)

From: Richardson, Mike (CI-StPaul)
Sent: Friday, June 28, 2019 7:31 AM
To: David Sullivan-nightengale
Cc: Butler, Sonja (CI-StPaul)
Subject: Re: Ford Design Standards Public Comment

Thank you very much, Mr. Sullivan-Nightengale.

Your comments will be forwarded to the Planning Commission for their consideration.

Regards,
Mike Richardson

On Jun 27, 2019, at 10:31 PM, David Sullivan-nightengale <dsullivannightengale@yahoo.com> wrote:

Think Before You Click: This email originated **outside** our organization.

Mr. Richardson:

Regarding the Ford Design Standard I make the following recommendations

1. Include the design criteria of the MNDOT Complete Streets Policy
2. Consider using barriers of any variety to prevent motor vehicles from colliding with pedestrians and businesses as this hazard continues to be unmitigated in both state and federal design standards
3. Consider making all new construction require fire sprinklers. Water has been proven to put out fires. Yes, the fire chief says it every year and we continue to ignore it.
4. Ensure adequate capability for inclusion of charging stations of electric vehicles especially bonding and grounding systems into the public right of way
5. Ensure all above ground power lines are buried to improve system reliability
6. Improve signage to assure people can find their way through the community without needing to own a smart phone
7. Include areas where snow can be piled without obstructing views between vehicle operators and pedestrians
8. Consider embedding a local operating network for public lighting to lower light pollution for the nearby Mississippi National River and Recreational Area.
9. Ensure people remain off of nearby dangerous sloped areas with improved guards and barriers
10. Provide safe access to the nearby river so people can connect with the paddlecraft community
11. Nothing is specifically zoned for schools or health clinics. We've seen how this can be problematic in the long run. It's a long way from the Ford Site to the get medical care as the former Ford workers know so very well.
12. The area is in the airport operations area for one of the runways at Minneapolis-St. Paul International Airport. Construction and habitation in this area falls inside the airport certification plan and risk reduction measures to lower bird strikes must be taken. Migratory pattern changes due to people living in this area as opposed to simply working in this area will occur resulting in a higher risk. Onsite use of composting or open garbage container restrictions are recommended for communities in close proximity to airports like MSP.
13. The advent of urban air mobility means that smaller and more frequent air travel across the site is possible. Consider noise mitigations for new construction.
14. Consider implementing signage and sensing elements to allow the safe use of autonomous vehicles now so we don't have to go through costly retrofits ten years from now.

Respectfully,

David J. Sullivan-Nightengale
Certified Safety Professional
1132 Norton Street
St. Paul, MN 55117
(651) 247-6410



Ford Site Design Standards - Ryan Companies Comments List 06.27.2019		
Change	Ryan Comments/Concerns	Page # or Section #
1	The Ryan Companies renderings from 2018 should be an additional reference point for the design standards	1
2	Specific requirements for programming/content in each of the open spaces will differ depending on the community engagement process and the funding resources available at the time of design/construction	3
3	Access restrictions to certain blocks must have flexibility depending on the final development conditions of the block (i.e. multiple buildings on one block may not be able to access from only one street). Ranger Way specifically is a concern for lack of vehicular access allowed if access is restricted from Cretin Ave	4
4	Preservation of trees must be considered along Mississippi River Boulevard	5, 10
5	Restrictions on wall heights pose issues with certain areas of the mass site grading and future building plans given the challenging topography of the site	8
6	Permitted material list for private walks should be expanded	8
7	Requirements on Commercial, Civic and Mixed-use areas of the plan regarding building form, doors and windows	8, 14
8	Building material palette should be expanded	9, 14, 23
9	Increased setback on specific F1 district lots creates inconsistencies between other lots in this district	11
10	Requiring walls to delineate public and private space can be successfully solved by other means	12
11	Redundancy with the Public Realm Plan in permitted building types	14
12	Due to the challenging topography of the site, the finished floor elevations in relationship to exterior grade will vary across all of the buildings. Restricting this relationship to such as tight delta (18-42") will not be possible	15
13	The specific dimension of the proposed step-back on upper floors does not work with standard floor plans of multi family projects	16
14	Restrictions to shopfront facades should only apply for frontage of the specific commercial use, not the entire building	17, 18, 20
15	Alternative square and plaza surface materials, such as stamped concrete, should be allowed at the discretion of city staff for specific design features	19, 21
16	Due to the challenging topography of the site, flexibility should be allowed to transition the ground floor grade to the adjacent sidewalk grade exterior to the building (i.e. ramping) in certain instances	20
17	Consideration should be given to the building setback and location of the gathering areas in order to allow for the informal nature of the park	24
18	Based on the location and surrounding land uses, a larger setback or building placement may be preferable	25
19	Review for inconsistencies with the Public Realm Plan	66.931
20	Consideration should be given to the minor and major modification change descriptions	66.951

Mr. Tony Barranco
4439 Fremont Avenue So.
Mpls. MN 55419

ATTACHMENT 5:

Draft Resolution

city of saint paul
planning commission resolution
file number _____
date _____

RECOMMENDATIONS ON PROPOSED FORD SITE DESIGN STANDARDS AMENDMENTS
TO THE *FORD SITE ZONING AND PUBLIC REALM MASTER PLAN* AND ZONING CODE

WHEREAS, Zoning Code § 61.801(b), based on Minnesota Statutes § 462.357, Subd. 4, provides that amendments to the Zoning Code may be initiated by the Planning Commission; and

WHEREAS, the *Ford Site Zoning and Public Realm Master Plan* was adopted by City Council Resolution PH 17-261, and associated amendments to the Zoning Code were adopted by Ordinance 17-40; and

WHEREAS, Zoning Code § 66.951 provides that the *Ford Site Zoning and Public Realm Master Plan* can be amended by City Council resolution after a public hearing and Planning Commission review and recommendation; and

WHEREAS, engagement efforts that informed the *Ford Site Zoning and Public Realm Master Plan* revealed significant interest in how the buildings and public spaces of the site would look and function; and

WHEREAS, the *Ford Site Zoning and Public Realm Master Plan* states that design standards for buildings and public spaces on the Ford site will be prepared and added to the document; and

WHEREAS, amendments to the Zoning Code are necessary for consistency with the *Ford Site Zoning and Public Realm Master Plan*; and

WHEREAS, the City Council, in Ordinance 10-17, directed that design standards for the Ford site redevelopment area would be reviewed by the Planning Commission and considered for adoption by the City Council; and

WHEREAS, the Comprehensive and Neighborhood Planning Committee, on May 29, 2019, forwarded its recommendation to the Planning Commission for initiation of a zoning study for Zoning Code amendments corresponding to proposed design standards amendments to the *Ford Site Zoning and Public Realm Master Plan*;

WHEREAS, the Saint Paul Planning Commission, on May 31, 2019, initiated a zoning study to consider Zoning Code amendments corresponding to proposed design standard amendments to the *Ford Site Zoning and Public Realm Master Plan*; released the draft design standards and zoning text amendments for public review; and set a public hearing for June 28, 2019; and

WHEREAS, the Saint Paul Planning Commission, on June 28, 2019, held a public hearing on the proposed design standard amendments to the *Ford Site Zoning and Public Realm Master Plan* and zoning text amendments, notice of which was published in the St. Paul Legal Ledger on June 13, 2019 and held the public record open for written comments until July 1, 2019; and

WHEREAS, the Planning Commission considered the public testimony and the recommendations of the Comprehensive and Neighborhood Planning Committee at their July 26, 2019 meeting; and

NOW, THEREFORE, BE IT RESOLVED, that the Saint Paul Planning Commission hereby recommends to the Mayor and City Council approval of the attached design standards amendments to the *Ford Site Zoning and Public Realm Master Plan* and corresponding Zoning Code text amendments.

moved by _____
seconded by _____
in favor _____
against _____