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# CITY OF SAINT PAUL

Christopher B. Coleman, Mayor

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**DATE:** July 21, 2017

**TO:** Saint Paul Planning Commission

**FROM:** Comprehensive Planning Committee and Neighborhood Planning Committee

**SUBJECT:** Review of Ford Site Zoning and Master Plan public hearing comments, draft plan,

and recommendation for Planning Commission

#### **BACKGROUND**

On July 28<sup>th</sup>, the Planning Commission will consider a recommendation on the Ford Site Zoning Study and Public Realm Master Plan, which involves three related actions:

- 1. Amending the Saint Paul Code of Ordinances to establish six new 'Ford' zoning districts under Article IX, 60.900, Ford Districts;
- 2. Rezoning four parcels owned by three property owners in the zoning study area to one or more of the six new zoning districts; and
- 3. Adopting the Ford Site Zoning and Public Realm Master Plan.

#### Materials related to each action are attached to this memo.

The Ford Site Zoning Study introduces six new zoning districts to the Saint Paul Zoning Code. The districts are intended for application on the Ford site properties. Amendments to the Saint Paul Zoning Code integrate the new zoning districts and standards into the current code format, under Article IX. 66.900, Ford Districts.

The Ford Site Zoning and Master Plan provides a more detailed description of the Ford districts and related standards.

The Ford Site Zoning and Master Plan is composed of nine chapters:

- 1. Preface
- 2. Vision and Principles
- 3. Existing Conditions
- Zoning Districts & General Standards
- 5. Zoning Building Types
- 6. Infrastructure
- 7. Parks and Open Space
- 8. Public Art
- 9. Sustainability

General citywide zoning regulations will apply within the Ford zoning districts, unless otherwise noted within the district standards.

Four properties are proposed for rezoning to one or more of the Ford zoning districts. The parcels are:

#### **Parcel A** - PIN 172823110092

Owned by Joan Lipschultz Burg Trustee and Fremajane Wolfson Trustee 3.76 acres

Current Zoning – T2 Traditional Neighborhood

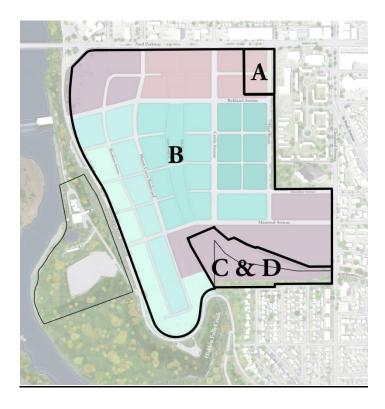
Current Land Use – retail

#### **Parcel B** - PIN 172823130002

Owned by Ford Motor Company
122.4 acres
Current Zoning I1 – Light Industrial
Current Land Use – vacant industrial site, former auto assembly plant

#### Parcels C & D - PINS 172823410001 and 172823410002

Owned by Canadian Pacific Railway Company
6.06 acres and 6.67 acres
Current Zoning I1 – Light Industrial
Current Land Use – unused railyard, formerly served Ford Motor Company



#### **PUBLIC HEARING COMMENTS**

On June 30, 2017, the Saint Paul Planning Commission held a public hearing on the proposed Ford Zoning and Public Realm Master Plan. At the hearing, 49 people spoke, 14 representing organizations and 35 individuals. The organizational representatives spoke in general support of the plan, with a couple requesting adjustments to the plan to provide more open space and lower heights in the blocks closest to the river. Of the individuals, 17 spoke in opposition, 12 spoke in favor, and 6 provided specific comments that were neither for nor against the plan in general.

In addition to spoken testimony, the Planning Commission received 312 written comments between the public comment period of May 19<sup>th</sup> to July 3<sup>rd</sup>. All written and oral comments for the hearing can be found at <a href="stepaul.gov/fordcomments">stepaul.gov/fordcomments</a>. Comments submitted in both written and oral form are noted.

This memo responds to key themes articulated in the comments, including but not limited to:

- A mix of opinions on the level of development density and heights proposed for the site, with more commenting in favor than against. Those concerned about height focus on the 7% of the site proposed to allow up to 110 feet and on the 2 blocks closest to the river than would allow up to 55 and 65 feet. Density concerns focused on negative impacts some anticipate it will create, while supporters cited its benefits. Often, the expected impacts were completely opposite one another such as, the added density will increase property values in the area versus it will decrease property values.
- A mix of opinions about whether future traffic generated by the Ford site will be manageable or overwhelming in the area. People strongly support good infrastructure for walking, biking and transit.
- A number of comments focused on the need and desire for affordable housing at the future site.
- A number of comments focused on how taller buildings and too many people at the site would degrade the livability and quality of the Highland neighborhood.
- Many comments highlighted the value of parks and open space at the future site and the desire to have even more than proposed in the plan.
- Some people expressed concern that the land owner, developer and the city will make too much money from the proposed plan, while others said it will support a needed increase in the city's tax base.
- Almost everyone expressed strong support for the proposed stormwater feature.

#### **DENSITY OF DEVELOPMENT**

#### **Height of Buildings**

The range of building heights allowed in the Ford zoning districts ranges from 20 feet minimum (2 stories) to 110 feet maximum (8-10 stories). The number of stories depends on the floor heights used. Retail floors tend to be 12-18 feet high and residential floors range from 11-14 feet typically. Height ranges across the 6 zoning districts are shown below.

ZONING DISTRICT MINIMUM HEIGHT MA	AXIMUM HEIGHT
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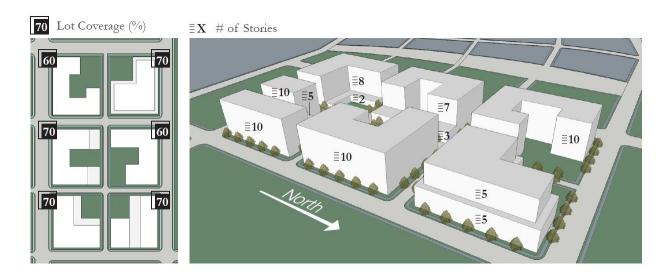
F1 – River Residential	20 feet	48 feet
F2 – Residential Low	30 feet	55 feet
F3 – Residential Mid	40 feet	75 feet
F4 – Residential High	48 feet	110 feet
F5 – Business Mixed	40 feet	75 feet
F6 - Gateway	30 feet	65 feet

The current scale of Highland near the site is generally 1.5 to 3 stories, with some 4 story businesses and apartments. The tallest structures nearby, adjacent to the site, are 740 Mississippi River Blvd at 208 feet, the Cleveland Public Housing apartments at 128 feet. The proposed heights for the Ford zoning are lower than the tallest buildings, but higher than the general scale of the area. Is the addition of taller buildings into the area a bad thing? People from the community vary widely in their opinions about that. The plan intentionally tiers heights away from the river and site edges to moderate the shift in scale from adjacent parcels, a tiering that provides views from the upper units on most new blocks. Few views from existing buildings will be obstructed, because treetops currently obscure views from most existing buildings. The proposed heights would make some buildings visible from other parts of the neighborhood where streets provide open angles to the new development. When the Ford site operated as a manufacturing plant the same vantage points would have looked upon the 214 foot tall paint building emissions stack with a blinking light on top.

#### **Scale of Buildings**

The scale of buildings is best described as the mass of the building relative to the open space on the lot. Floor Area Ratio (FAR) is the formula that calculates building mass on a lot. Each Ford zoning district has a minimum and maximum FAR. Floor Area Ratio can be used to moderate the overall scale of building, in addition to height and setback requirements. Let's look at how the standards interact.

The illustration below shows a diagram of six different ways that the F4 Residential Mixed – High blocks could be built to their maximum volume using proposed setbacks and reaching the maximum FAR of 6.0. All are shown at a height of 110 feet and there is a variety of lot coverage up to the maximum of 70%.



Another way to control building mass is to limit its width. A maximum building width is used for the smaller residential building types in the Ford zoning to ensure that their scale is consistent with the district they are in – smaller buildings for lower scale blocks and larger buildings for larger scale blocks.

The combination of lot and building standards within each Ford zoning district aims to achieve a responsible balance between building mass, open space, and transition space to public rights-of-way. The setback and open space requirements in the Ford districts are higher than those in the City's T2, T3, and T4 districts, ensuring that all blocks have a soft edge between buildings and sidewalks, creating a comfortable and attractive pedestrian environment.

#### **Number of People**

We are often asked "how many people will be on the site?" We roughly estimate a possible 1,500 employees and 4,000-8,000 residents. Estimates are based on a number of assumptions. For housing, we assume 1.8 people per unit, the 2010 census average household size for multifamily housing units in Highland Park. Since the Ford site is proposed for a range of multi-family housing types, we use 1.8 people per household, although future household sizes are expected to shrink in general. Even within Saint Paul today there is wide variation, with the 2010 census finding an average of 1.3 persons per multi-family unit in downtown St. Paul.

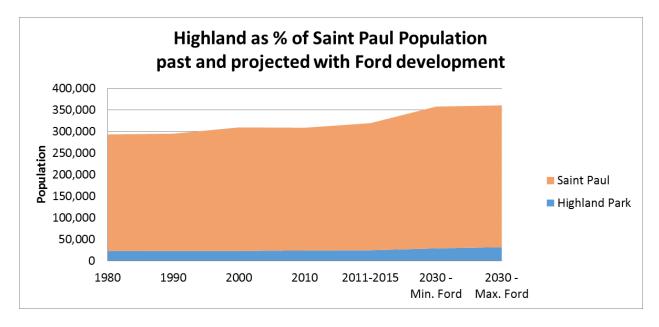
The proposed Ford plan sets a lower and upper limit on sitewide housing units -- 2,400 to 4,000 units. Assuming 1.8 people per unit, we have a residential population range of 4,320 to 7,200 people on the site by 2040.

Below are 2 tables and a graph showing past and estimated future population growth of Saint Paul, relative to the potential population growth of Highland related to Ford site development under the proposed plan. The table shows that even with the high end estimates of Ford site population, Highland will remain at the same percent of overall City population as it is today. Some development may occur in Highland that is not on the Ford site, but with such a strong

market pull to the site, we expect that the majority of market demand for new development in Highland will go to the Ford site.

	Saint Paul F	opulation	Fo	ord Site Estim	ated Popula	tion	Highland Population			
	Current and Forecasted by Met Council	Change in Population per decade	High Estimates based on Ford Plan	% of Citywide population growth	Low Estimates based on Ford Plan	% of Citywide population growth	Current and Estimated with High Ford site development	% of Citywide population	Current and Estimated with Low Ford site development	% of Citywide population
2010	285,068	-	-		0		24,589	9%	24,589	9%
2020	315,000	29,932	-		0		25,000	8%	25,000	8%
2030	329,200	44,132	3,600	8%	2160	5%	28,600	9%	27,160	8%
2040	344,100	59,032	7,200	12%	4320	7%	32,200	9%	29,320	9%

	1980	1990	2000	2010	2011-2015	2030 Min. Ford	2030 Max. Ford
Highland as % of City	8.7%	8.5%	8.1%	8.4%	8.4%	8.8%	9.7%



What type of place does one expect the Ford site to be with a new concentration of people and how will it affect the area around it? Opinion on this varies widely, as heard in the public hearing testimony. It might help to picture being in other such places in the Twin Cities, places where residential density attracts retail and services, activity in the public realm and a range of residents. These residents are seeking proximity to services and activities for convenience, time savings or necessity due to mobility limitations. One might think of 50<sup>th</sup> and France, Centennial Lakes, or the North Loop in Minneapolis. Have these areas experienced a spike in crime and a reduction in surrounding property values? The places in the region that struggle most with crime and declining property values tend to be low density and lack new development. The following articles address the question of infill development impacts on surrounding property values:

- <a href="http://www.denverrealestatewatch.com/2011/11/13/research-high-density-good-for-home-values/">http://www.denverrealestatewatch.com/2011/11/13/research-high-density-good-for-home-values/</a>
- <a href="https://factsmatteraddison.com/special-informational-article-mixed-use-housing-decrease-home-value/">https://factsmatteraddison.com/special-informational-article-mixed-use-housing-decrease-home-value/</a>
- https://streets.mn/2016/02/07/no-large-apartment-buildings-wont-devalue-your-home/

There is no denying that the proposed Ford plan envisions introducing new blocks to the neighborhood that have a different character than many of the existing blocks in Highland. The old blocks will remain the same and the new blocks will be a change. The shift in scale toward larger buildings for new infill development is a tension felt in this neighborhood, the city, the region and in communities across the country. As Saint Paul grows in the coming decades, new development will come to neighborhoods at a height and scale that is larger than the historic precedent. The economics of development today are different than 100, 75 and 50 years ago; more development value is needed per acre to support the costs of site preparation and construction. In the living memory of most residents of Highland, the population of Saint Paul has been on the decline and is only recently seen an increase. We are now approaching our alltime high population that occurred in 1960 with projected growth to continue as discussed earlier. This is a positive for the city, but represents a departure from what many perceive to be the history and status quo for growth in Saint Paul. We are tasked with thinking about the future in terms of decades and centuries and must acknowledge this shift to growth. Our existing infrastructure can service more people and buildings, while growing the tax base to support needed upgrades and maintenance of streets, parks and services. Cities are not sustainable environmentally and economically in the long term without the growth of population and tax base, and with this will come growth in the scale and density of neighborhoods across the city.

#### **TRANSPORTATION**

#### **Automobile Traffic**

The volume and patterns of future auto trips to and from the Ford site is an essential consideration in determining what mix and level of development the Ford site can accommodate. For that reason, the City hired a transportation consulting team to evaluate potential trips to and from the Ford site at full build out. The team, led by national experts in multi-modal transportation modeling for mixed use development, used today's travel patterns to and from the Highland area to estimate when and where people would go on the future Ford site, based on the mix and level of uses proposed.

The Ford Site Transportation Study and results can be found athttps://www.stpaul.gov/departments/planning-economic-development/planning/ford-site-21st-century-community/project-studies#1 A mixed use area can expect that some trips each day are made on foot or bicycle, going to a nearby coffee shop, walking to the drugstore, etc. The remaining trips will travel to and from the site by car, walking, biking and transit. The proportion of walking and biking trips on and off the site was assumed in the study to be the same proportion that exists in Highland today. The proportion of trips by transit was assumed to go up about 9% from today's share, with a corresponding decrease in vehicle trips. The assumption of greater transit ridership for site users was made based on growing transit ridership in Highland today, expected growth in transit ridership over the next 20 years, and the self-selecting nature of people living in multiunit housing units; people who tend to own fewer cars and drive less than single-family households which make of much of Highland today.

The Ford transportation study was thorough and provides expected traffic estimates for all roads connecting to the Ford site. The additional trips on each roadway, which will increase gradually over the next 5-25 years, can be absorbed into the existing, unused road capacity, with an ultimate level of service that is acceptable from a traffic movement standpoint. Individuals have personal feelings about what an "acceptable" level of traffic is on a given road, but that level may fall well below how many cars the road is designed to carry.

#### **Walking and Biking**

The proposed Ford plan includes bicycle and pedestrian trails connecting north to south, east to west, to the surrounding neighborhood, and to the Mississippi River, Highland commercial district and other major destinations. Each street right-of-way includes sidewalks on both sides and bike lanes are included on y number of the roads. This level of bicycle and pedestrian infrastructure is one of the defining, high-amenity features of the site that will draw residents who value walking and biking, while enhancing the connectedness of the Highland area as a whole. Making connections at the site's edges is equally important to increase the quality and usability of this infrastructure and way of travel in the neighborhood. The plan's proposed network for walking and biking has been widely and strongly supported.

#### **Transit**

Transit service in Highland Village and along this site is very strong today, with multiple bus routes and the A-Line bus rapid transit. Future transit should route through the Ford site, running along Cleveland and Montreal Avenues. Space for dedicated transit lanes is included in the plan's Cretin right-of-way and continues southeast along the Canadian Pacific Railway alignment. The dedicated transit right-of-way could accommodate bus, streetcar, or light rail transit vehicles. Metro Transit has worked closely with the City to consider transit's role in the future site and has clearly indicated that it will introduce and match service levels through the future site in relation to the number of people living and working there. Strong transit service is a key factor in reducing vehicle use and trips and is a priority for the site.

#### **Streets**

The proposed network of streets through the site is based on a fundamental principle used throughout the City of Saint Paul - to provide a grid network of regular street spacing to keep block sizes small and to maximize connectivity to adjacent streets in the area. Streets around

the Ford site are limited and therefore all connections were made to help disperse trips through the site and onto the surrounding road infrastructure. The proposed street connections and number of streets would not vary if land uses at the site changed or the volume of development shifted; the proposed street network is a basic framework to support any type of site redevelopment. The proposed street network has been widely supported, although some residents of Mount Curve Boulevard oppose a signalized intersection connecting Mt Curve north and south of the site due to concerns about added through traffic.

#### **HOUSING**

#### Type

Zoning for the site allows a wide range of housing options from townhouses, carriage houses, and live-work units, to multi-family units in buildings of different sizes and styles to fit the occupants -- young or old, singles or families, wealthy or limited income. The one housing type not included in the plan is single-family. Single-family is already very well supplied in Highland, while demand grows for condos and apartments in Saint Paul and Highland. Saint Paul's Comprehensive Plan states that new residential development should focus on multi-family units, senior housing, and affordable units across the City. The Ford site offers an excellent opportunity to meet market demand for greater housing variety in a neighborhood setting. Over the decade, City staff has heard over and over again from people that want to live at the Ford site – people who want to move out of a single-family home and into an apartment or condo in a thriving, active and walkable urban place with lots of people around and things to do nearby.

Another consideration for future housing at the site is economic. Single-family housing does not create nearly enough land value to pay for new development infrastructure - streets, water, sewer, lighting, etc.

As a final consideration, Ford may disallow single-family reuse as a condition of site sale, since single-family use poses the greatest liability risk for former brownfield sites. Ford will clean the site up to a residential reuse standard, but liability risk remains theirs for perpetuity.

#### **Ownership vs Rental**

The City has no direct control over whether future units at the site will be ownership or rental. At any time, the real estate market may favor one type or the other, but trends shift and over the course of the 12-20 years that it will take this site to build out, we expect a mix of both types. Rental units today appeal to all types of people, regardless of age, income, or even tenure, and renters can be just as engaged and committed to the community as homeowners.

#### **Affordability**

Providing affordable housing at the Ford site is a priority for the City and will require a long-term strategic plan to implement with the limited tools available.

1. The City can require the inclusion of affordable units if the City provides public financing for site redevelopment.

- 2. The City can try to direct limited funding for affordable housing construction in the city to the Ford site, but it would be at the expense of providing units elsewhere in the city.
- 3. The City can institute inclusionary zoning on the site to require the provision of affordable units within each residential project, but it's a sensitive tool and has to be very carefully crafted to address the needs and limitations of the market.

#### **OPEN SPACE**

#### **City Parks**

The plan includes 9% of land area for city parks, the maximum that the City can require for dedication by the landowner under the City's parkland dedication ordinance. The 9% park space is geographically dispersed across the site and designed to serve different recreational functions:

- a civic square for community events and gathering in the business district
- a gateway park in the northwest corner to provide open space along the river valley and to serve as an attractive visual entry to Saint Paul from Minneapolis
- a neighborhood park with flexible open space and room for a playground, a small dog area, a community garden, or other uses
- a picnic area type park on the southwest near the bluff top of Hidden Falls Regional Park
- park land along the edges of the linear water feature, providing more space for activities and leisure along this natural amenity.

Many people have expressed an interest in more park space on the future site. This may be desirable, but the City cannot require any more than the 9% land area that is already in the plan. A future private developer may choose to provide additional park space as an amenity to increase adjacent land values for development, but this will be a market decision. The only other option would be for the City to purchase additional land from Ford for park space. However, even if the City had money available for park land purchases, which it currently does not, some other areas of the city have as much need for park space as the Ford site does.

#### **Trails**

Pedestrian and bicycle trails run across the site, occupying 4% of the site land area. These trails will be landscaped and used for outdoor recreation and travel, similar to a linear park, although they are technically a transportation infrastructure use. The trail network is a widely popular site amenity that would better connect people in the neighborhood and on the site to parks and amenities in the immediate area.

#### **Recreational Fields**

The City strongly supports the inclusion of recreational field space on the redeveloped Ford site. After testing various locations and configurations for this type of space on the site relative to other uses and their location, the southeast corner was ultimately identified as the best location for recreational fields. Part of this land is owned by Ford and the remainder is owned by Canadian Pacific Railway.

The Highland Little League has played on Ford's land for decades and is very interested in staying on the site. However, other recreational groups have also expressed interest in the site, including Blackhawks soccer and lacrosse. Regardless of the sport, a path to acquisition of land for field space must be determined with the leadership of recreation groups. The City will continue to work with Ford and recreational groups to try to find a solution, since the additional land, up to 11% of the site, is above and beyond the 9% of parkland already included in the plan.

#### **Water Feature**

The stormwater feature shown on the plan was located and sized for the anticipated level of development at the future site. A 2016 study by Barr Engineering found that a centralized treatment feature had a number of advantages to traditional treatment under individual private lots, with comparable cost and better environmental performance. The stormwater feature is the most popular part of the proposed Ford site plan.

The full Ford Site Stormwater Study can be found at - <a href="http://www.capitolregionwd.org/wp-content/uploads/2012/09/Ford-Site-Sustainable-Stormwater-Management-August-2016-1.pdf">http://www.capitolregionwd.org/wp-content/uploads/2012/09/Ford-Site-Sustainable-Stormwater-Management-August-2016-1.pdf</a>

#### **RETAIL AND EMPLOMENT**

#### **Employment**

The proposed plan focuses on employment uses within the Gateway and Mixed Business districts. The expected amount of employment at the site, an estimated 1,500 jobs, is based on ongoing conversations with real estate professionals, two market studies, and two employment studies.

In 2016, the Ford Site Jobs Strategy Work Group composed of city, business and economic development professionals examined best fit employment for the Ford site based on its unique constraints and opportunities within the employment marketplace of the Twin Cities.

The Ford Site Jobs Strategy Report can be found at - <a href="https://www.stpaul.gov/sites/default/files/Media%20Root/Planning%20%26%20Economic%20">https://www.stpaul.gov/sites/default/files/Media%20Root/Planning%20%26%20Economic%20</a> Development/Jobs%20Strategy%20Report Final.pdf

In 2009, the Minnesota Legislature paid for a study to examine the feasibility of repositioning the Ford site for a new industrial use to replace the auto manufacturing jobs being lost. The study found that industrial reuse options were extremely limited for the site. The Ford Site Green Manufacturing Reuse Study can be found at -

https://www.stpaul.gov/sites/default/files/Media%20Root/Planning%20%26%20Economic%20 Development/Ford%20Site%20Green%20Mfg%20Reuse%20Study%20Aug%202009.pdf

Attracting employers to the Ford site, beyond retail and service, will require proactive outreach by the future developer, the City, and business and economic development partners. The Ford Jobs Strategy report identified key steps and roles to accomplish the goal of bringing a mix of high quality jobs to the future Ford site.

#### Retail

Highland Village today is a thriving commercial area serving the retail and service needs of the neighborhood and the wider community. The Ford site should complement Highland Village, expanding retail and service options for the existing neighborhood while meeting the new market demands of a developed Ford site. Over the decade, the community has been clear about its interest in small to medium size neighborhood-serving businesses at the Ford site. Large, destination uses and big box stores have not received support. Many retail brokers agree that Highland Village is not well-suited to large, destination stores, which favor areas well served by large roads and nearby highways.

The Ford site zoning and master plan identifies a range of retail and service space for the site -- 150,000 to 300,000 square feet. This level fits our current understanding of the site's potential market and the desired retail character of the Highland community.

#### COMPREHENSIVE AND NEIGHBORHOOD PLANNING COMMITTEES RECOMMENDATIONS

On July 11<sup>th</sup>, the committees met to consider the June 30<sup>th</sup> public hearing comments and potential changes to the proposed Ford Site zoning and master plan. A staff presentation on key topics and input was reviewed and main items discussed by committee members. The committees approved the following items for recommendation to the full Planning Commission:

- Adopt the Ford Site Zoning and Public Realm Master Plan and associated Amendments to Zoning Code Article IX. 66.900 FORD DISTRICTS, establishing the new districts, and apply the Ford districts zoning to the four subject parcels, with two suggested refinements to the Master Plan
  - MRB Realignment -- Identify support for potential realignment of Mississippi River Boulevard at the south edge of the site, by softening the S curve and shifting the roadway north a bit, if the opportunity arises to add bluff top park space to Hidden Falls Regional Park. [This recommended change is noted in text on page 101 of the Plan]
  - Operiction of Recreational Fields -- Clarify that the area shown on the plan as "non-public recreation" is the future desired use for the land in that area of the site, but is not part of the site's future city parkland secured through City Parkland Dedication nor is it future infrastructure such as the land for stormwater management. As such, maps showing future public land should not imply that the recreation field space is also guaranteed for open space use. "Public or private park, playground" and "Recreation, noncommercial" are both allowed uses under the Gateway zoning proposed for that part of the site and therefore, non-public recreation fields can be developed there, but as with other non-public land on the site, it should be communicated that other uses are allowed on the land as well. [This further recommendation is addressed in Staff Recommendations below.]

#### **STAFF NOTES AND ADDITIONAL RECOMMENDATIONS**

In consideration of public hearing comments and discussion at the July 11<sup>th</sup> Comprehensive and Neighborhood Planning Committees meeting, and after consultation with the City Attorney's Office, Planning staff have some additional revisions to recommend for consideration by the full Planning Commission at its July 28<sup>th</sup> meeting. The main revision proposals from staff are noted below with a brief explanation.

- a) Page 43 Add 'Animal day care' to the use table and identify in which districts it would be allowed. Rationale: "Animal day care' is a new use definition recently added to the city zoning code and should be reflected in the use table for the Ford districts.
- b) Page 58 Limit surface parking lot size and include standards for structured parking design. Rationale: Throughout public and PC presentations on the Ford plan, intent to propose a 20 space maximum for surface lots was communicated, but staff inadvertently missed putting the text into the 5-4-17 DRAFT. It is now added. As for structured parking, ensure that building space for parking has level floors which can be costeffectively converted to other uses in the future if the parking is no longer needed or moves elsewhere. This language was similarly applied to the Snelling-Midway Master Plan.
- c) Page 69 Introduce building width maximum of 500 feet for 'Mixed Residential & Commercial', 'Civic & Institutional', 'Commercial & Employment' and 'Parking Structure' building types. These changes are also noted on the corresponding Building Type pages 76-79. Rationale: to ensure that buildings of this type are consistently scaled to the site's typical block size, even if built on longer blocks than typical to the site, such as in the southeast 'Gateway' zoning district. Add caveat that maximum setback limit only has to apply to 60% of a building façade. Rationale: to allow buildings to have courtyards, corner cut outs or other variations in façade depth for design interest, amenity areas, or publicly visible open space.
- d) Page 69 Reduce lot coverage maximum from 80% to 70% and increase open space coverage from 20% to 25% for 'Mixed Residential & Commercial', 'Civic & Institutional', 'Commercial & Employment' and 'Parking Structure' building types. These changes are also noted on the corresponding Building Type pages 76-79. Rationale: to provide consistent lot coverage and open space standards for all larger building types for ease of future building and lot reuse from one building type to another and to increase open space across the site.
- e) Page 72 Add more descriptive language for Live/Work unit. Rationale: New language provides greater consistency with Saint Paul Zoning Code definition for Live/Work units, while identifying that the Ford zoning districts allow up to 50% GFA to be non-residential use, which is above the general City-wide limit of 30% GFA.

- f) Page 81 Identify intent for broadband capability on the site. *Rationale: Such* infrastructure will be important to the site for the reason noted in the text. A similar statement was included as a condition of approval for the Snelling-Midway Master Plan.
- g) Page 85 Acknowledge need for future study of Ford Parkway right-of-way design. Rationale: The City of Saint Paul Bicycle Plan identifies Ford Parkway on the north edge of the property as an enhanced shared lane that would connect to an in-street separated lane on the Ford Bridge. With a reconfigured right-of-way, there is an opportunity to extend separated lanes east of the bridge and improve connections to the north-south trail on Mississippi River Boulevard. The route of the Riverview corridor may also have implications on the design of the Ford Bridge and connections through and adjacent to the Ford Site.
- h) Pages 34-39, 110-111 and 117 Remove depictions of non-public recreational fields from zoning and land use maps in the Ford Site Zoning and Public Realm Master Plan and revise text describing the use in the open space section to be non-sport specific. [NOTE: These changes are not shown in the revised Plan, but are attached to this memo as an example of what it would look like if it was done in the Plan.] Rationale: The plan maps should not show a pre-supposed, specific land use designation for private land. A range of uses are allowed on private land within each zoning district and until a specific use is advanced through agreement or sale to a specified user, any allowed use under the zoning is a possibility for the land.

**NOTE:** When the committees discussed this item, the example of the Central Corridor plans showing desired areas for future private parks was cited as a possible precedent for showing non-City recreation space in the Ford Plan. Upon consultation with the City Attorney's Office, staff learned that the nature of Small Area and District Plans is more visionary / aspirational in nature, whereas Master Plans are more definitive, indicating a commitment to implementation.

In addition to the above items, there are more minor text changes proposed in the document (also shown in red) which are clarifications or additional information and should be self-explanatory.

#### COMPREHENSIVE AND NEIGHBORHOOD PLANNING COMMITTEES RECOMMENDATION

The committees recommend that the Planning Commission forward the proposed Ford Site Zoning and Public Realm Master Plan with suggested revisions from the committee, amendments to Saint Paul Code of Ordinances Article IX. 66.900. FORD DISTRICTS, and Ford Site Zoning Study to the Mayor and City Council for review and adoption.

#### **MATERIALS FOR REVIEW**

- 1. DRAFT Planning Commission Resolution recommending the proposed Ford Site Zoning and Public Realm Master Plan, amendments to Saint Paul Code of Ordinances Article IX. 66.900. FORD DISTRICTS, and Ford Site Zoning Study to the Mayor and City Council for review and adoption.
- 2. Saint Paul Code of Ordinances Amendments to Article IX, 60.900, Ford Districts
- 3. Proposed Ford Zoning and Public Realm Master Plan Planning Commission webpage
- 4. June 30<sup>th</sup> Ford Public Hearing Comments <u>www.stpaul.gov/fordcomments</u>
- 5. Alternative for Discussion Recreational Fields not shown as designated land use in Plan

# city of saint paul planning commission <u>DRAFT</u> resolution file number date

# RECOMMENDATIONS ON FORD ZONING STUDY AND PUBLIC REALM MASTER PLAN

WHEREAS, pursuant to Sec. 61.801(b) of the Saint Paul Zoning Code, the Saint Paul Planning Commission in Resolution 17-31 initiated the Ford Site Zoning Study and on May 19, 2017 released for public review and hearing the Ford Site Zoning Study and Public Realm Master Plan; and

WHEREAS, consideration of the Ford Zoning Study and Public Realm Master Plan involves three actions, (1) amending Saint Paul Code of Ordinances to establish six new 'Ford' zoning districts under Article IX, 60.900, Ford Districts; (2) rezoning four parcels owned by three property owners in the zoning study area to one of the six new zoning districts; and (3) adopting the Ford Site Zoning and Public Realm Master Plan; and

WHEREAS, the Saint Paul Comprehensive Plan identifies the Ford site area as part of a 'Mixed Use Corridor', described by the Comprehensive Plan as an area along a primary thoroughfare that is well-served by transit and intended to accommodate the highest-density development outside of Downtown; and

WHEREAS, planning to identify a mixed use redevelopment framework for the Ford site has been ongoing for over a decade; and

WHEREAS, a total of fourteen (14) professional studies have been conducted during the decade on behalf of the City, as well as additional studies by Ford Motor Company, to examine redevelopment considerations, including industrial reuse, geotechnical analysis of the tunnel system, open space priorities, sustainable design, stormwater management, traffic impacts, market potential, and financial feasibility; and

WHEREAS, these studies were essential to understanding opportunities for and limitations on the redevelopment of the site – economically, environmentally, socially, and within the context of the community, as well as identifying infrastructure efficiencies, cost-effectiveness, opportunities for environment design and conservation, and how to balance development with the creation of vibrant public spaces; and

WHEREAS, during the decade of planning work, the City undertook an extensive public engagement process, hosting 39 Ford Site Planning Task Force meetings, dozens of large public meetings, over 80 presentations to business, civic and non-profit groups, 18 neighborhood focus group meetings in spring 2017, sent meeting notice and project updates to over 3,500 subscribers on the Ford email list regularly, and was covered extensively in the media: and

WHEREAS, the City kept open lines of communication with Ford Motor Company and Canadian Pacific Railway Company, the primary property owners, throughout the planning process; and

Planning Commission DRAFT Resolution #
Recommendations on Ford Site Zoning Study and Public Realm Master Plan
July 28, 2017
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WHEREAS, a technical advisory committee of City staff from PED, Public Works, Parks and Recreation, Safety and Inspections, Fire, Water, Financial Services, Mayor's Office, City Council, and the public artist worked throughout the planning process to review professional studies and findings, to consider public input, and to discuss options to shape and refine the proposed zoning and public realm master plan; and

WHEREAS, the Ford Site Zoning Study created a zoning framework sensitive to the unique context of the site by introducing six new zoning districts that allow a flexible mix and level of uses that can expand job opportunities, provide more diverse housing options to meet market demand, and support new retail and services for the community; and

WHEREAS, the public realm master plan reconnects the site with the surrounding neighborhood, creates a vibrant, high quality place that is pedestrian- and transit-friendly, preserves over 20% of the site as parks, trails and public open space, and is carefully designed to balance development with public amenities in a manner that is financially viable, technically feasibility and environmentally sustainable; and

WHEREAS, the Saint Paul Planning Commission finds that the goals of the Saint Paul Comprehensive Plan and applicable neighborhood plans are advanced by the zoning recommendations and public realm master plan; and

WHEREAS, on June 30, 2017, the Saint Paul Planning Commission held a public hearing on the Ford Site Zoning and Public Realm Master Plan, notice of which was published in the St. Paul Legal Ledger on June 15, 2017, and held the public record open for written comments until July 3, 2017; and

WHEREAS, the Saint Paul Planning Commission referred the Ford Site Zoning and Public Realm Master Plan and public testimony back to a joint meeting of the Comprehensive and Neighborhood Planning Committees for review and consideration; and

WHEREAS, the Comprehensive and Neighborhood Planning Committees forwarded their recommendation and rationale for amendments to the Saint Paul Zoning Code and adoption of the Ford Site Zoning and Public Realm Master Plan in a July 21, 2017, memorandum to the Saint Paul Planning Commission; and

WHEREAS, the Planning Commission considered the public testimony and the recommendations of the Comprehensive Planning Committee;

NOW, THEREFORE, BE IT RESOLVED, that the Planning Commission hereby recommends to the Mayor and City Council adoption of amendments to Saint Paul Code of Ordinances, Article IX. 66.900. Ford Districts, to adopt six new zoning districts with standards and uses; and

NOW, BE IT FURTHER RESOLVED, under the provisions of Minnesota Statutes § 462.357 and § 61.801 of the Legislative Code, that the Planning Commission hereby recommends to the Mayor and City Council the rezoning of certain properties as shown on the maps incorporated into the Ford Site Zoning Study; and

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Recommendations on Ford Site Zoning Study and Public Realm Master Plan
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NOW, BE IT FURTHER RESOLVED, that the Planning Commission recommends the City Council adopt the Ford Site Zoning and Public Realm Master Plan within the redevelopment area, subject to the following conditions:

- 1. Design Standards for the Ford site zoning districts shall be prepared and amended into the zoning districts by August 2018.
- 2. Public realm the street pattern, block layout, and park or open spaces for redevelopment of the subject area should be as shown on the master plan. New public streets or removal of a public street segment, park or open space, or entire block shall be considered a major modification of the master plan and shall require amending the master plan.

#### ARTICLE IX. 66.900. FORD DISTRICTS

Division 1. 66.910. Ford District Intent

#### Sec. 66.911. General intent, F Ford districts.

The Ford districts are designed specifically for the Ford site for use with the *Ford Site Redevelopment and Zoning Master Plan*, which provides additional standards for specific building types and standards to address sustainability objectives. The *Ford Site Redevelopment and Zoning Master Plan* was adopted, and can be amended, by City Council resolution after a public hearing and planning commission review and recommendation. The Ford districts are intended to provide for a desired mix of residential, civic and commercial uses across the site, and a mix of housing styles, types and sizes to accommodate households of varying sizes, ages and incomes.

#### Sec. 66.912. Intent, F1 river residential district.

The F1 river residential district provides for high quality, large home structures with two to six dwelling units each and rear carriage house dwellings with an additional one to two dwelling units in a combined garage structure. The district is characterized by deep setbacks from Mississippi River Boulevard, consistent with the historic form of homes along the parkway.

#### Sec. 66.913. Intent, F2 residential mixed low district.

The F2 residential mixed-use low-rise district provides for compact, pedestrian-oriented residential with at least seventy (70) percent of the development acres dedicated for townhouse use. The district provides for some low-scale multi-family structures, live-work units, and limited neighborhood serving retail, office, civic and institutional uses.

#### Sec. 66.914. Intent, F3 residential mixed mid district.

The F3 residential mixed-use mid-rise district provides for a more extensive range of multi-family residential and congregate living types, as well as transit-oriented mixed-use development with retail, office, civic and institutional uses. A variety of housing and land uses within each block is encouraged to provide visual interest and convenient pedestrian access to amenities and services.

#### Sec. 66.915. Intent, F4 residential mixed high district.

The F4 mixed-use high-rise district provides for high density, transit-supportive, pedestrian-oriented multi-family residential and congregate living; with integrated retail, office, civic and institutional uses; and with the scale and mass of buildings moderated by use of vegetative buffers, step backs on upper floors, courtyards, and architectural features that break up the mass of facades.

#### Sec. 66.916. Intent, F5 business mixed district.

The F5 business mixed district provides for a variety of retail, dining, office and service establishments, with buildings oriented to public right-of-way, ground floor activity that transitions between outdoor public spaces and indoor uses. Multi-family residential use may be incorporated on upper floors.

#### Sec. 66.917. Intent, F6 gateway district.

The F6 gateway district is intended to serve as the main entrance and economic heart of the Ford redevelopment site. The district provides for a variety of business and office uses independently or in combination with retail and service establishments. Civic and educational uses may also be present. The district is focused on employment activity and complementary work force services.

#### Division 2. 66.920. Ford District Uses

#### Sec. 66.921. Ford district use table.

Table 66.921, Ford district uses, lists all permitted and conditional uses in the F1-F6 Ford districts, and notes applicable development standards and conditions.

Table 66.921. Ford District Uses

Use	F1	F2	F3	F4	F5	F6	Definition (d) Standards (s)
Residential Uses							
Dwellings							
Two-family dwelling	P						(d)
Multiple-family dwelling	P	P	P	P	P		(d)
Carriage house dwelling	P	P					(d)
Mixed Commercial-Residential Uses				ı			
Home occupation	P	P	P	P	P	P	(d), (s)
Live-work unit		P	P	P	P	P	(d), (s)
Mixed residential and commercial use		P	P	P	P	P	. ,, , ,
Congregate Living	I		I	I			l.
Adult care home		P	P	P	P	P	(d)
Community residential facility, licensed correctional		C	C	C	-	<u> </u>	(d), (s)
Dormitory				P	P		(d), (s)
Emergency housing facility		С	С	C			(d), (s)
Foster home	P	P	P	P			(d)
Shareable housing		P	P	P	P		(d)
Shelter for battered persons	P/C	P/C	P/C	P/C	P/C		(d), (s)
Sober house	P/C	P/C	P/C	P/C	P/C		(d), (s)
Supportive housing facility	P/C	P	P	P	P		(d), (s)
Civic and Institutional Uses	17.0			_			(0), (5)
Club, fraternal organization, lodge hall		P	P	P	P		(d)
College, university, specialty school		P	P	P	P	P	(d), (s)
Day care, primary and secondary school		P	P	P	P	P	(d), (s)
Public library, museum	P	P	P	P	P	P	(4), (5)
Public and private park, playground	P	P	P	P	P	P	
Recreation, noncommercial		P	P	P	P	P	(d)
Religious institution, place of worship		P	P	P	P	P	(d)
Public Services and Utilities	l l	_					(0)
Antenna, cellular telephone	P/C	P/C	P/C	P/C	P/C	P/C	(d), (s)
Electric transformer or gas regulator substation	1,0	170	P	P	P	P	(s)
Municipal building or use		P	P	P	P	P	(s)
Public utility heating or cooling plant		P	P	P	P	P	(5)
Utility or public service building	P	P	P	P	P	P	(d), (s)
Commercial Uses	-						(0), (5)
Office, Retail and Service Uses							
General office, studio		P	P	P	P	P	(d)
General retail		P	P	P	P	P	(d)
Service business, general		P	P	P	P	P	(d)
Service business, general Service business with showroom or workshop		P	P	P	P	P	(d)
Animal day care		1	1	1	P	P	(d), (s)
Business sales and services			<u> </u>		P	P	(d)
Dry cleaning, commercial laundry			P	P	P	1	(u)
Farmers market		P/C	P/C	P/C	P/C	P/C	(d), (s)
Garden center, outdoor		1/0	P	P	P	P	(d)
Greenhouse			1	P	P	P	(d), (s)
Hospital			<u> </u>	P	P	P	(d)
Mortuary, funeral home				P	P	P	(u)
Outdoor commercial use	-		P/C	P/C	P/C	P/C	(d), (s)

Use	F1	F2	F3	F4	F5	<b>F6</b>	Definition (d)
						D	Standards (s)
Package delivery service					P	P	(d)
Small engine repair, automotive bench work		- D	- P	- P	P	P	(1) ()
Veterinary clinic		P	P	P	P	P	(d), (s)
Food and Beverages							T
Bar				P/C	P/C	P/C	(d), (s)
Brew on premises store			P	P	P	P	(d), (s)
Coffee shop, tea house		P	P	P	P	P	(d)
Restaurant		P	P	P	P	P	(d), (s)
Restaurant, fast-food					P/C	P/C	(d), (s)
Commercial Recreation, Entertainment and Lodging							
Bed and breakfast residence	P						(d), (s)
Health/sports club			P	P	P	P	(d)
Hotel, inn			P	P	P	P	
Indoor recreation			С	С	С	С	(d), (s)
Reception hall/rental hall			С	С	P	P	
Short-term rental dwelling unit	P/C	P/C	P/C	P/C	P/C	P/C	(d), (s)
Theater, assembly hall, concert hall			С	С	С	С	
Automobile Services	•						•
Auto convenience market					С		(d), (s)
Auto service station, auto specialty store					С		(d), (s)
Auto repair station					С		(d), (s)
Auto sales, indoor					С		
Car wash, detailing					С		(s)
Parking Facilities	II.						
Parking facility, commercial		С	С	С	С	С	(d)
Transportation	l l						
Bus or rail passenger station				С	С	С	
Railroad right-of-way	С	С	С	C	P	P	(s)
Limited Production, Processing and Storage							(~)
Agriculture	P	P	P	P	P	P	(d), (s)
Brewery, craft	1	P	P	P	P	P	(d), (s)
Distillery, craft		1	P	P	P	P	(d)
Finishing shop			-	1	P	P	(d), (s)
Limited production and processing			P	P	P	P	(d), (s)
Mail order house			P	P	P	P	(4), (8)
Printing and publishing			P	P	P	P	
Recycling drop-off station			1	1	C	C	(d), (s)
Research, development and testing laboratory					P	P	(4), (5)
Wholesale establishment					P	1	(d)
Winery, craft		P	P	P	P	P	(d)
Accessory Uses		1	1	1	1	1	(u)
Accessory Uses Accessory use	D	D	D	P	P	D	(d) (a)
Accessory use	P	P	P	Р	ľ	P	(d), (s)

P – Permitted use C – Conditional use requiring a conditional use permit

Notes to table 66.921, Ford district uses:

- (d) Definition for the use in Chapter 65, Land Use Definitions and Development Standards.
- (s) Standards and conditions for the use in Chapter 65, Land Use Definitions and Development Standards.

#### Sec. 66.922. Ford district required mix of uses.

The Ford Site Redevelopment and Zoning Master Plan, Chapter 4.4, requires a specific mix of residential, commercial, employment, and civic/institutional uses within each of the six (6) Ford districts. There are

minimum and maximum requirements for these four land use types as a percentage of total floor area constructed within a district, including all current and planned construction for the district.

#### Division 3. 66.930. Ford District Dimensional Standards

#### Sec. 66.931. Ford district dimensional standards table.

Table 66.931, Ford district dimensional standards, sets forth density and dimensional standards that are specific to Ford districts. These standards are in addition to the provisions of chapter 63, regulations of general applicability. Where an existing building does not conform to the following requirements, the building may be expanded without fully meeting the requirements as long as the expansion does not increase the nonconformity.

Table 66.931. Ford District Dimensional Standards

Building Type by Zoning District (a)	Floor Area Ratio Min Max	Lot Width Min.	Building Width Max.	`	g Height eet) Max.	Max. Lot Coverage by	Building S (feet) ROW(c)	
	With. With	(feet)	(feet)	win.	wax.	Buildings	MinMax.	Min.
F1 river residential								
Multi-unit home	0.25 - 1.5	80	60	20	48	30%	10 - 40 (d)	10
Carriage house	0.25 - 1.5	n/a	60	n/a	30	30%	10 - 20	6
Nonresidential or mixed	0.25 - 1.5	n/a	500 n/a	20	48	70% <del>80%</del>	5 - 30 (d)	6
F2 residential mixed low								
Townhouse, rowhouse	1.0 - 2.0	30	150	30	55	50%	10 - 20	6 (e)
Multifamily low	1.0 - 2.0	60	200	30	55	70%	10 - 20	10 (e)
Carriage house	1.0 - 2.0	n/a	60	n/a	30	50%	10 - 20	6 (e)
Live/work	1.0 - 2.0	30	150	30	55	70%	5 - 20	6 (e)
Nonresidential or mixed	1.0 - 2.0	n/a	500 n/a	30	55	70% 80%	5 - 15	6 (e)
F3 residential mixed mid								
Townhouse, rowhouse	2.0 - 4.0	30	150	40	75	50%	10 - 20	6 (e)
Multifamily	2.0 - 4.0	60	n/a	40	75	70%	10 - 20	6 (e)
Live/work	2.0 - 4.0	30	150	40	75	70%	5 - 20	6 (e)
Nonresidential or mixed	2.0 - 4.0	n/a	500 n/a	40	75	70% 80%	5 - 15	6 (e)
F4 residential mixed high								
Townhouse, rowhouse	3.0 - 6.0	30	150	48	110	50%	10 - 20	6 (e)
Multifamily medium	3.0 - 6.0	n/a	n/a	48	110	70%	10 - 20	6 (e)
Live/work	3.0 - 6.0	30	150	48	110	70%	5 - 20	6 (e)
Nonresidential or mixed	3.0 - 6.0	n/a	500 n/a	48	110	70% <del>80%</del>	5 - 15	6 (e)
F5 business mixed								
Nonresidential or mixed	2.0 - 4.0	n/a	500 n/a	40	75	70% <del>80%</del>	5 - 15	6 (e)
F6 gateway								
Nonresidential or mixed	1.0 - 3.0	n/a	500 n/a	30	65	70% 80%	5 – 15	6 (e)

Min.-Minimum

Max. - Maximum

ROW – Public Right-of-Way

n/a - not applicable

Notes to table 66.331, Ford district dimensional standards:

- (a) Building types are described and defined in Chapter 5 of the *Ford Site Redevelopment and Zoning Master Plan*.
- (b) Building setback is the horizontal distance between a lot line and the nearest above-grade point of a building. An interior setback is measured from an interior lot line, which is a lot line separating a lot from another lot or lots. A public right-of-way (ROW) setback is measured from a lot line that is not an interior lot line: a lot line separating a lot from a street, alley, or public way.

- (c) Maximum building setback limit shall apply to at least 60% of the building façade along the right-of-way.
- (de) Buildings shall be setback a minimum of thirty (30) feet from a lot line separating a lot from Mississippi River Boulevard.
- (ed) No setback is required for building walls containing no windows or other openings when the wall meets the fire resistance standards of the Minnesota State Building Code and there is a Common Interest Community (CIC) or recorded maintenance easement that covers the affected properties.

#### Division 4. 66.940. Ford District Development Standards

#### Sec. 66.941. Ford district accessory building standards.

In addition to the standards for accessory buildings in Section 63.501, accessory buildings in Ford districts shall be subject to the following regulations:

- (a) Accessory buildings shall meet required public right-of-way setback requirements for a carriage house in F1-F2 districts, and for the principal building on the lot in F3-F6 districts.
- (b) The *Ford Site Redevelopment and Zoning Master Plan*, Chapter 5, regulates the number of accessory buildings permitted on a lot by building type.

#### Sec. 66.942. Ford district vehicle parking standards.

Off-street parking shall be provided as follows. These requirement supersede the parking requirements in section 63.207.

Table 66.942. Vehicle Parking Requirements by Use

Land Use	Minimum Number of Parking Spaces	Maximum Number of Parking Spaces
Residential, dwellings	0.75 space per dwelling unit	2 spaces per dwelling unit
Residential, congregate living	0.25 space per bedroom	1 space per bedroom
Nonresidential	1 space per 600 square feet GFA	1 space per 400 square feet GFA

GFA - Gross Floor Area

The Ford Site Redevelopment and Zoning Master Plan, Chapter 4.6, sets vehicle parking facility standards that are in addition to the parking facility standards in chapter 63.

#### Sec. 66.943. Ford district bicycle parking standards.

Bicycle parking and related facilities shall be provided as follows:

Table 66.943. Bicycle Parking Requirements by Use

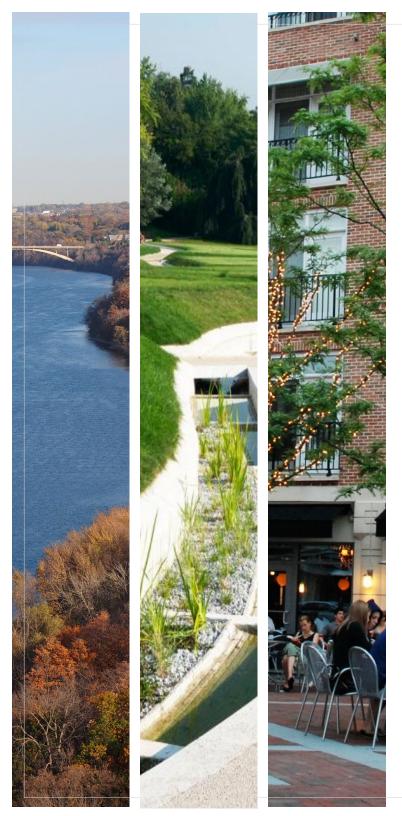
Land Use	Minimum Number of Bicycle Parking Spaces
Residential, dwellings	1 space per dwelling unit
Residential, congregate living	1 space per bedroom
Education	1 space per 5 students
Recreation	1 space per 300 square feet of facility land or gross floor area
General civic and commercial	1 space per 5000 square feet gross floor area
Production and processing	1 space per 15,000 square feet gross floor area

Office and production/processing uses shall provide one (1) shower per fifty (50) employees.

The Ford Site Redevelopment and Zoning Master Plan, Chapter 4.6, sets bicycle parking standards that are in addition to the parking facility standards in chapter 63.

#### Sec. 66.945. Ford district general development standards.

- (a) The design standards in section 66.343 for the T3 traditional neighborhood district apply in all Ford districts.
- (b) The *Ford Site Redevelopment and Zoning Master Plan*, Chapter 4.6, sets standards for vegetation and landscaping, lighting, solar energy, and roofing that are in addition to chapter 63 standards.





Ford Site Zoning and Public Realm Master Plan

DRAFT (7/21/2017)
For review by the
Saint Paul Planning Commission



# **ACKNOWLEDGEMENTS**

# City of Saint Paul

Mayor Christopher B. Coleman Deputy Mayor Kristin Beckmann Councilmember Chris Tolbert, Ward 3

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Anne Weber, Civil Engineer IV
Jenny Wolfe, Debt Administrator

# Ford Site Planning Task Force

#### Members serving through 2017:

William Klein, Co-Chair

Kyle Makarios, Co-Chair

Tony Schertler, Co-Chair

Peter Armstrong

James Bricher

Ronnie Brooks

Rob Cory

David Drach

Charles Hathaway

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Morgan Tamsky

Bruce Valen

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Richard Broderick

Anthony Desnick

Terri Dooher Fleming

Carol Faricy

Jay Gardner

Lance Neckar

Scott Malcolm

Dennis Rosemark

Matthew Schuerger

Dave Sellergren

Stuart Simek

# Community and Partners

- Ford Motor Company -- particularly project leads from Ford Land and from the Ford Environmental Quality Office
- All members of the public that attended meetings, provided input and shared ideas
- The Highland District Council for hosting, supporting and facilitating numerous meetings and conversations with the Highland community
- Dozens of consultants that assisted us on over a dozen studies regarding aspects of the Ford Site redevelopment
- Dozens of organizations, agencies and foundations that provided staff, expertise and funding to support the Ford site planning work across a wide range of topics and issues



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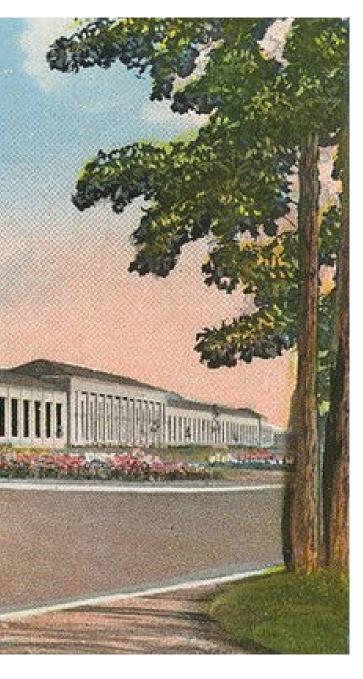
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# Introduction

The Ford Site Zoning and Master Plan provides a framework to guide mixed-use redevelopment of the former Ford Motor Company assembly plant and the adjacent Canadian Pacific railyard. This plan is the culmination of a decade of planning for the site. The City of Saint Paul has been in active dialogue with Ford Motor Company and Canadian Pacific Railway representatives during those years, to examine all ideas for property reuse. There has been extensive dialogue with members of the public, with partner agencies at the regional, state and federal level, with interested organizations, and with real estate and development professionals. The City has collaborated with foundation and professional consultants to undertake a series of studies examining site reuse priorities and their technical and financial feasibility. The decade of research and dialogue has led to the firm conclusion that the site is best repositioned as a mixed-use urban neighborhood that advances economic, environmental and social sustainability.

This document – The Ford Site Zoning and Public Realm Master Plan – plays a large role in guiding the future of the site. It provides the regulatory zoning standards for future uses, building form, and public realm design, as well as the layout and general design for future public right-of-ways, parks and open space, and stormwater management. This document complements the City of Saint Paul Zoning Code by providing site-specific standards in six new zoning districts for the Ford site, while relying on the existing City zoning for more general requirements not referenced herein.

It is anticipated that this document may be updated over the course of the site's final planning and expected decade plus build out, in order to accurately reflect evolving conditions, opportunities and priorities for site redevelopment. Updates to this document must be reviewed and adopted as a plan amendment by the Saint Paul Planning Commission and the Saint Paul City Council.



# 1.1 Opportunity

The Ford site offers an unparalleled opportunity for the future of Saint Paul and for the entire region. Rarely does a city have the chance to redevelop 135 acres of land on the banks of one of the world's major rivers and in the heart of a thriving neighborhood and commercial area. It is a unique opportunity and one that requires the utmost care and commitment to ensure that a framework is in place for future development to take place as envisioned.

The Ford site should reemerge as a connected, livable and sustainable site that will serve as a world-wide model for a 21st Century Community. It will look to the future with clean technologies and high quality design for energy, buildings and infrastructure. The redeveloped site will support walking, biking and transit, and provide services, jobs and activities that every generation can enjoy. The site can be redeveloped in a way that respects the history and context of the neighborhood, while designing a thriving community that significantly lowers its impact on the environment. A redeveloped Ford site can demonstrate that residents, employers, workers, and visitors can enjoy all the amenities and comforts of modern living while using much less energy, producing clean energy on site, reducing waste, reducing and treating storm-water runoff, restoring a natural ecosystem, and providing an infrastructure system that reduces vehicle trips and encourages walking, biking and transit.

The Ford Site is five to seven miles from MSP's International Airport, the downtowns of Saint Paul and Minneapolis and the Mall of America. It is in the heart of a thriving business community along the Mississippi River, is near dozens of educational and research institutions, and has transit and nearby rail corridor access. Employers can draw from a pool of more than 280,500 employees within a 7-mile and 22-minute commute. This active, amenity-rich area will attract high-quality employers and employees, in addition to residents and visitors. Returning a strong base of employment to the site is a priority for the City.

Housing on the site will expand the range of living options available in the neighborhood and the city, with a focus on a range of townhomes, carriage houses, multi-family buildings in different sizes and styles, senior living, and residential above retail and services. The housing will vary in size and price, meeting the needs of many people in different stages in their lives.







Open space, vegetation and public places will form the backbone of the community, bringing people together through nature, for recreation and at community events. A network of parks, trails, and attractive public streets will provide the fabric of places and spaces for people.

Transportation will focus on all modes of travel – walking, biking, public transit and personal vehicles. The site will be designed to balance movement with safety and encourage trips to be made to, from and around the site without a car. Vehicles will be accommodated with streets and parking, but not given primacy over other forms of travel, safety and livability. All elements together provide a site that is environmentally, socially and economically sustainable. A bold vision for redevelopment of the Ford site can and will be realized, making it a model of urban living that people from around the world can emulate.





# 1.2 Site History

Long before an auto assembly plant came to the banks of the Mississippi River in Saint Paul, Minnesota, the land was oak savanna wilderness, visited by transitory settlements of the Santee Dakota, who lived in the wider area. In the early 1800s, European and American fur traders arrived in the region, followed by Western settlers and the United States government, displacing the native inhabitants. The property now known as the "Ford site" became farm land for the new settlers, while early city plans identified it for platting into a new residential and commercial area of Saint Paul. However, before that development could occur, Henry Ford purchased the land.

Ford Motor Company's long history in Minnesota began in 1912 in a small, converted warehouse in Minneapolis where 100 employees assembled Ford Model Ts with hand tools. As technology evolved, Henry Ford became interested in a much grander vision for production in the Twin Cities.

Working with industrial architect Albert Kahn, Henry Ford began plans for a new, single-level assembly plant, hydropower facility and a steam plant on farm land in Saint Paul. In 1925, Ford Motor Company opened the Twin Cities Assembly Plant (originally for Model T automobiles) in Highland Park.

Over the years, the plant manufactured a series of products, including the Model T, armored cars and light tanks during WWII, the Sportsman convertible, the Galaxie, the LTD, and the Ford Ranger pickup truck. The plant employed up to 2,100 at its peak, providing well-paid jobs to tens of thousands of people over its lifetime. After 84 years in operation, Ford Motor Company closed the plant in 2011 as part of its national restructuring plan.













# 1.3 Moving to Redevelopment

Ford has worked diligently since 2012 to prepare its 122-acre former assembly plant parcel the site for redevelopment. All buildings and foundations were removed, a full environmental investigation was conducted, and Ford is remediating the property for the envisioned range of uses under close observation by the Minnesota Pollution Control Agency. Ford Motor Company's 13-acre parcel adjacent to the river is also being evaluated by Ford for possible sale and reuse, but due its unique circumstances and strictly limited reuse potential under the Critical Area overlay rules, it was not included by the City in the larger site planning and zoning study. Review of potential plans for the river parcel will occur independently.

When clean-up is near completion, likely in 2018 or early 2019, Ford will put the site on the market for sale to a Master Developer. It is anticipated that a Master Developer will buy the property and do detailed planning for the site, based on the framework provided by the City zoning and the Ford Site Master Plan. It is expected that the master developer may identify new opportunities or considerations for site redevelopment and apply for amendments to the Ford Site Zoning and Public Realm Master Plan. The Planning Commission and City Council will consider and decide whether to adopt such amendments.

The final developer master plan is likely to have new ideas not envisioned in the City adopted zoning and site master plan, in which case, the City Council may approve amendments to the City plans following community review of the proposed changes.

Given the scale of the site, environmental review such as an Alternative Urban Areawide Review and detailed traffic study, will be required on the Master Developer's detailed plan. Once environmental review is complete and possible refinements made to the City zoning and Master Plan, the City Council can adopt the detailed development plan and plat for the property. Construction of streets, sewer, and other infrastructure can then begin, with 2020 or 2021 as the very soonest opportunity.

Development of buildings on the site will follow the start of infrastructure and proceed in phases across the site, with total site build out expected to take 12-20 years.





# 1.4 Genesis of the Plan

## Community Engagement

The City began an extensive engagement effort in 2007, seeking people's vision and priorities for the site. Planning for the Ford site has involved an extensive public process.

Since 2007 there have been...

- Over 40 public meetings
- Over 1,300 different people have attended meetings
- Over 50 articles in print, radio and television media
- Over 50 presentations to business, civic, and non-profit groups
- Student group input and participation from Horace Mann Elementary, Highland Catholic School, St. Paul Academy, and the Humphrey School of Public Affairs

The City received thousands of ideas and suggestions over the past ten years and these shaped the backbone of the proposed redevelopment framework for the Saint Paul Ford site. In 2015, the City hosted eight large public meetings to review the key principles and to receive input on how to advance them.

The big messages heard were:

- Weave the site into the neighborhood with good connections and amenities
- Make it a safe and welcoming place for all ages and incomes to live, work and visit
- · Provide a wide range of housing options to expand choices in the neighborhood and the city
- Minimize traffic impacts and do not overburden surrounding streets
- · Provide connections to the river and networks for walking and biking
- Create a great water feature through the site and down to Hidden Falls
- Provide civic space and parks for special events, markets, art, recreation, and leisure









This input was used to further refine priorities for the site and shaped the concept zoning and public realm plan released publicly in November 2016.

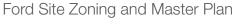
Since November 2016, there have been...

- Seven community and 12 stakeholder meetings on the concept plan
- Over 400 people attended a Ford meeting for the first time

The City has received hundreds of comments on the concept plans since November. The comments reflect a wide range of opinions and sentiment regarding the site's future. In general, people were supportive of the concept plan layout for streets, trails, water features and parks. Responses to the zoning concept have been more varied. Some people embrace the type and scale of redevelopment presented, some people want to see a more intense level of development, and some people want to site to look like Highland does today. For this latter group, concerns have focused on the scale of buildings, the number of people that will live and work there, traffic, and the amount of open space.

Determining the nature of future development at the site and the mix of uses is critical and is dependent on many important considerations that involve both community sentiment and broader trends and issues related to housing needs, urban growth, real estate economics and vibrant and livable neighborhoods for people of all ages and backgrounds.





### **Partnerships**

The City of Saint Paul had invaluable assistance from key partners at the local, regional, state and Federal level, as well as businesses, civic groups and non-profits. These partners offered technical advice, identified policy considerations, provided study funding, and pushed us to think more broadly. A number of key funders were instrumental in supporting the professional studies and expert input.























East Metro

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#### Studies

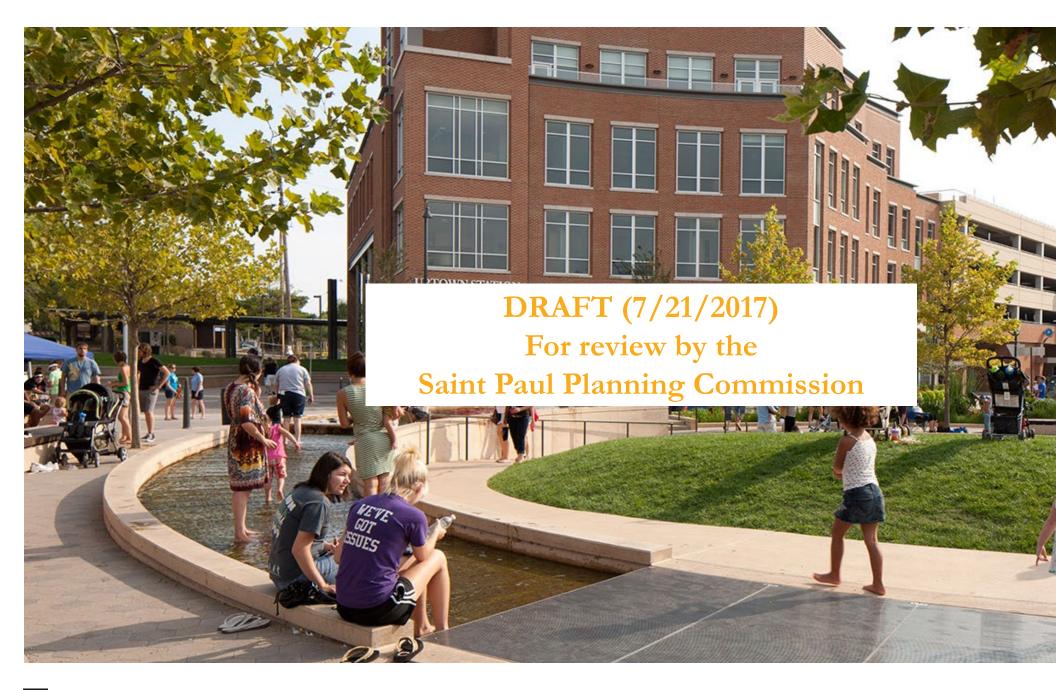
Since 2006, when Ford Motor Company announced its intent to close the plant, the City of Saint Paul has been working actively to plan for the site's future. A variety of studies by the City have looked at a wide range of topics, such as industrial reuse potential, geotechnical analysis of the tunnels under the site, open space priorities, sustainable design, stormwater management and traffic impacts.

The studies were essential to understand opportunities for and limitations on site redevelopment -- economically, environmentally, socially, and within the context of the community. The studies identified infrastructure efficiencies, cost-effectiveness, opportunities for environment design and conservation, and how to strike a balance between development and the creation of great public spaces and places.

- 1. Ford Site Transportation Study (2016)
- 2. Aquifer Thermal Energy Storage (ATES) Feasibility Study (2016)
- 3. Integration of Rooftop Photovoltaic Systems in St. Paul Ford Site's Redevelopment Plans (2016)
- 4. Sustainable Stormwater Management Study (2016)
- 5. Jobs Strategy Report (2016)
- 6. Sustainable Ford Site Redevelopment A LEED-ND Evaluation (2016)
- 7. Saint Paul Ford Site Energy Study Report (2015)
- 8. Market Analysis for the Ford Site (2015)
- 9. Ford Site Zoning Framework Study (2013)
- 10. The Roadmap to Sustainability for the Saint Paul Ford Site (2011)
- 11. Ford Site Open Space Guidelines (2010)
- 12. Sustainable Stormwater Feasibility Report (2009)
- 13. Green Manufacturing Potential for the Ford Site (2009)
- 14. Vision, Goals and Five Redevelopment Scenarios (2007)



# CHAPTER 2: VISION AND GUIDING PRINCIPLES







### Vision Statement

The redeveloped Ford site will balance economic, social and environmental sustainability in a way that conserves and improves the qualities and characteristics of the unique Highland Park neighborhood and Mississippi River valley in which it sits while advancing the City's economic wealth and community goals, resulting in a forward-thinking 21st Century development.

-Ford Site Planning Task Force, 2007



# **Guiding Principles**



#### Mix of Uses and Activities

- Vibrant place to live, work, and recreate for all people.
- Services and amenities that meet local needs to reduce auto dependency.
- Well-designed urban neighborhood that complements and integrates into the Highland Park area and broader community.
- Mix of traditional and modern building forms, styles and materials.



### Housing Variety

 Range of housing types and affordability that expand choices in the area and in the city.



#### Jobs and Tax Base

- Significant increase in the tax base over time that strengthens surrounding property values.
- Increased regional significance and economic value.
- Range of business and employment opportunities with an emphasis on family supporting jobs.







### Energy and Sustainability

- Regional, national, and global model for sustainable planning, design, and day-to-day living that protects our air, water and natural resources for future generations.
- Locally generated power from an integrated, renewable, site-based energy system.
- Best technologies in infrastructure and buildings to save money, increase efficiency, and reduce impacts on the environment.



### Transportation Choice

- Mix and density of activities to support transit through and around the site.
- Interconnected system of streets, bikeways, and walkways that is safe and accessible for people of various ages and abilities.
- Urban design and site layout to reduce auto trips and manage traffic impacts.

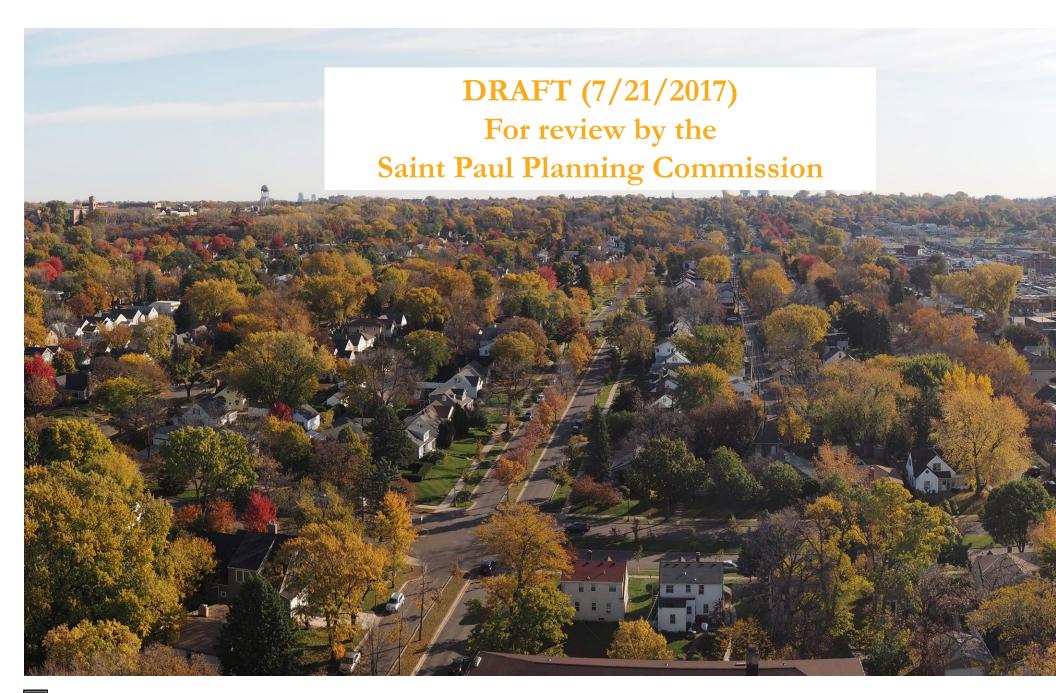


#### Parks and Amenities

- Natural spaces and active places for people to play, relax, and interact, including the Ford Little League fields.
- Greenery, public art, and cultural activities to create an attractive and vibrant community.
- Reflect the heritage of the Ford plant and its employees through design and amenities.
- Strong connections to the Mississippi River, within the site and to the neighborhood.









## 3.1 Community Context

Naturally bordered by the scenic and winding Mississippi River, Highland Park is centrally located in the Twin Cities. The Village, a shopping district with numerous shops, restaurants, and a movie theater, is in the heart of the Highland Park neighborhood. Households in the area enjoy the variety of schools, services, tree-lined streets, a community center and library, recreational opportunities like golf, ball fields and playgrounds, and the proximity of the Mississippi River valley, lined with trails and natural parks.

Highland Park was farm land on the western edge of Saint Paul until the 1920s, when the construction of the Intercity bridge between Saint Paul and Minneapolis and the arrival of the Ford Plant in 1924 sparked development. The first major residential projects were the Highland Manor and the Highland Village apartments which opened in October of 1939 at the intersection of Cleveland and Ford Parkway. Businesses grew up around the intersection and spread down Ford Parkway and Cleveland Avenue. With the growth of the business community came the housing boom for most of Highland Park.

Highland Park consists of many single-family houses dating from the 1920s and 1930s in many period-revival styles as well as many homes from the 1950s and later in ranch, split-level, and rambler styles. Apartments and condominiums are primarily sprinkled around the Village area and in the West 7th Street-Shepard Road area.

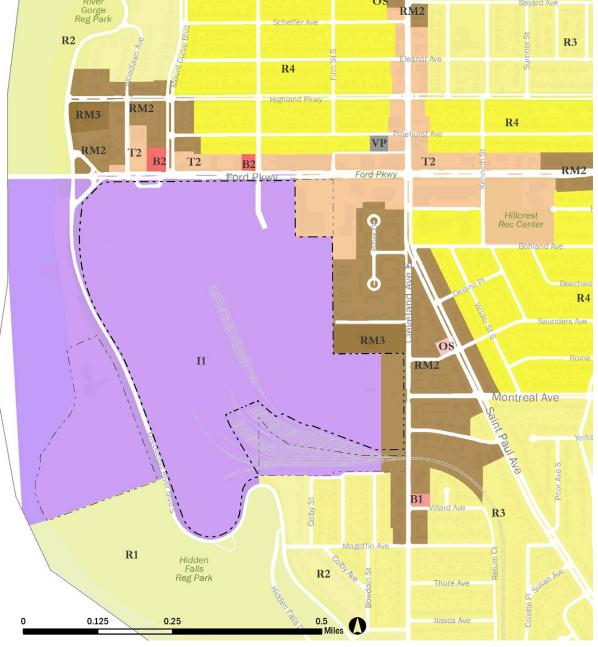
Highland Park today is one of the City's wealthier neighborhoods, with a population demographic that tends to be better educated, less racially diverse, and somewhat older than the population of Saint Paul as a whole.



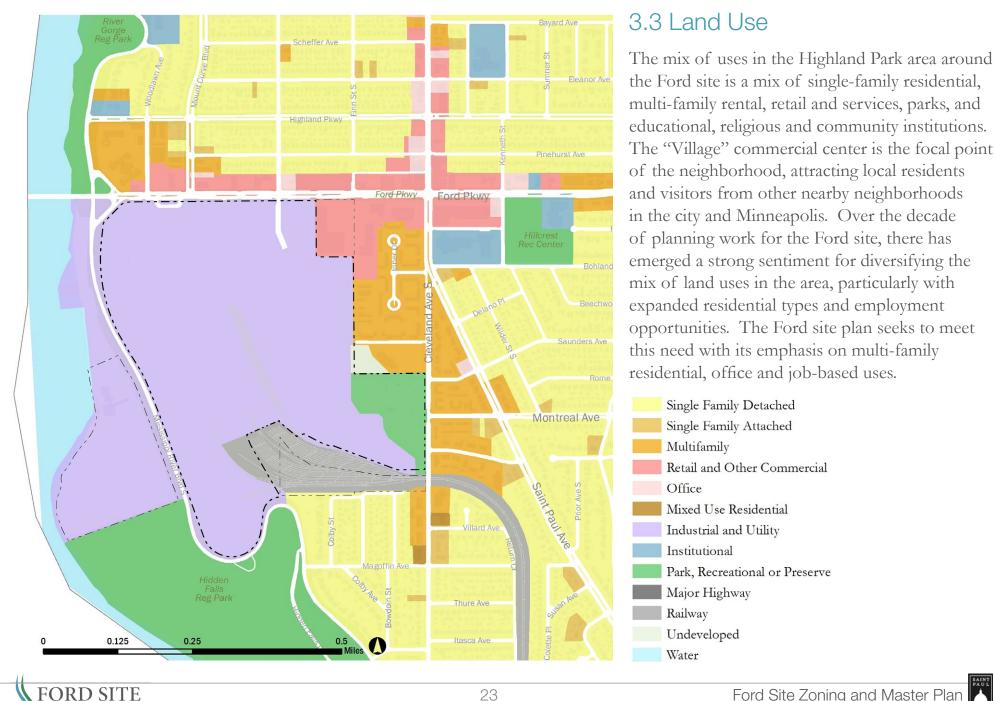
### 3.2 Current Zoning

Existing zoning around the Ford site is a mix of single-family residential, multi-family residential, business, and traditional neighborhood mixeduse. The zoning for the Ford site properties is I1 Light Industrial, which allows a wide range of industrial uses and warehousing, most retail, commercial, office and educational uses, as well as some types of congregate living and multi-family housing above commercial. While expansive, I1 zoning is inconsistent with the future of the site, which has a very limited industrial market, a soft retail market, and an expansive residential market.











A 21st Century Community

## 3.4 Airport Zones

The proximity of the Ford site to the Minneapolis St. Paul International Airport means that portions of the site fall within airport restriction zones for land use and height, as described below.

#### Horizontal Surface Zone

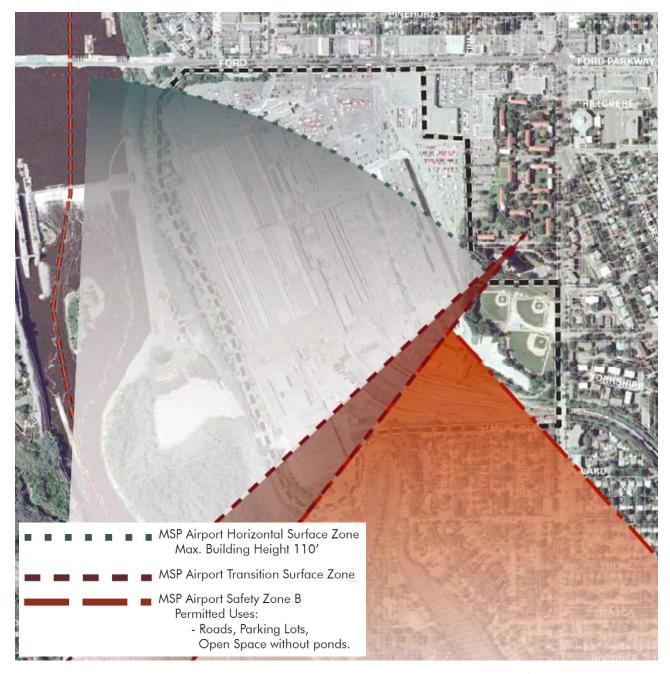
Within the boundaries of the gray cone on the map, there is a maximum building height of 110'.\*

### Safety Zoning B

Within the boundaries of Safety Zone B, the area indicated by the large red triangle on the southern edge of the site, the following land uses are NOT allowed\*:

Churches, hospitals, schools, theaters, stadiums, hotels, motels, trailer courts, campgrounds, and other places of frequent public or semi-public assembly, and ponds.

\*Source: MSP Long Term Comprehensive Plan Update Metropolitan Airports Commission, CHAPTER 6: LAND USE COMPATIBILITY 6.3.3









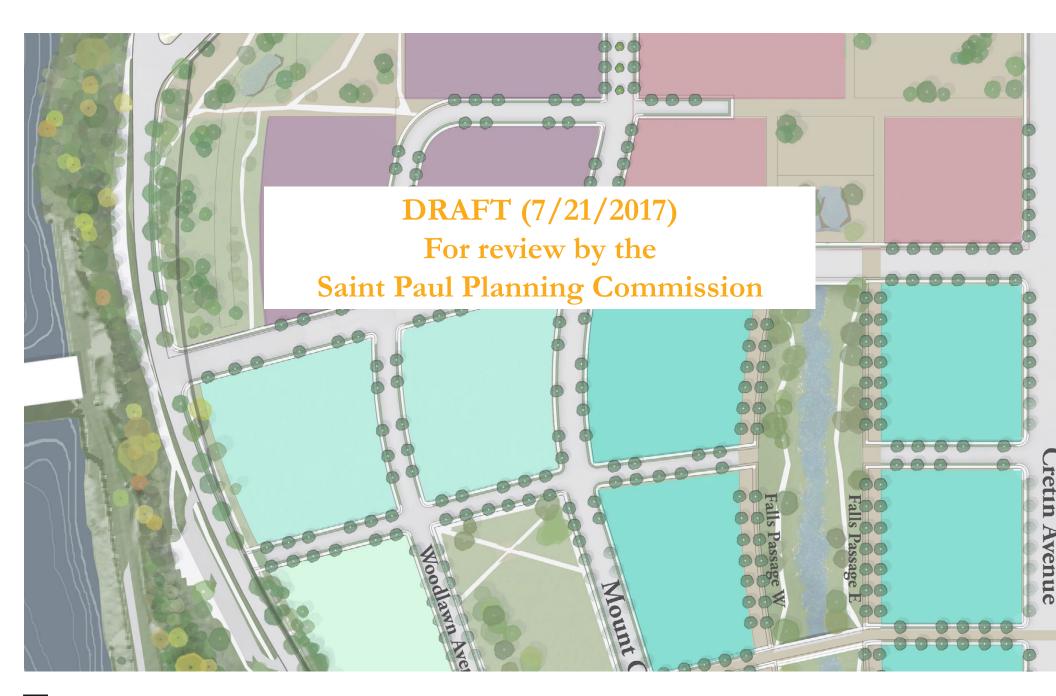
### 3.5 Critical Area

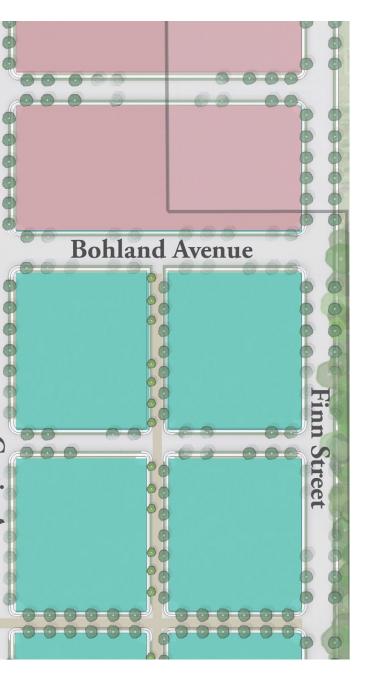
The Mississippi River Corridor Critical Area (MRCCA) is a land corridor along the Mississippi River in the Twin Cities Metropolitan Area governed by special land development regulations that protect and preserve the unique natural, recreational, transportation, and cultural features of this section of the Mississippi River. It comprises 72 miles of river and 54,000 acres of surrounding land in 30 local jurisdictions. The MRCCA protects these resources through local governments' land use plans and zoning ordinances that regulate structure placement, height, vegetation clearing, land alteration, and subdivision of land, in a manner consistent with the administrative provisions, districts, and standards contained in [State of Minnesotal Executive Order 79-19. The map and general standards below act as overlay zoning for development at the Ford site. Additional detail and guidance on Critical Area regulations are available from the Minnesota Department of Natural Resources.





# CHAPTER 4: ZONING - DISTRICTS & GENERAL STANDARDS





### 4.1 Zoning Overview

Six zoning districts will guide the type and scale of redevelopment at the Ford site. The zoning districts, labeled F1-F6, were designed for the Ford site and modeled closely on the Traditional Neighborhood zoning districts used in other mixed-use areas of the city. The unique characteristics of the Ford districts are context sensitive to the Highland area and Mississippi River valley, provide a desired mix of uses across the site, incorporate design elements that balance larger building scales with open space to maintain a neighborhood feel throughout the site, and feature standards that address sustainability objectives for lighting, roofing, and energy.

This chapter outlines the basic standards for the six zoning districts, including:

- location of the districts
- allowed land uses
- floor area ratios
- · building heights
- lighting
- roofing and solar
- vegetation and open space
- parking for vehicles and bicycles

The next chapter on Building Types and Form Standards provides greater zoning detail, based on the building type. The two chapters are to be used in concert to identify key requirements for lots and buildings on the Ford site. The requirements identified are defining elements of the site zoning, but not exhaustive. The Saint Paul Zoning Code acts as the backdrop of zoning regulations for all elements not specifically referenced within this document and is to be used in tandem throughout the design and review process.



## 4.2 Character & Site Organization

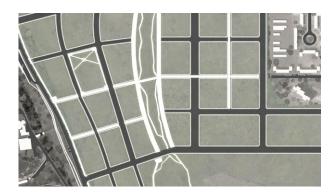
#### General Character

The Ford site will be developed over time to become a vibrant, mixed-use, urban neighborhood with interwoven green space. Development may consistent of a wide range of attached two-family, townhouse, and multi-family buildings, neighborhood retail and services focused in mixed-use nodes and along transit corridors, and office and light-industrial hubs blended into mixed-use areas.



### Streets, Blocks & Movement

The site will be organized on a general grid network of east-west and north-south connections serving vehicles, bicycles and pedestrians. Distinctive to the site will be a network of non-vehicular trails and paths connecting within and through the site, providing convenient and attractive movement options for pedestrians and bicyclists. Block sizes will be similar to those found in other parts of the city, with some variation in shape for transitions to adjacent, curved properties, streets and natural features.



#### Public Realm

The public realm is intended to serve as the connective tissue within the site and to the neighborhood beyond. It is made up of the space between buildings - the right-of-way for streets and trails, the central stormwater spine, and the park spaces. The private space between the right-of-way and buildings will be further defined by design standards to be added to the Ford site zoning districts.







# 4.3 Where Regulations Apply

The regulations in this document apply to different elements of the built environment. The graphic below describes which section applies to which part and generally indicates what is addressed in each section.

Regulation

Addresses

### Zoning District



**Building Type** 



Street Type



Frontage Type\*



Location

Dwelling Units per Building

Setback

Lot Coverage

Right of Way Width Travel Modes



Entries Landscaping Architectural Elements

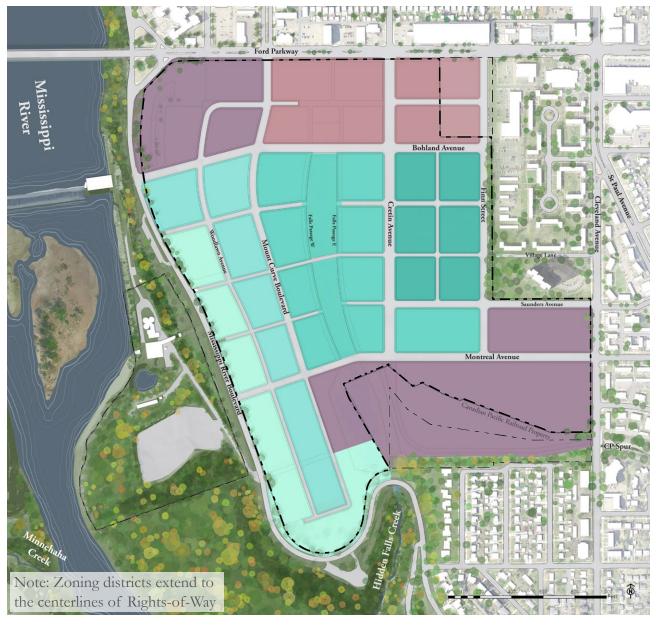


Uses



<sup>\*</sup> Frontage Types to be addressed in forthcoming design standards (2018)

### 4.4 Zoning Districts



The range of land use and development across the districts aims to achieve four overarching goals for the site:

- (a) A mix of uses, including residential, commercial, civic and open space uses in close proximity to one another;
- (b) A mix of housing styles, types and sizes to accommodate households of varying sizes, ages and incomes;
- (c) A system of interconnected streets and paths that offer multiple routes for motorists, pedestrians and bicyclists, and are connected to existing and future streets;
- (d) A system of open space resources and amenities; and incorporation of environmental features into the design of the neighborhood.

#### ZONING DISTRICTS

- River Residential (48' Max)
- Residential Mixed Low (55' Max)
- Residential Mixed Mid (75' Max)
- Residential Mixed High (110' Max)
- Business Mixed (75' Max)
- Gateway (65' Max)





The site shall be developed in a moderately dense, urban manner that reflects the historic pattern of mixed-use urban neighborhoods. Buildings are to be located toward the front of the lot, ranging in height from two (2) to ten (10) stories, and occupying much of the available lot area, with remaining space used efficiently for landscaping, small yard or common areas, parking access, and storage or waste facilities.

Table 4.1 Zoning District Summary

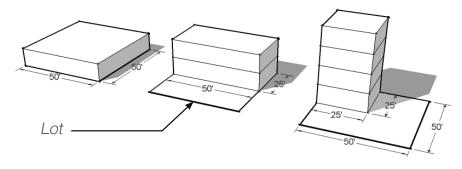
Zoning District	Description	Land Uses	Building Heights	Floor Area Ratio
F1 River Residential	High quality design and residential form that mirrors look of Mississippi River Boulevard	Residential mix of multi-unit homes and carriage houses	20 feet - Minimum 48 feet - Maximum	0.25 - 1.5
F2 Residential Mixed Low	Primarily residential with few business uses; lower density	Residential mix of primarily townhouses with some small multi-family	30 feet - Minimum 55 feet - Maximum	1.0 - 2.0
F3 Residential Mixed Mid	Primarily residential with some business uses; medium density	Predominantly multi-family residential, with limited retail, service and office	40 feet - Minimum 75 feet - Maximum	2.0 - 4.0
F4 Residential Mixed High	Mix of residential and business uses; high density	Predominantly multi-family residential, with limited retail, service and office	48 feet - Minimum 110 feet - Maximum	3.0 - 6.0
F5 Business Mixed	Primarily retail, office and service with some multi-family residential	Primarily retail, service & office with some multi-family	40 feet - Minimum 75 feet - Maximum	2.0 - 4.0
F6 Gateway	Attractive gateways into site, focused on employment with some retail and service	Office, institutional, retail & service	30 feet - Minimum 65 feet - Maximum	1.0 - 3.0



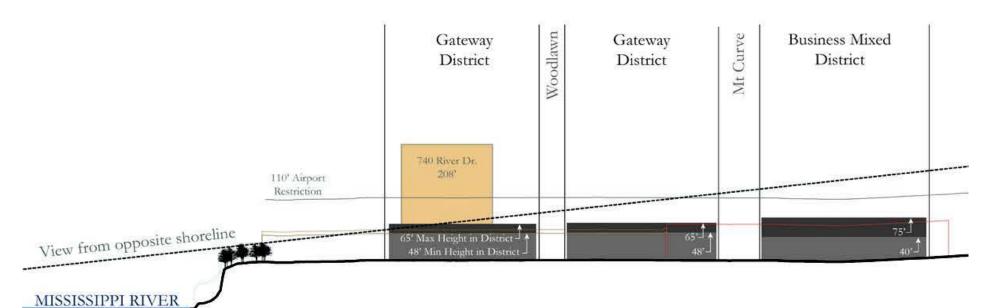
#### Floor Area Ratio (FAR)

Floor Area Ratio, or FAR, is a measurement of the total floor area of all buildings and structures on a zoning lot divided by the area of said lot. FAR establishes the overall mass of buildings on a property and is used to complement to or in place of other lot and building standards such as height, lot coverage and setbacks.

All examples have a Floor Area Ratio of 1.0









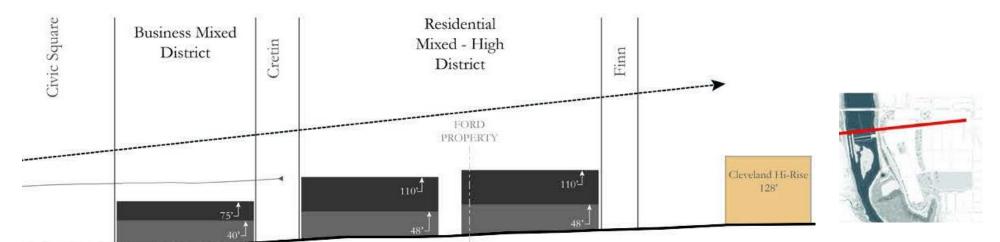


# 120 100 80 60 40 20 F1 F2 F3 F4 F5 F6

Range of Heights by Zoning District

### Building Heights

The location of the site along the bluffs of the Mississippi River and within a mixed-use neighborhood lends itself to a range of building heights. In keeping with the general intent for the site to become a vibrant, moderate-density neighborhood, one-story buildings for primary structures are not permitted. Heights on the site are to will allow a range from two-story to ten-story buildings, tiered across the site starting with lower buildings on the west and moving steadily upward in height to the east. Heights on any single street or block may have some limited variation, within an allowed range as identified by the sitewide height standards. Limited shifts in adjacent building heights will enable each building to preserve rooftop solar access, and utilize the natural grade of the area to provide views opportunities to the west for buildings across the site. Maximum allowed heights on blocks zoned F2 and F6 that lay within the Critical Area overlay may require Conditional Use Permit review to determine if heights above Critical Area standards will be allowed.



This diagram shows the visibility of allowed height ranges if standing on the opposite shoreline and looking towards the site. This analysis assumes a condition of full foliage, per DNR definition of "treeline."





### F1 River Residential District

The River Residential District provides for high quality, large home structures with two to six dwelling units each and rear carriage units with an additional one to two housing units in a combined garage structure. The district is characterized by deep setbacks from Mississippi River Boulevard, consistent with the historic form of residential homes along the corridor.

General Character High quality design and residential form that m					
	look of Mississippi River Boulevard				
Land Uses	Residential mix of multi-unit homes and carriage				
	houses				
Heights					
Minimum	20 feet				
Maximum	48 feet				
FAR	0.25 - 1.5				















### F2 Residential Mixed Low District

The Residential Mixed Low District is intended to provide for compact, pedestrian-oriented residential focused on townhouse blocks with some low-scale multi-family structures and live-work units. Very limited neighborhood serving retail, office, civic or institutional may be tucked into the district. At least 70% of the development acres in the Residential Mixed Low District shall be dedicated for townhouses. The F2-zoned block on Mississippi River Boulevard is located to serve as a transition in use and scale between the River Residential blocks to the south and the Gateway block to the north.

General Character	Primarily residential with few business uses; lower
	density
Land Uses	Predominantly residential; very limited retail, office &
	service
Heights	
Minimum	30 feet
Maximum	55 feet
FAR	1.0 - 2.0













### F3 Residential Mixed Mid District

The Residential Mixed Mid District is intended to provide for more extensive range of multi-family residential types and a variety of congregate living arrangements, as well as transitorientated mixed-use development with retail, office, civic and institutional uses. Variety of housing and land uses within each block is encouraged to provide visual interest and convenient pedestrian access to amenities and services.

General Character	Primarily residential with some business uses; medium
	density
Land Uses	Predominantly residential; some retail, service and
	employment
Heights	
Minimum	40 feet
Maximum	75 feet
FAR	2.0 - 4.0

















# F4 Residential Mixed High District

The Residential Mixed High District provides for high-density, transit-supportive, pedestrian-friendly multi-family residential and congregate living arrangements with integrated retail, office, civic and institutional uses. The scale and mass of buildings shall be moderated with the use of vegetative buffers, step backs on upper floors, courtyards, and architectural features that add variety to the appearance of the facades.

General Character	Mix of residential and business uses, high density
Land Uses	Predominantly residential; some retail, service and
	employment
Heights	
Minimum	48 feet
Maximum	110 feet
FAR	3.0 - 6.0













### F5 Business Mixed District

The Business Mixed District provides for a variety of retail stores, dining, office buildings and service establishments. Buildings will orient to public right-of-ways and provide dynamic, ground floor activity that transitions between the outdoor public spaces and the building uses. Exterior edges will provide attractive vegetation, patios, amenities and public art that enlivens the public realm. Multi-family dwellings may be incorporated on upper floors.

General Character	Primarily retail, office and service with some				
	multi-family residential				
Land Uses	Retail, service & employment; some multi-family				
Heights					
Minimum	40 feet				
Maximum	75 feet				
FAR	2.0 - 4.0				

















# F6 Gateway District

Sites within the Gateway District will serve as the main entrance to and the economic heart of the Ford redevelopment site. The District provides for a variety of business and office uses independently or in combination with retail and service establishments. Educational or civic uses may also be present. The district is focused on employment activity and complementary work force services.

General Character	Attractive gateways into site, focused on employment
	with some retail and service
Land Uses	Office; institution; retail & service; employment
Heights	
Minimum	30 feet
Maximum	65 feet
FAR	1.0 - 3.0







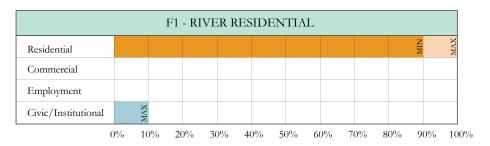


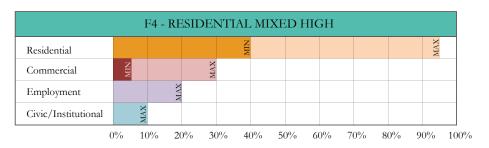


### 4.5 Required Mix of Uses

There is a difference between allowing a mix of uses within a district and requiring a mix. Requiring a minimum and maximum mix of uses ensures visual, functional, and economic diversity within proximity. Each district on the Ford site shall include a mix of uses as identified below. The required range of land uses by type may range from no requirement, to a base minimum, and up to a maximum percentage of floor area constructured within the entire zoning district, including all current and planned construction for that district. The requirements should be used by the site master developer throughout site build-out, to guide the selection of subdevelopers and projects for each block.

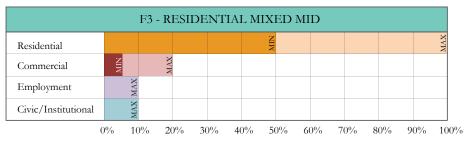
Table 4.2 Required Mix of Uses

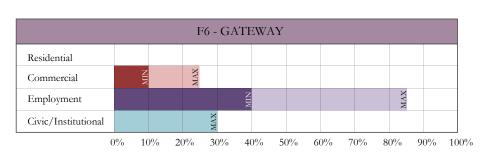
















The following table sets the overall range of development for the master plan by land use. In addition to the zoning district-based tool, it is intended to ensure a balance of land uses throughout the site, while allowing flexibility to allow for the market to make fine-grained decisions about the location of uses. Setting upper and lower limits provides the foundation for a financially feasible development, while controlling the maximum buildout, and provides clear expectations for the community and the private sector.

Development Range for Master Plan									
Land Uses Minimum Maximum									
Housing	2,400 Dwelling Units	4,000 Dwelling Units							
Retail & Service	150,000 Sq. Ft. GFA	300,000 Sq. Ft. GFA							
Office & Employment	200,000 Sq. Ft. GFA	450,000 Sq. Ft. GFA							
Civic & Institutional	50,000 Sq. Ft. GFA	150,000 Sq. Ft. GFA							

GFA: Gross Floor Area



DRAFT (7/21/2017) - For review by the Saint Paul Planning Commission

### 4.6 District Uses

Use	F1	F2	F3	F4	F5	F6	Definition (d) Standards (s)
Residential Uses							Standards (s)
Dwellings							
Two-family dwelling	Р						(d)
Multiple-family dwelling	Р	Р	Р	Р	Р		(d)
Carriage house dwelling	Р	Р					(d)
Mixed Commercial-Residential Uses							
Home occupation	Р	Р	Р	Р	Р	Р	(d), (s)
Live-work unit		Р	Р	Р	Р	Р	(d), (s)
Mixed residential and commercial use		Р	Р	Р	Р	Р	<del>(d)</del>
Congregate Living	•		•			•	
Adult care home		Р	Р	Р	Р	Р	(d)
Community residential facility, licensed correctional		С	С	С			(d), (s)
Dormitory				Р	Р		(d), (s)
Emergency housing facility		С	С	С			(d), (s)
Foster home	Р	Р	Р	Р			(d)
Shareable housing		Р	Р	Р	Р		(d)
Shelter for battered persons	P/C	P/C	P/C	P/C	P/C		(d), (s)
Sober house	P/C	P/C	P/C	P/C	P/C		(d), (s)
Supportive housing facility	P/C	Р	Р	Р	Р		(d), (s)
Civic and Institutional Uses							
Club, fraternal organization, lodge hall		Р	Р	Р	Р		(d)
College, university, specialty school		Р	Р	Р	Р	Р	(d) <u>, (s)</u>
Day care, primary and secondary school		Р	Р	Р	Р	Р	(d), (s)
Public library, museum	Р	Р	Р	Р	Р	Р	
Public and private park, playground	Р	Р	Р	Р	Р	Р	
Recreation, noncommercial		Р	Р	Р	Р	Р	(d)
Religious institution, place of worship		Р	Р	Р	Р	Р	(d)

#### Table 4.3 District Uses

Uses allowed in the Ford site Zoning Districts are identified in this table. Most of the uses identified are in the citywide zoning code, Chapter 65 Land Use Definitions and Standards.

Uses unique to the Ford zoning districts are noted as such in the rightmost column, Definitions and Standards, and are defined in this plan, Chapter 10: Definitions.



# 4.6 District Uses (Cont.)

Use	F1	F2	F3	F4	F5	F6	Definition (d) Standards (s)		
Public Services and Utilities									
Antenna, cellular telephone	P/C	P/C	P/C	P/C	P/C	P/C	(d), (s)		
Electric transformer or gas regulator substation			Р	Р	Р	Р	(s)		
Municipal building or use		Р	Р	Р	Р	Р	(s)		
Public utility heating or cooling plant		Р	Р	Р	Р	Р			
Utility or public service building	Р	Р	Р	Р	Р	Р	(d), (s)		
Commercial Uses			•		-	•			
Office, Retail and Service Uses									
General office, studio		Р	Р	Р	Р	Р	(d)		
General retail		Р	Р	Р	Р	Р	(d)		
Service business, general		Р	Р	Р	Р	Р	(d)		
Service business with showroom or workshop		Р	Р	Р	Р	Р	(d)		
Animal day care					<u>P</u>	<u>P</u>	<u>(d), (s)</u>		
Business sales and services					Р	Р	(d)		
Dry cleaning, commercial laundry			Р	Р	Р				
Farmers market		P/C	P/C	P/C	P/C	P/C	(d), (s)		
Garden center, outdoor			Р	Р	Р	Р	(d)		
Greenhouse				Р	Р	Р	(d), (s)		
Hospital				Р	Р	Р	(d)		
Mortuary, funeral home				Р	Р	Р			
Outdoor commercial use			P/C	P/C	P/C	P/C	(d), (s)		
Package delivery service					Р	Р	(d)		
Small engine repair, automotive bench work					Р	Р			
Veterinary clinic		Р	Р	Р	Р	Р	(d), (s)		



# 4.6 District Uses (Cont.)

Use	F1	F2	F3	F4	F5	F6	Definition (d) Standards (s)		
Food and Beverages									
Bar				P/C	P/C	P/C	(d), (s)		
Brew on premises store			Р	Р	Р	Р	(d), (s)		
Coffee shop, tea house		Р	Р	Р	Р	Р	(d)		
Restaurant		Р	P	Р	Р	Р	(d)		
Restaurant, fast-food					P/C	P/C	(d), (s)		
Commercial Recreation, Entertainment and Lodging									
Bed and breakfast residence	<u>P</u>						(d), (s)		
Health/sports club			Р	Р	Р	Р	(d)		
Hotel, inn			P	P	Р	Р			
Indoor recreation			С	С	С	С	(d), (s)		
Reception hall/rental hall			С	С	Р	Р			
Short-term rental dwelling unit	P/C	P/C	P/C	P/C	P/C	P/C	(d), (s)		
Theater, assembly hall, concert hall			С	С	С	С			
Automobile Services									
Auto convenience market					С		(d), (s)		
Auto service station, auto specialty store					С		(d), (s)		
Auto repair station					С		(d), (s)		
Auto sales, indoor					С				
Car wash <u>, detailing</u>					С		(s)		
Parking Facilities									
Parking facility, commercial		С	С	С	С	С	(d)		
Transportation									
Bus or rail passenger station				С	С	С			
Railroad right-of-way	С	С	С	С	Р	Р	(s)		

# 4.6 District Uses (Cont.)

Use	F1	F2	F3	F4	F5	F6	Definition (d) Standards (s)	
Limited Production, Processing and Storage								
Agriculture	Р	Р	Р	Р	Р	Р	(d), (s)	
Brewery, craft		Р	Р	P	Р	Р	(d)	
Distillery, craft			Р	Р	Р	Р	(d)	
Finishing shop					Р	Р	(d), (s)	
Limited production and processing			Р	Р	Р	Р	(d), (s)	
Mail order house			Р	P	Р	Р		
Printing and publishing			Р	Р	Р	Р		
Recycling drop-off station					С	С	(d), (s)	
Research, development and testing laboratory					Р	Р		
Wholesale establishment					Р		(d)	
Winery, craft		Р	Р	Р	Р	Р	(d)	
Accessory Uses								
Accessory use	Р	Р	Р	Р	Р	Р	(d), (s)	

P - Permitted use C - Conditional use requiring a conditional use permit

Notes to table 66.921, Ford district uses:

- (d) Definition for the use in Chapter 65, Land Use Definitions and Development Standards.
- (s) Standards and conditions for the use in Chapter 65, Land Use Definitions and Development Standards.



### 4.7 General Standards

#### Introduction

All zoning districts in the City of Saint Paul have general standards to guide the design and form of buildings, public amenities and rights-of-way. Most of the regulations are identified in Saint Paul Zoning Code Chapter 63. Regulations of General Applicability. In addition to the Citywide standards are standards unique to the Ford zoning districts.

Standards specific to the Ford zoning districts focus on design to achieve higher levels of sustainability performance, related to energy conservation and efficiency, reduced auto dependence and vehicle emissions, reduced water runoff, reduced localized heating effects, and enhanced vegetation.

The general standards specific to the Ford site, as described in the following pages, relate to:

- Vegetation and Landscaping
- Lighting
- Solar
- Roofing
- Parking

## Design Standards

Design standards for buildings and public spaces on the Ford site redevelopment will be prepared for and added to this Ford Site Zoning and Master Plan in 2018. Until such time, the Traditional Neighborhood design standards for the T3 district shall apply, City Zoning Code Section 66.343.







**Purpose**: To maximize ecosystem restoration, preservation and stability to the greatest extent practical is critical to economic, social, biological, and aesthetic value and sustainability of the site. The flora and fauna on the site will provide aesthetic and health benefits for all inhabitants and visitors to the site.



The previous state of the site was largely developed with little vegetative and habitat layer. Reintroducing a strong system of plants will increase the site's value economically, socially and environmentally. Planting and vegetation across the site and in smaller areas should focus on visual interest through all seasons and be attractive to wildlife, especially birds and pollinators. The intent of these standards is to:

- Maximize biodiversity of the site and provide maximum possible contribution to local landscape ecology
- Reduce removal of significant existing vegetation
- Re-establish habitat and extensive vegetation on site with new plantings
- Create visual interest
- Provide wildlife habitat
- Maximize ecological services

The following standards are to be used in place of standards in Saint Paul Zoning Code Section 63.115. Landscaping and plant materials.

### Open Space Coverage

Required open space coverage for lots is addressed in Chapter 5 Building Types. Open space is defined as areas covered by landscape materials, gardens, walkways, patios, recreation facilities, or play areas.





### Landscaping Requirements

- 1. Areas between a building façade and a public right-of-way shall be landscaped, except for public patios or seating areas and pathways between the right-of-way and building entrances and exits.
- 2. Street trees are required if the front setback is greater than six (6) feet.
  - One deciduous tree with 3" minimum caliper is required to be planted within the front setback for every 30 feet of frontage if the front setback is greater than six (6) feet.
  - Trees in paved areas shall have a minimum 25 square feet of permeable area for growth.
  - Trees in islands shall have a minimum of 50 square feet of permeable area for growth.
- 3. At least fifty percent (50%) of landscaped areas shall be combination of indigenous grasses, trees and shrubs commonly found in Minnesota. Alternatives to turf grass are strongly encouraged on public and private properties. Up to one hundred (100) percent of boulevards and private yards may be planted with a xeriscape mixture of live plants and ground cover.
- 4. All landscaped areas shall be continuously maintained and irrigated. Plant materials shall be organically maintained to the maximum extent possible.
- 5. Visibility No plantings shall obscure site entrance and exit drives and road intersections.

### Tree and Vegetative Habitat Preservation

Trees over 10 inch diameter shall be preserved and protected from construction within their tree canopy diameter. Exceptions shall be made for ash, box elder, or invasive species. Exceptions may also be made with administrative approval if removal of the tree is necessary for the installation of public infrastructure, such as roads, sewer or stormwater.

Do not disturb vegetative habitat determined significant by Minnesota DNR Natural Heritage Program; maintain or install appropriate buffer width around significant habitats that comprise part of a development

### Vegetative Types

Design Standards for the site to be adopted in 2018 will provide an appendix of plant types, areas for use (boulevard, lawns, etc), and ideal planting conditions (sun/shade; wet/dry; etc.).





Table 4.4 Vegetative and Soil Requirements

	Unit	F1 River Residential	F2 Residential Mixed Low	F3 Residential Mixed Mid	F4 Residential Mixed High	F5 Business Mixed	F6 Gateway		
Planting Size									
Trees - minimum planted size	(Caliper; Ht)			2.4 in;	6 feet				
Shrubs	Diameter		18 inches						
Vegetative Variety (minimum	mix of species)								
Native overall	Minimum	85%	85%	75%	75%	75%	75%		
Trees	Min Species Mix		6 per acre; 4 per block						
Shrubs	Min Species Mix			5 per	racre				
Perennials	Min Species Mix			10 pe	er acre				
Tree Canopy (measured as the	e % of the area)								
Tree canopy cover		50%	50%	30%	20%	20%	20%		
Public Canopy Cover									
Civic space minimum	Area Covered	50%	% of non-built lot	area	25%	% of non-built lot	area		
Street tree requirements	Spacing	clust	tered		30' on center				
Private Canopy Cover									
Private lot minimum	Area Covered	1 pe	r 7500 sf of lot or	12%		none			
Parking lot minimum	Area Covered			30	)%				
Healthy Tree Standards									
Minimum permeable surface per tree	Area	270 sf	270 sf	25 sf	25 sf	25 sf	25 sf		
Structural soil per tree	Area	180 sf	180 sf	250 sf	250 sf	250 sf	250 sf		
Soil Volume Standards for T	ree Planting								
Soil volume		Minimum 2 cuft of soil per 1 sq ft of canopy, based on average mature tree size; or 400 cu ft for small trees, 800 cu ft for medium trees, or 1,200 cu ft for large trees.							
Soil volume type and location		Soil volume goals may be achieved through connected or combined soil beds or grouped tree planting. Use of structural soil under hardscapes, planting soil in open planting beds. Volume of structural soil/engineered soil structures to be determined by percentage of soil volume available.							





**Purpose** - To reduce unnecessary lighting and light pollution, to minimize lighting impacts on surrounding properties and to minimize energy consumption for lighting purposes.

The following standards are to be used in place of standards in Saint Paul Zoning Code Section 63.116. Exterior lighting.

Table 4.5 Lighting Requirements by Zoning District

	F1 - River Residential	F2 - Residential Mixed Low	F3 - Residential Mixed Medium	F4 - Residential Mixed High	F5 - Business Mixed	F6 - Gateway
Ambient Light Level Goal	medium	low	low	medium	medium-high	medium-high
Lighting Standards, Maximum	Full cutoff lighting, controlled with dimmer, time switch or motion sensors	Full cutoff lighting, controlled with dimmer, time switch or motion sensors	Full cutoff lighting, some low wattage, non-full cutoff lighting, controlled with dimmers, time switch or motion sensors	Full cutoff lighting, some low wattage, non-full cutoff lighting, controlled with dimmers, time switch or motion sensors	Full cutoff lighting, some low wattage, non-full cutoff lighting, controlled with dimmers, time switch or motion sensors	Full cutoff lighting, some low wattage, non-full cutoff lighting, controlled with dimmers, time switch or motion sensors
Allowed Initial Lamp Lumens per square foot, Maximum	2.5 - 3.2 lumens per square foot	3.3 - 4.2 lumens per square foot	7.6 - 9.7 lumens per square foot	7.6 - 9.7 lumens per square foot	7.6 - 9.7 lumens per square foot	7.6 - 9.7 lumens per square foot
Lamp Allowance (Lumens), Maximum	17,000 lumens	24,000 lumens	44,000 lumens	44,000 lumens	44,000 lumens	44,000 lumens
Foot Candles at Property Line, Maximum	0.1 horizontal and vertical	0.1 horizontal and vertical	0.2 horizontal and vertical	0.2 horizontal and vertical	0.2 horizontal and vertical	0.2 horizontal and vertical
Required Shielding	Fully shielded luminaire with no uplight or better	Shielded luminaire with no uplight or better	Shielded luminaire with no uplight or better	Shielded luminaire with no uplight or better	Partially shielded luminaire with no uplight or better	Partially shielded luminaire with no uplight or better
Lighting Curfew for Non- Residential	10 pm or close of business, whichever is later	10 pm or close of business, whichever is later	10 pm or close of business, whichever is later	10 pm or close of business, whichever is later	10 pm or close of business, whichever is later	10 pm or close of business, whichever is later
Height of Light Fixture, Maximum	20 feet for freestanding fixture; height of building for attached fixture	20 feet for freestanding fixture; height of building for attached fixture	20 feet for freestanding fixture; height of building for attached fixture	20 feet for freestanding fixture; height of building for attached fixture	20 feet for freestanding fixture; height of building for attached fixture	20 feet for freestanding fixture; height of building for attached fixture





## Light Source Types

All exterior lights shall be LED "warm-white" or LED filtered light to minimize blue emission (CCT < 3,000 K; S/P ratio < 1.2).

## Uplighting and Aimed Lighting

Direct upward lighting and lighting aimed at structures is prohibited except as follows:

- Accent lighting of architectural features: provided that no glare or off-site light spillover is produced. Lamps for this type of accent lighting must be low intensity and utilize less than 100 watts and emit less than 1,600 lumens
- Accent lighting of other features, such as statues, public art, or other objects of interest that cannot be illuminated with down lighting, may use narrow-cone spotlights that use less than 100 watts and emit less than 1,600 lumens. A narrow-cone spotlight image is located in Section 19.81.040 F.1.
- All other lighting aimed against structures can be aimed against structures if (1) the light is contained by the structure, (2) no glare is visible off site and (3) the fixture is fully shielded so none of the light emitted above the horizontal plan crosses over the property line.
- Low voltage landscape light (such as lighting used to illuminate fountains, shrubbery, trees, and walkways etc.) shall be permitted provided that the lighting is not mounted on a pole or building, it is shielded to eliminate glare and light spillover and each fixture uses only a maximum of 60 watts or emits a maximum of 750 lumens (whichever is less).



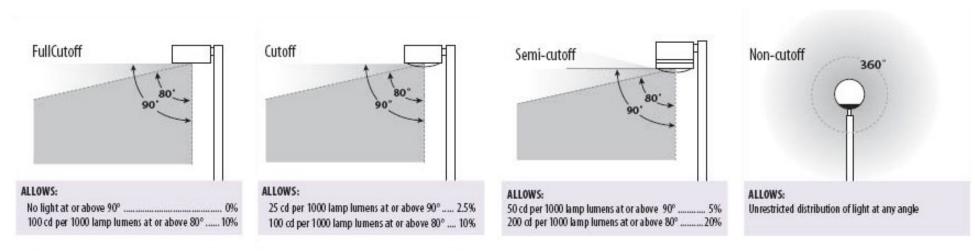


#### Outdoor Performance, Sport, and Recreation Facilities

- Playing fields or other special activity areas illuminated by floodlights should be mounted and shielded so their beams fall within the primary play area and its immediate surroundings. It should avoid spillover onto residentially zoned property.
- Recreational facilities should be illuminated only during use of the facility. Non-security lights should be turned off by 9 PM or 30 minutes after the event, whichever is later.

#### Maintenance

Outdoor light fixtures must be kept in good order and maintained to serve the original design intent of the lighting system and ensure compliance with this chapter.



Lighting Cutoff Designations







**Purpose:** to promote the safe, effective and efficient use of active solar energy systems installed to reduce the on-site consumption of fossil fuels or utility-supplied electric energy. The following solar energy standards support the installation of solar systems or the design of buildings to be solar ready for future installations.

The following standards are to be used in place of standards in Saint Paul Zoning Code Section 65.921. Solar energy system.

#### Solar Access

Solar access for adjacent buildings must be preserved such that at least 50% of the roof area of any building shall not be shaded at noon on December 22 of the year.

### Permitted Accessory Use

Active solar energy systems shall be allowed as an accessory use in all zoning classifications where structures of any sort are allowed, subject to certain requirements as set forth in Table 4.5 Solar Standards.

#### Solar Administration

**Approved Solar Components**: Electric solar energy system components must have a UL listing and solar hot water systems must have an SRCC rating.

**Plan Approval Required**: All solar energy systems shall require administrative plan approval by the City of Saint Paul zoning administrator.







#### Solar Administration (Continued)

**Plan Applications:** Plan applications for solar energy systems shall be accompanied by to-scale horizontal and vertical (elevation) drawings. The drawings must show the location of the system on the building or on the property for a ground-mount system, including the property lines.

- Pitched Roof Mounted Solar Energy Systems: For all roof-mounted systems other than a flat roof, the elevation must show the highest finished slope of the solar collector and the slope of the finished roof surface on which it is mounted.
- Flat Roof Mounted Solar Energy Systems: For flat roof applications a drawing shall be submitted showing the distance to the roof edge and any parapets on the building and shall identify the height of the building on the street frontage side, the shortest distance of the system from the street frontage edge of the building, and the highest finished height of the solar collector above the finished surface of the roof.

**Plan Approvals**: Applications that meet the design requirements of this ordinance, and do not require an administrative variance, shall be granted administrative approval by the zoning official and shall not require Planning Commission review. Plan approval does not indicate compliance with Building Code or Electric Code.

### **Compliance with Codes**

- All active solar energy systems shall meet approval of local building code officials, consistent with the State of Minnesota Building Code, and solar thermal systems shall comply with HVAC-related requirements of the Energy Code.
- All photovoltaic systems shall comply with the Minnesota State Electric Code.
- Solar thermal systems shall comply with applicable Minnesota State Plumbing Code requirements.

**Utility Notification:** All grid-intertie solar energy systems shall comply with the interconnection requirements of the electric utility. Off-grid systems are exempt from this requirement.

**Restrictions on Solar Energy Systems Limited:** No homeowners' agreement, covenant, common interest community, or other contract between multiple property owners within a subdivision shall restrict or limit solar energy systems to a greater extent than these solar energy standards.





Table 4.6 Solar Standards

	Roof-Mounted	Ground or Pole Mounted	Building Integrated
Height	Height shall not exceed the maximum allowed building height in any zoning district, except for height exceptions as allowed for building mounted mechanical devices or equipment.	Height shall not exceed twenty (20) feet when oriented at maximum tilt.	
Setback	Setback shall not extend beyond the exterior perimeter of the building on which the system is mounted or built, unless the collector and mounting system has been explicitly engineered to safely extend beyond the edge, and setback standards are not violated. Exterior piping for solar hot water systems shall be allowed to extend beyond the perimeter of the building on a side yard exposure.	Setback from property line must be at least one (1) foot and may not extend into the side-yard or rear setback when oriented at minimum design tilt.	The building component in which the system is integrated shall meet all required setback, land use or performance standards for the district in which the building is located.
Coverage	Coverage of the building, excluding building-integrated systems, shall allow for adequate roof access to the south-facing or flat roof upon which the panels are mounted.	The surface area of pole or ground mount systems shall not exceed half the building footprint of the principal structure.	
Visibility	Roof-mount systems that are visible from the nearest edge of the street frontage right-of-way shall not have a highest finished pitch steeper than the roof pitch on which the system is mounted, and shall be no higher than twelve (12) inches above the roof.	Ground-mount solar energy systems shall not be restricted if the system is not visible from the closest edge of any public right-of-way other than an alley.	May be visible from the public right-of- way, provided the building component in which the system is integrated meets all required setback, land use or performance standards for the district in which the building is located.
Aesthetics	Active solar energy shall be designed to blend into the architectu performance. The color of the solar collector is not required to	9	·





Purpose: To meet multiple objectives for sustainable design in roofing, including:

- Reduce localized heat pockets generated by heat absorbent surfaces
- Reduce building cooling loads by reducing heat absorption on roofs
- Enable rooftop solar systems on buildings
- Reduce stormwater run-off from rooftops

#### Pitch, Orientation, Materials and Reflectivity

Design buildings for, or in anticipation of, solar system installations, following solar ready building design guidelines and recommendations.

Flat roofs should be single ply membrane type with a minimum pitch of 1/4 inch per foot.

Pitched roofs must be surfaced with materials designed to last at least 25 years, and provide at least one primary roof face at a south or southwest-facing pitch between 25-45 degrees.

Roofing materials shall have a Solar Reflective Index (SRI) of minimum 29 where the pitch is greater than 2:12, or an SRI of minimum 78 where the pitch is less than or equal to a 2:12 pitch.

If building size allows, provide a minimum of 200 square feet of contiguous roof area for solar systems.

#### Equipment and Structures

Locate rooftop structures and equipment, such as plumbing, exhaust vents, chimneys, or gables, are away from south facing roofs and as far from potential solar system locations as possible while still meeting other design requirements.

The visual impact of rooftop equipment shall be reduced through such means as location, screening, or integration into the roof design. Screening shall be of durable, permanent materials that are compatible with the primary building materials.









#### Green Roofs

Functional Green Roof Area shall be defined as area atop a roof surface on a building, open to the sky and air, which is surfaced with soil and living plant materials for the purpose of retaining rainwater and absorbing heat from sunlight. The depth of soil and planted material shall be at least two (2) inches to be considered Functional Green Roof Area.

### Roof Design Exemptions for Functional Green Roof Area

Functional Green Roof Areas shall be exempt from the rooftop design standards identified in "Pitch, Orientation, Materials and Reflectivity".

#### Green Roof Areas as Open Space

Where a rooftop surface above the third floor includes Functional Green Roof Area, adjacent open-air outdoor space intended for use by building occupants or other persons that does not meet the definition of Functional Green Roof Area, such as a patio or deck, is eligible to meet up to 50% of the open space requirements of the property/site, as measured in gross square feet of the usable adjacent space. All such usable outdoor space shall be set back at least ten (10) feet from all outer roof edges, and shall be located and oriented in relation to adjacent properties to minimize potential visual, noise and privacy impacts to abutting uses.







**Purpose:** to provide vehicular parking that meets the basic demand created by uses on the site, through the predominant use of structured parking. To provide convenient, plentiful and secure bicycle parking at places of residence, employment, shopping or service, and recreation.

Parking Standards are Pursuant to Saint Paul Zoning Code Section 63.300, except as noted herein (obviates Saint Paul Zoning Code subsections 63.303-07)

Table 4.7 Vehicle Parking Requirements by Use

Required Parking					
Land Uses	Minimum	Maximum			
Non-residential	1 space per 600 square feet GFA	1 space per 400 square feet GFA			
Residential, dwellings	0.75 space per dwelling	2.0 spaces per dwelling			
Residential, congregate living	0.25 space per bedroom	1.0 space per bedroom			

Note: There are no special provisions to reduce minimum required parking or to exceed maximum required parking.

Surface parking shall not exceed 20 spaces per development block.

Structured parking is permitted provided, at a minimum, that the entire ground floor of the structure contains active uses with entrances on all street frontages. Underground parking is permitted in any structure. Structured parking must be designed with level parking floors and adequate floor to ceiling clearance height to allow the space to be converted to finished floor area if parking is no longer needed in the future.

**Shared parking** facilities are allowed and encouraged, but uses sharing facilities are not eligible for reductions to minimum parking requirements as a result of sharing, per 63.206(d), since off-street parking requirements already anticipate lower parking space demand due to sharing.

**Residential Parking for 1-6 unit buildings:** Parking garages for residential structures of one to six-units shall be placed at the rear of the building with access to a lane or alley or as a tuck under placed at least ten (10) feet behind the building facade. Tuck under parking on the main frontage of a residential structure is only permitted if there is no rear access to the lot from an alley or lane. Surface parking spaces or semi-covered spaces are allowed in lieu of garages, if placed at the rear of the building with access to a lane or alley. Driveways shall only provide access to the alley or lane at the rear of the lot, unless no such right-of-way exists.

For residential buildings on lots less than 50 feet wide and for all Sideyard, Rearyard, and Courtyard residential buildings, garages or off-street parking shall be accessed from an alley or via a shared driveway only.





## Fee-in-Lieu of Parking

For non-residential uses, properties may satisfy parking requirements by paying a fee in-lieu of parking for each required space. Fees collected shall become part of a site wide parking fund and shall be used solely for the repayment, development or maintenance of parking that satisfies the demand requirements of new development projects within the parking in-lieu fee area. A sitewide parking district will establish the terms, conditions and use for fee-in-lieu payments.

The number of parking spaces required by the change in occupancy shall be the difference between the number required by the new use and the number required by the previous occupancy.

The per-space fee for new construction, additions or changes in occupancy must be paid by the following method: In a lump sum, prior to the issuance of construction permits for the structure or occupancy for which the parking is required or prior to the issuance of a city business license for the activity for which the parking is required, if no construction permit is required. (Ord. 1411 § 1, 2002; Ord. 1101 § 3 (part), 1987)

Once a property is subject to an obligation for parking, provided either with onsite parking spaces or by in-lieu parking fees, the following shall apply if the use is changed or discontinued:

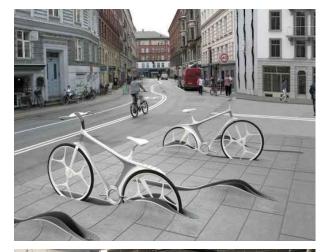
If a structure is enlarged or a use is replaced with a use for which more parking is required according to the zoning regulations, the additional parking requirement may be met with additional onsite parking spaces or by paying additional in-lieu fees.

If a structure is reduced in area, or wholly or partially becomes vacant, or a use is replaced with a use for which less parking is required according to the zoning regulations, onsite parking spaces shall be reduced to meet the new requirement. If in-lieu fees were paid to meet the original parking requirement, there shall be no refund of lump-sum payment or abatement of installment payments.

If a structure is destroyed and in-lieu payments for parking are in place, then upon the property owner's request the city shall cancel the obligation for following installment payments. Any future structure or use at the same location would be required to meet the parking requirement as provided in the zoning regulations for new development.

A change of ownership or the dividing or merging of properties shall not affect an obligation for parking in-lieu fees or a determination that parking requirements have been met according to fees paid for a particular use.









### Bike Parking

Bicycle parking is an area and facility used for the securing of bicycles. It includes enclosed bicycle storage, covered bicycle racks or fixed bicycle racks which permit the locking of the bicycle frame and one (1) wheel to the rack and support the bicycle in a stable position, anchored to prevent easy removal.

Table 4.8 Required Bicycle Spaces, by Use

Residential, dwellings	1 space per dwelling
Residential, congregate living	1 space per bedroom
Educational	1 space per 3 students
Recreational	1 space per 300 square feet of surface area
Commercial, Office, Civic	1 space per 5,000 square feet GFA
Production and Processing	1 space per 15,000 square feet GFA

**Location and design** - The following standards shall apply to bicycle parking facilities provided per the requirements of this code:

- a) Each bicycle parking space shall provide 6 feet by 2 feet in area per bicycle plus the area needed for access.
- b) Bicycle parking shall be located no closer than 3 feet from any wall or 3 feet from face of curb to provide adequate space for access and maneuvering.
- c) Outdoor bicycle facilities or facilities within interior spaces shall be lit for ease of use and safety.
- d) Bicycle parking facilities shall be maintained in accordance with City of Saint Paul Zoning Code Section 63.315, and kept free from rust and corrosion.







### Bicycle Parking Location and Design (cont.)

- e) The location of bicycle parking facilities shall be located to be convenient to the main entrance of the primary use. If required bicycle parking is not visible from the street or main building entrance, a sign shall be posted at the main entrance indicating the location of the parking.
- f) Outdoor bicycle parking shall be visible from the public right-of-way or from inside the building. With a use of right-of-way permit from the city engineer, bicycle parking may be located in the public right-of-way.
- g) Bicycle racks installed on sidewalks shall provide for a clear, unobstructed width of at least 5 feet for pedestrians.
- h) Bicycle racks and bicycle storage facilities shall be secured to the ground or the building structure to prevent them from being removed from the location.
- i) Bicycle parking provided within a building shall be signed for bicycles.
- j) Bicycle parking spaces located in attended parking lots or garages shall be located adjacent to the attendant's booth or in an area under constant surveillance.
- k) Indoor bicycle parking for commercial uses shall be accessible during regular hours of operation. Indoor bicycle parking for multi-family dwellings shall be accessible to residents at all times.
- l) Where motor vehicle parking spaces are monitored, covered or weather protected, required bicycle parking spaces shall be provided on the same basis.
- m) Office and production/processing uses shall provide 1 shower per 50 employees.





### Special Parking Facilities

#### Car Share Parking

One (1) designated space for car share vehicles shall be provided for every 20 spaces of individual parking.

#### Electric Vehicle Infrastructure

The intent of this section is to support the use of electric vehicles and to expedite the establishment of convenient, cost-effective electric vehicle infrastructure.

## A. Parking

- 1. All new and expanded parking areas shall provide the electrical capacity necessary to accommodate the future hardwire installation of Level-2 electric vehicle charging stations. A minimum of one (1) parking space or two percent (2%) of the total parking spaces, whichever is greater, shall be prepared for such stations.
- 2. An electric vehicle charging station space may be included in the calculation for minimum required parking spaces required in accordance with Section 1804.
- 3. Electric vehicle charging stations in public parking facilities or available for public use shall be reserved for use of electric vehicles for charging purposes only. Electric vehicles may not park in the designated electric vehicle charging space if the vehicle is not charging.
- B. Accessible Spaces: A minimum of one (1) accessible electric vehicle charging station shall be provided in parking facilities with five (5) or more charging spaces. Accessible electric vehicle charging stations should be located in close proximity to the building or facility entrance and connected to a barrier-free accessible route of travel.









- C. Lighting: Site lighting shall be provided where an electric vehicle charging station is installed, unless charging is for daytime purposes only.
- D. Equipment Standards and Protection
  - 1. Battery charging station outlets and connector devices shall be no less than 36 inches and no higher than 48 inches from the surface where mounted. Equipment mounted on pedestals, lighting posts, bollards, or other devices shall be designed and located as to not impede pedestrian travel or create trip hazards on sidewalks.
  - 2. Adequate battery charging station protection, such as concrete-filled steel bollards, shall be used. Curbing may be used in lieu of bollards, if the battery charging station is setback a minimum of 24 inches from the face of the curb.
- E. Usage Fees: The property owner is not restricted from collecting a service fee for the use of an electric vehicle charging station made available to visitors of the property.
- F. Signage for Special Parking
  - 1. Information shall be posted identifying voltage and amperage levels and any time of use, fees, or safety information related to the electric vehicle charging station.
  - 2. Each electric vehicle charging station space shall be posted with signage indicating the space is only for electric vehicle charging purposes. For purposes of this subsection, "charging" means that an electric vehicle is parked at an electric vehicle charging station and is connected to the battery charging station equipment.
- G. Maintenance: Electric vehicle charging stations shall be maintained in all respects, including the functioning of the equipment. A phone number or other contact information shall be provided on the equipment for reporting when it is not functioning or other problems are encountered.

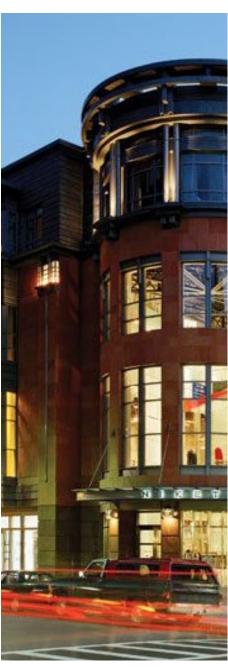




# CHAPTER 5: ZONING - BUILDING TYPES







## 5.1 Overview

The Ford site zoning districts establish basic standards to guide development, while allowing a wide range of uses. Given the variability of building types within each district, the Ford site zoning uses building type as the basis for most of the standards. This provides for consistency in built form related to use as opposed to geography. The following standards based on building type relate to the following elements:

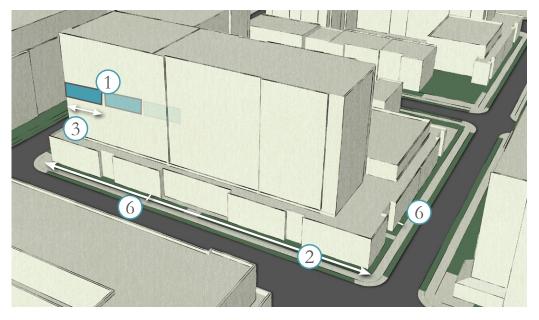
- Units per Building
- Lot Width
- Lot Coverage
- Setbacks
- Parking
- Accessory Structures

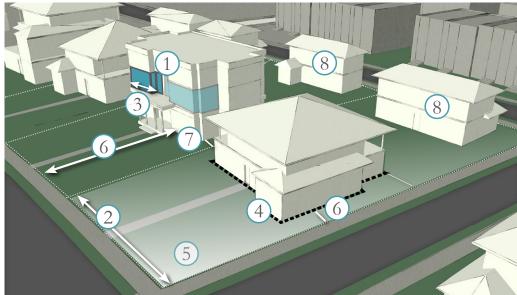


## Building and Lot Terminology

The same zoning concepts and regulations can apply to multiple building and lot types. The concepts as they apply to two example situations are shown here.

- 1 Dwelling Unit Counts
- (2) Lot Width
- 3 Dwelling Unit Width
- (4)÷(5) Lot Coverage
  - 6 Right-of-Way Setback
  - 7 Interior Lot Line Setback
  - 8 Accessory Structures



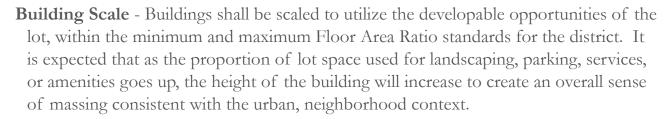






#### Building Placement, Scale & Design

Building Placement and Location - On major, mixed-use corridors, buildings shall be located at the front of the lot along the primary pedestrian-way, with shallow setbacks designed for public use and enjoyment. On predominantly residential blocks, buildings shall be oriented to the primary pedestrian-way, with shallow to medium setbacks landscaped for private use. Office, research and industrial buildings on any corridor shall be oriented to the main pedestrian-way, with shallow to medium setbacks designed for public or private use.



**Building Design** - High quality design, materials, and construction standards are expected for all buildings, while providing a range of architectural styles within proximity to create interest and reduce repetitiveness. A mix of traditional and modern buildings forms are encouraged, while providing some compatibility with surrounding buildings through the use of common cornice lines, roofs treatments, or other design features.

Design standards will be developed in 2018 that address buildings exteriors and the space around them.











## 5.2 Building Types by Zoning District

The zoning districts for the Ford site allow a range of land uses and building types. Some of the zoning standards for the site are specific to the underlying zoning district, as described in Chapter 4, and some are specific to the building type, as described in this chapter. The following table shows the building types that are allowed within each zoning district. Four of the six zoning districts allow a mix of residential and commercial uses. The two more restrictive districts are River Residential, which allows a limited residential form, and Gateway, which does not allow any residential. Civic and Institutional Uses are allowed in all districts.



Table 5.1 Building Types Allowed by Zoning District

		BUILDING TYPE									
ZONING DISTRICT	Carriage House	Multi-Unit Home	Townhouse	Multi- Family Low	Multi- Family Medium	Multi- Family High	Live / Work	Mixed Residential & Commercial	Commercial & Employment	Parking Structure	Civic & Institutional
F1 - River Residential											
F2 - Residential Mixed Low											
F3 - Residential Mixed Mid											
F4 - Residential Mixed High											
F5 - Business Mixed											
F6 - Gateway											





## 5.3 Building Type Standards

Zoning standards related to each building type are summarized in the table below. These standards are in addition to those related to the underlying zoning district. Subsequent pages address each building type individually, identifying their specific zoning standards and characteristics.

Table 5.2 Building Type Standards Summary Table

		BUILDING TYPE								
STANDARD	Multi-Unit Home	Carriage House	Townhouse / Rowhouse	Multi-Family, Low	Multi-Family, Medium	Live/Work	Mixed Residential & Commercial	Civic & Institutional	Commercial & Employment	Parking Structure
Units per Bldg	2-6	1-2	3-16	6 - 40	40 and over	2-8		n	ı/a	
Building Width, maximum	6	0'	150'	200'	60' min, no max	150'		n/a	<u>500°</u>	
Lot Width, minimum	8	0'	30'	60'	n/a	30'		n/a		
Lot Coverage by Bldgs, maximum	30	)%	50% 70% <del>80%</del> 70%				<del>5</del> <u>70%</u>			
Lot Coverage for Open Space, minimum	50	0%	25% <del>20%</del> _				<u>25%</u>			
Building Height	Determined by Zoning District	30' maximum				Determined by	Zoning District			
Public Right-of-Way Setback (a),(b)	Min. = 10' Max. = 40'		Min. Max.			Min. = 5' Max. = 20'			. = 5' . = 15'	
Interior Lot Line Setback (c)	Min. = 10' $Max. = n/a$		Min. = 6'  Max. = n/a							
Parking			unit and Max. = 2.0 spaces per dwelling unit; Min. =  Max. = 1.0 space per bedroom for congregate living.  Use combined standards for residential uses  Use combined standards for residential uses  Min. = 1.0 space per 600 square feet grown area.							
Accessory Structures	Up to 3	3 per lot	Up to 1 per dwelling unit	Up to 2 per structure	Up to 2 per structure	Up to 1 per dwelling unit	Up to 2 per structure	Up to 2 per structure	Up to 2 per structure	Up to 2 per structure

<sup>(</sup>a) Maximum building setback limit shall apply to at least 60% of the building façade along the right-of-way.

<sup>(</sup>c) No setback is required for building walls containing no windows or other openings when the wall meets the fire resistance standards of the Minnesota State Building Code and there is a Common Interest Community (CIC) or recorded maintenance easement that covers the affected properties.





<sup>(</sup>b) Buildings shall be setback a minimum of thirty (30) feet from a lot line separating a lot from Mississippi River Boulevard.

## Multi-Unit Home

Dwelling units	2-6 units per building
Lot width, minimum	80 feet
Building width, maximum	60 feet
Lot coverage by buildings, maximum	30% (includes coverage by secondary building - Carriage
	House, and by other accessory buildings)
Lot coverage by open space, minimum	50%
Building height	Minimum 20 feet; maximum 48 feet
Setbacks	
Public Right-of-Way	Minimum 30 feet minimum from Mississippi River Boule-
	vard and minimum 10 feet from other rights-of-way;
	maximum 40 feet
Interior Lot Line	10 feet minimum
Parking requirements	Minimum 0.75 spaces per dwelling unit; maximum 2.0
	spaces per dwelling unit; except as noted in Chapter 4,
	Parking.
Accessory buildings allowed	Up to 3 including the Carriage House building

**Definition:** The Multi-Unit Home building type is a small- to medium-sized building that consists of side-by-side or stacked dwelling units.

**Access**: Each unit will have a private interior entrance, but may share front and rear ingress/egress with other units. Building exteriors shall be accessed from the front street.

















## Carriage House

Dwelling units	1-2 units per building
Lot width, minimum	80 feet (per requirement for primary structure - Multi-unit
	home)
Building width, maximum	60 feet
Lot coverage by buildings, maximum	30% (includes coverage by primary building and other
	accessory secondary buildings)
Lot coverage by open space, minimum	50%
Building height	Maximum 30 feet
Setbacks	
Public Right-of-Way	Minimum 10 feet; maximum 20 feet
Interior Lot Line	Minimum 6 feet
Parking requirements	Minimum 0.75 spaces per dwelling unit; maximum 2.0 spaces
	per dwelling unit; except as noted in Chapter 4, Parking.
Accessory buildings allowed	Up to 3 including the Carriage House building

**Definition:** A combined residential and garage building, with small accessory secondary dwelling unit(s) located above and/or adjacent to the garage.

**Access**: Vehicles shall access this building type from alley or service streets. Pedestrians may access carriage houses from alleys, directly from the primary structure or from front streets.





## Live/Work

Dwelling units	2-8 units per building
Lot width, minimum	30 feet
Building width, maximum	150 feet
Lot coverage by buildings, maximum	70%
Lot coverage by open space, minimum	25%
Building height	Determined by zoning district
Setbacks	
Public Right-of-Way	Minimum 5 feet; maximum 20 feet
Interior Lot Line	At least 6 feet, except as noted in Table 5.2, Building Type standards Summary Table, footnote (b).
Parking requirements	Minimum 0.75 spaces per dwelling unit; maximum 2.0 spaces per dwelling unit; except as noted in Chapter 4, Parking.
Accessory buildings allowed	Up to 1 per dwelling unit

Use: A building that consists of two to eight dwelling units above and/or behind a flexible ground floor space that can be used for residential, offices, small service establishments, homecrafts which are typically considered accessory to a dwelling unit, or limited retailing associated with fine arts, crafts, or personal services or retail uses. The non-residential component of the unit shall not exceed 50% of the total gross floor area of the unit. Both the ground-floor flex space and the unit(s) directly above it are owned by one entity. This building type is especially appropriate for incubating neighborhood-serving retail and service uses and allowing neighborhood main streets to expand as the market demands.

**Access**: Upper floor units, both residential and office, share common exterior entries. Ground floor units will have individual store fronts and exterior access. Vehicular access shall be confined to side and rear streets.















## Townhouse / Rowhouse

Dwelling units	3-16 units per building
Lot width, minimum	30 feet
Building width, maximum	150 feet
Lot coverage by buildings,	50%
maximum	
Lot coverage by open space,	25%
minimum	
Building height	Determined by zoning district
Setbacks	
Front	Minimum 10 feet; maximum 20 feet
Interior Lot Line	Minimum 6 feet, except as noted in Table 5.2 Building Type
	standards Summary Table, footnote (b).
Parking requirements	Minimum 0.75 spaces per dwelling unit; maximum 2.0 spaces
	per dwelling unit; except as noted in Chapter 4, Parking.
Accessory buildings allowed	Up to 1 per dwelling unit

**Definition:** A residential building consisting of three or more dwelling units attached horizontally in a linear arrangement, with each unit having a private entrance and and having totally exposed front and rear walls to be used for access, light, and ventilation.

**Access**: Each unit has independent front and rear egress, and may have private space in the front and/or rear of the unit. If stairs are needed, they will directly connect the sidewalk to the front door.





## Multi-Family, Low

Dwelling units	6-40 units per building
Lot width, minimum	60 feet
Building width, maximum	200 feet
Lot coverage by buildings, maximum	70%
Lot coverage by open space, minimum	25%
Building height	Determined by zoning district
Setbacks	
Public Right-of-Way	Minimum 10 feet; maximum 20 feet
Interior Lot line	Minimum 6 feet, except as noted in Table 5.2, Building Type standards Summary Table, footnote (b).
Parking requirements	Minimum .75 spaces per dwelling unit, maximum 2.0 spaces per dwelling unit; except as noted in Chapter 4, Parking.
Accessory buildings allowed	Up to 2 per main (principal) building

**Definition:** A small to medimum sized building with multiple dwelling units, occupying a portion, but not all of, a city block. The dwelling units may be of mixed sizes (number of bedrooms) and styles to encourage mixed-income development and to meet the needs of families of all sizes. This building type allows for different types of housing arrangements besides single family, such as senior housing or congregate living. The building may include other uses, such as local office and commercial.

**Access**: Entry to individual units on the ground floor may be shared through one exterior entry, or units may have individual entries along the front facades. Upper floor units shall be accessed through common exterior entries. Vehicular access shall be confined to side and rear streets.















## Multi-Family, Medium

Dwelling units	40 or more
Lot width, minimum	n/a
Building width, maximum	n/a
Lot coverage by buildings, maximum	70%
Lot coverage by open space, minimum	25%
Building height	Determined by zoning district
Setbacks	
Public Right-of-Way	Minimum 10 feet; maximum 20 feet
Interior Lot Line	Minimum 6 feet, except as noted in Table 5.2, Building Type
	standards Summary Table, footnote (b).
Parking requirements	Minimum 0.75 spaces per dwelling unit; maximum 2.0 spaces
	per dwelling unit; except as noted in Chapter 4, Parking.
Accessory buildings allowed	Up to 2 per main (principal) building

**Definition:** A medium to large building with multiple dwelling units, which may occupy a portion of a city block or a full city block. The building may include other uses, such as local office and commercial. The dwelling units may be of mixed sizes (number of bedrooms) and styles to encourage mixed-income development and to meet the needs of families of all sizes. This building type allows for different types of housing arrangements besides single family, such as senior housing or congregate living.

**Access**: Units typically share exterior access with one shared entry along the front facade. Ground level non-residential units may have individual access on front facades. Vehicular access shall be confined to side and rear streets.





### Mixed Residential & Commercial

Dwelling units	n/a
Lot width, minimum	n/a
Building width, maximum	<del>n/a</del> <u>500°</u>
Lot coverage by buildings, maximum	<del>80%</del> <u>70%</u>
Lot coverage for open space, minimum	<del>20%</del> <u>25%</u>
Building height	Determined by zoning district
Setbacks	
Public Right-of-Way	Minimum 5 feet; maximum 15 feet
Interior Lot Line	Minimum 6 feet, except as noted in Table 5.2, Building
	Type standards Summary Table, footnote (b).
Parking requirements	Residential: minimum .75 spaces per unit; maximum 2
	spaces per unit, except as noted in Chapter 4, Parking.
	Commercial: minimum 1 space per 600 square feet gross
	floor area; maximum 1 space per 400 square feet gross floor
	area
Accessory buildings allowed	Up to 2 per main (principal) building

**Use**: A building type intended to provide a vertical mix of uses with ground-floor retail, office or service uses; and upper-floor office or residential uses. The commercial uses should include a range of business sizes, from small neighborhood services to large office spaces.

**Access**: Upper floor units, both residential and office, share common exterior entries. Ground floor units will have individual store fronts and exterior access. Vehicular access shall be confined to side and rear streets.

















## Commercial & Employment

Dwelling units	n/a
Lot width, minimum	n/a
Building width, maximum	<del>n/a</del> <u>500'</u>
Lot coverage by buildings, maximum	<del>80%</del> <u>70%</u>
Lot coverage for open space, minimum	<del>20%</del> <u>25%</u>
Building height	Determined by zoning district
Setbacks	
Public Right-of-Way	Minimum 5 feet; maximum 15 feet
Interior Lot Line	Minimum 6 feet, except as noted in Table 5.2, Building Type
	standards Summary Table, footnote (b).
Parking requirements	1 space per 600 square feet gross floor area, minimum;
	1 space per 400 square feet gross floor area, maximum
Accessory buildings allowed	Up to 2 per main (principal) building

**Use**: A building that contains primarily commercial uses. The ground floor shall primarily contain retail, restaurants, professional services and offices; the upper floors shall contain primarily offices and support spaces for the ground floor businesses. These commercial uses should include a range of business and retail sizes, from small neighborhood services to large office spaces, to serve a variety of local, neighborhood, and city needs.

**Access**: Upper floor units will share common exterior entries. Ground floor units will have individual exterior store fronts. Vehicular access shall be confined to side and rear streets.





## Civic & Institutional

Units per building	n/a
Lot width, minimum	n/a
Building width, maximum	<del>n/a</del> <u>500'</u>
Lot coverage by buildings, maximum	<del>80%</del> <u>70%</u>
Lot coverage for open space, minimum	<del>20%</del> <u>25%</u>
Building height	Determined by zoning district
Setbacks	
Public Right-of-Way	Minimum 30 feet from Mississippi River Boulevard and
	minimum 5 feet from other rights-of-way;
	maximum 15 feet
Interior Lot Line	Minimum 6 feet, except as noted in Table 5.2, Building
	Type standards Summary Table, footnote (b).
Parking requirements	1 space per 600 square feet gross floor area, minimum;
	1 space per 400 square feet gross floor area, maximum
Accessory buildings allowed	Up to 2 per main (principal) building

**Use**: Civic Buildings should be provided as locations that reinforce community identity and support self-government.

**Access**: Building design should reinforce accessibility for all members of the community, and entrances should be clearly discernable from the public realm. Vehicular access shall be confined to side and rear streets.

















## Parking Structures

Units per building	n/a
Lot width, minimum	n/a
Building width, maximum	<del>n/a_500'</del>
Lot coverage by buildings, maximum	<del>80%</del> <u>70%</u>
Lot coverage for open space, minimum	<del>20%</del> <u>25%</u>
Building height	Determined by zoning district
Setbacks	
Public Right-of-Way	Minimum 5 feet, maximum 15 feet
Interior Lot Line	Minimum 6 feet, except as noted in Table 5.2, Building Type
	standards Summary Table, footnote (b).
Parking requirements	n/a
Accessory buildings allowed	Up to 2 per main (principal) building

**Use:** Parking structures provide off-street parking in a spatially efficient manner and minimize the need for surface-level parking lots. This type may be accessory to a main (principal) building use or may be the main (principal) building on a lot.

**Access:** Pedestrians shall access parking structures from the front facade. Vehicular access shall be confined to side and rear streets and be subject to zoning district-specific entrance widths.





# CHAPTER 6: INFRASTRUCTURE





### 6.1 Introduction

The Ford Site will require extensive new infrastructure to support any development that happens there, including water service, sewer, stormwater, streets and utilities. Through its operation as an assembly plant, the Ford Motor Company installed services to support its independent operation and to fit its specialized needs. Much of that infrastructure was removed with site decommissioning. Most of the new services will connect into infrastructure networks that currently run along the periphery of the site to serve the Highland neighborhood.

Modern communication infrastructure on-site is essential to attract and retain strong employers, entrepreneurs, creative industries and residents. Broadband capability will be an essential element of this infrastructure and therefore conduit should be installed during construction of new public streets to ensure the Ford site is ready for installation of fiber optic cable.



## 6.2 Transportation Network

The Ford site as a large-scale industrial site acted as a barrier within the Highland neighborhood, elongating trips in order to move around it and contributing to congestion at its periphery. The redeveloped property will remove this barrier and reweave the site into the area transportation network, ensuring access for all modes of transportation. The site will provide multiple connections to the surrounding Highland neighborhood and to the Mississippi River, enabling live, work, and play opportunities in a compact, mixed-use neighborhood.

These connections will build on the robust options already available in the Highland neighborhood. The A Line provides high quality Bus Rapid Transit service on Ford Parkway, with ridership well exceeding expectations. Growing pedestrian and bicycle amenities in the region are serving the already walkable, diverse neighborhood.

Meanwhile, there are openings for transformative changes, such as making direct connections to Mississippi River Boulevard and the regional trail system along it, and the city's aspiration to convert the Canadian Pacific Railway Ford Sspur into a regional facility for bicyclists and pedestrians, possibly co-located with transit.

After full redevelopment and with the recommended improvements, conditions for pedestrians and cyclists in and around the Ford site improve dramatically, with manageable vehicle travel.

The street sections shown in this document reflect general design and function. Future design and engineering will use the Saint Paul Street Design Manual for additional guidance, particularly when adding pedestrian-friendly elements such as bump-outs and crosswalks.





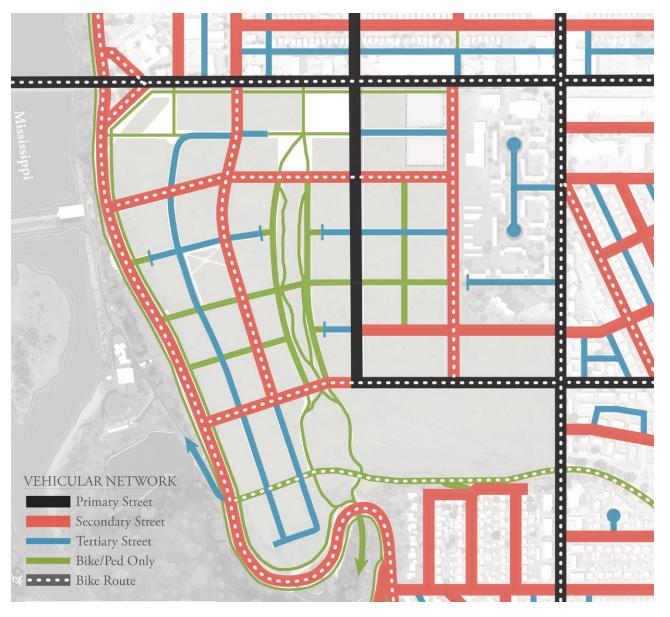




## 6.3 Street Network System



The street network consists of nonvehicular routes (green) through the site to augment the vehicular street grid (gray) and better connect to the neighborhood and Mississippi River.







### Cretin Avenue with Transit

Cretin Avenue is one of the main north-south roadways on the site. It extends the Avenue south to connect to Montreal. Space has been allocated for future enhanced transit service. Commercial activity on Cretin would be stronger than on other streets on the site.





TOTAL RIGHT-OF WAY		93
CURB TO CURB DISTANCE		65
VEHICULAR	2-way (feet/lane)	11
	Median/turn lane (feet)	11
	Curb reaction distance per	2
	side (feet)	
	Total Vehicular ROW	37
PEDESTRIAN	Sidewalks (feet/side) *	8
	Boulevard (feet/side)	6
	Total Pedestrian ROW per side	14

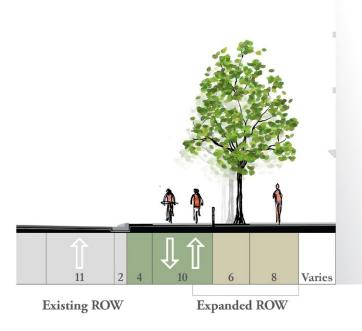
\*Sidewalks south of Village Lane would decrease to 6'





## Ford Parkway

The existing Ford Parkway right-of-way will be extended eighteen (18) feet to the south between the Ford Parkway bridge on the west and the Finn Street alignment on the East. The added right-of-way will include a dedicated, bi-directional bike lane, a planted boulevard and an eight foot sidewalk. The wider boulevard and sidewalk are intended to provide more space for landscaping, public seating, and decorative paving and public art to serve as an attractive and comfortable pedestrian way connecting the site and Highland Village. Due to the important connections that exist beyond the boundary of the Ford Property, additional study is warranted for the design and functionality of Ford Parkway between Ford bridge and Cleveland Avenue.





EXPANDED RIGHT-OF WAY		18
BIKE	2-Way Path	10
	Total Bike ROW	10
PEDESTRIAN	Sidewalk	8
	Boulevard	6
	Total Pedestrian ROW	14





## Montreal Avenue - West of Cretin

Montreal Avenue between Mississippi River Boulevard and Cretin Avenue is a two lane roadway with single direction, dedicated bicycle lanes next to the traffic lanes. There are no on-street parking lanes. A six foot tree-line boulevard and six-foot sidewalk line the edges.



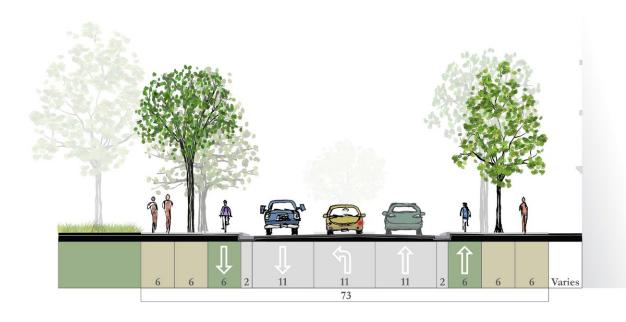


TOTAL RIGHT-OF WAY		62
CURB TO CURB DISTANCE		42
VEHICULAR	2-way (feet/lane)	11
	Curb reaction distance per	2
	side (feet)	
	Total Vehicular ROW	26
BIKE	Separated - 1 per side	6
	(feet/lane)	
	Total Bike ROW	12
PEDESTRIAN	Sidewalk (feet/side)	6
	Boulevard (feet/side)	6
	Total Pedestrian ROW per side	12



## Montreal Avenue - East of Cretin

Montreal Avenue between Cretin Avenue and Cleveland Avenue serves as the main street accessing the site from the east. It is designed as a through street with two lanes of travel and a center turn lane, dedicated bicycle lanes next to the traffic lanes, and no on-street parking. A six foot tree-lined boulevard and six-foot sidewalk line the edges.





TOTAL RIGHT-OF WAY		73
CURB TO CURB DISTANCE		53
VEHICULAR	2-way (feet/lane)	11
	Median/turn lane (feet)	11
	Curb reaction distance per	2
	side (feet)	
	Total Vehicular ROW	37
BIKE	Separated - 1 per side	6
	(feet/lane)	
	Total Bike ROW	12
PEDESTRIAN	Sidewalk (feet/side)	6
	Boulevard (feet/side)	6
	Total Pedestrian ROW per side	12





# Mount Curve Boulevard (North)

Mount Curve Boulevard between Ford Parkway and Beechwood Avenue is an access road into and out of the site. It has two lanes of travel and a center turn lane, dedicated bicycle lanes next to the traffic lanes, and no on-street parking. A four foot tree-lined boulevard and six-foot sidewalk line the edges. The center median could be planted when not needed for vehicular movements to continue the pattern from the north.



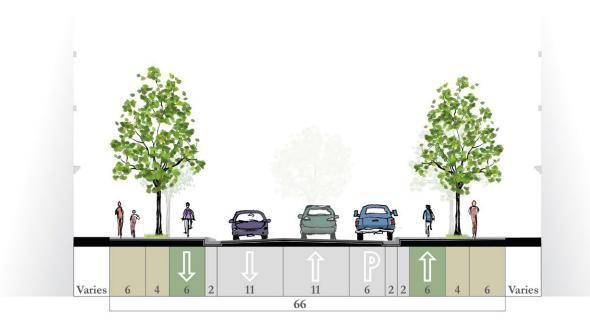


TOTAL RIGHT-OF WAY		68
CURB TO CURB DISTANCE		52
VEHICULAR	2-way (feet/lane)	11
	Median/turn lane (feet)	10
	Curb reaction distance per	2
	side (feet)	
	Total Vehicular ROW	36
BIKE	Separated - 1 per side	6
	(feet/lane)	
	Total Bike ROW	12
PEDESTRIAN	Sidewalk (feet/side)	6
	Boulevard (feet/side)	4
	Total Pedestrian ROW per side	10



# Mount Curve Boulevard (South)

Mount Curve Boulevard south of Beechwood Avenue is a local street with two lanes of travel, one side of on-street parking, and dedicated bicycle lanes in each direction. The bicycle lane on the east is buffered from traffic by the parking lane. A four foot tree-lined boulevard and six-foot sidewalk line the edges.





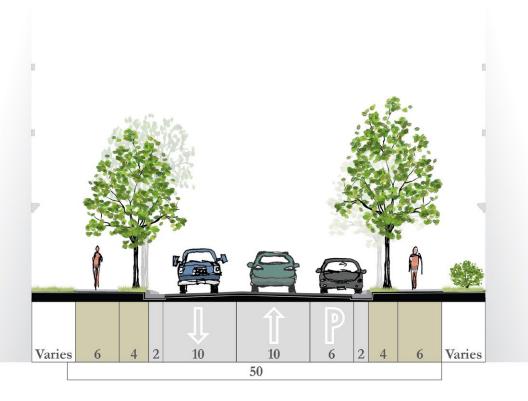
TOTAL RIGHT-OF WAY		66
CURB TO CURI	CURB TO CURB DISTANCE	
VEHICULAR	2-way (feet/lane)	11
	Curb reaction distance per	2
	side (feet)	
	Total Vehicular ROW	26
PARKING	1-sided parking (feet/lane)	6
	Total Parking ROW	6
BIKE	Separated - 1 per side	6
	(feet/lane)	
	Door zone	2
	Total Bike ROW	12
PEDESTRIAN	Sidewalk (feet/side)	6
	Boulevard (feet/side)	4
	Total Pedestrian ROW per side	10





# Woodlawn (North) and Beechwood

These are narrow, local streets with two lanes of travel and one side of on-street parking. A four foot tree-line boulevard and six-foot sidewalk line the edges.

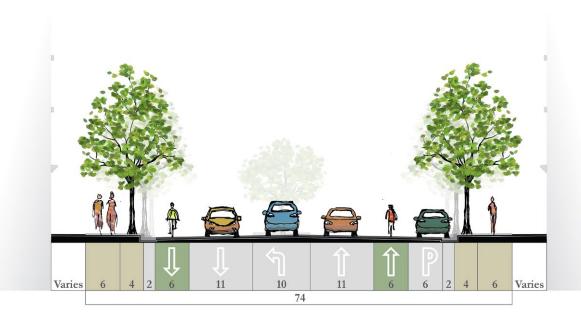




50
30
10
2
24
6
6
6
4
10

### **Bohland Avenue**

Bohland Avenue is one of the main east-west roadways on the site. It connects Mississippi River Boulevard in the west to Finn Street in the east. Street parking is allowed on one side of the street for access to the square, retail district, and stormwater feature. There are dedicated bike lanes in each direction. A turn lane allows access to parking. A four foot tree-lined boulevard and six-foot sidewalk line the edges.





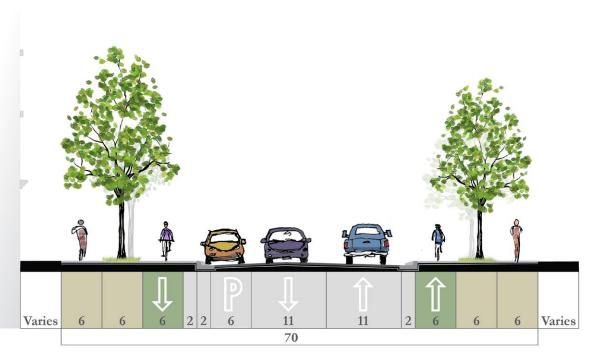
TOTAL RIGHT-OF WAY		74
CURB TO CURI	B DISTANCE	58
VEHICULAR	2-way (feet/lane)	11
	Median/turn lane (feet)	10
	Curb reaction distance per	2
	side (feet)	
	Total Vehicular ROW	36
PARKING	1-sided parking (feet/lane)	6
	Total Parking ROW	6
BIKE	Separated - 1 per side	6
	(feet/lane)	
	Total Bike ROW	12
PEDESTRIAN	Sidewalk (feet/side)	6
	Boulevard (feet/side)	4
	Total Pedestrian ROW per side	10





## Finn Street

Finn Street connects between Ford Parkway and Montreal Avenue, offering an alternative route for north-south travel on and through the site. It has two lanes of travel, a parking lane on the west side, and dedicated bicycle lanes in each direction. The bicycle lane on the west is buffered from traffic by the parking lane. A six foot tree-lined boulevard and six-foot sidewalk line the edges.





TOTAL RIGHT-OF WAY		70
CURB TO CURB DISTANCE		32
VEHICULAR	2-way (feet/lane)	11
	Curb reaction distance per	2
	side (feet)	
	Total Vehicular ROW	26
PARKING	1-sided parking (feet/lane)	6
	Total Parking ROW	6
BIKE	Separated - 1 per side	6
	(feet/lane)	
	Door zone	2
	Total Bike ROW	12
PEDESTRIAN	Sidewalk (feet/side)	6
	Boulevard (feet/side)	6
	Total Pedestrian ROW per side	12



# Saunders Avenue (East)

Saunders Avenue is the only road, other than Montreal, offering an east connection to the site. It runs two blocks between Cleveland and Cretin Avenues. It has two lanes of travel, a center turn lane, and a parking lane on the north side. A four foot tree-lined boulevard and six-foot sidewalk line the edges.





TOTAL RIGHT-OF WAY		60
CURB TO CURI	B DISTANCE	44
VEHICULAR	2-way (feet/lane)	10
	Median/turn lane (feet)	10
	Curb reaction distance per	2
	side (feet)	
	Total Vehicular ROW	34
PARKING	1-sided parking (feet/lane)	6
	Total Parking ROW	6
PEDESTRIAN	Sidewalk (feet/side)	6
	Boulevard (feet/side)	4
	Total Pedestrian ROW per side	10





## Hillcrest

Hillcrest Avenue is a short connector street linking Finn Street and Cretin Avenue. It is intended for local circulation and to provide access to interior parking and building services. It has two lanes of travel and a center turn lane. A six foot tree-lined boulevard runs along the south side and an eight foot tree-lined boulevard along the north, to provide more access to sunlight. A six foot sidewalk runs along each side.

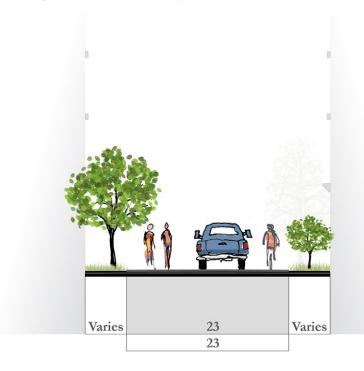




TOTAL RIGHT-OF WAY		60
CURB TO CURB DISTANCE		34
VEHICULAR	2-way (feet/lane)	10
	Median/turn lane (feet)	10
	Curb reaction distance	2
	per side (feet)	
	Total Vehicular ROW	34
PEDESTRIAN	Sidewalk (feet/side)	6
	Boulevard (feet/side)	6,8
	Total Pedestrian ROW per	12,14
	side	

# Woodlawn Lane (South)

Woodlawn Lane is a shared lane for local travel only to the adjacent blocks and residences. There is no demarcation within the 23 foot wide right-of-way to separate cars, pedestrians and bicycles. All users will share the lane and travel speeds will be very low. Since pedestrians and bicyclists are intended to use the lane for travel, there is no adjacent boulevard or sidewalk space. A private setback for vegetation and driveways will separate buildings from the roadway.





TOTAL RIGHT-OF WAY		23
CURB TO CURB DISTANCE		23
VEHICULAR		
BIKE	Shared	
PEDESTRIAN		





# Falls Passage (East and West)

Falls Passage is the main north-south pedestrian and bicycle connection through the site, connecting the civic square and Highland Village in the north to Hidden Falls and the Mississippi River beyond. The rights-of-way line the stormwater feature, which will be designed and engineered in the future. The paved area would allow emergency vehicle access and the boulevard space would allow for amenities to support the pathway and stormwater feature.



For illustration only. Design and engineering to be completed at a future date.







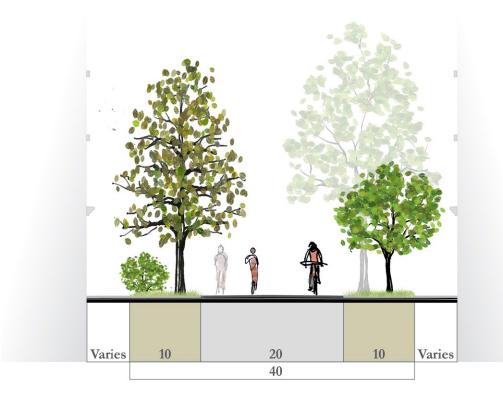


TOTAL RIGHT-OF WAY (Each)		34
PEDESTRIAN	Sidewalk (feet/side)	20
	Boulevard (feet/side)	14
	Total Pedestrian ROW per side	34



# Village Way (West)

Village Way serves as the main east-west pedestrian and bicycle way through the site. It connects the existing neighborhood and development in the site to the Mississippi River. The landscaped areas are wide to enhance the park-like experience of connecting important pieces of the public realm. The paved section is wide enough to allow emergency vehicle access.

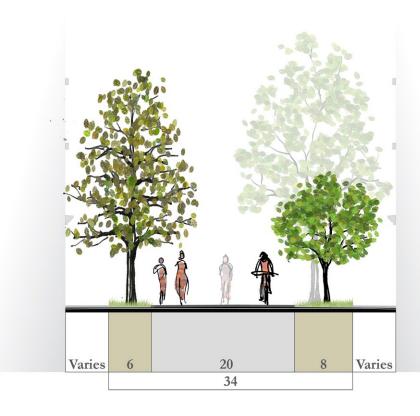




TOTAL RIGHT-OF WAY		40
BIKE	Shared	20
PEDESTRIAN		
BOULEVARD	Feet per side	10

# Village Way (East)

Village Way to the east of the stormwater feature has the same function as the west. The spacing on the boulevard is offset to increase the area receiving greater solar access since the allowed heights in this area are greater.





TOTAL RIGHT-OF WAY		34
BIKE	Shared	20
PEDESTRIAN		
BOULEVARD	Feet per side	6,8



# Ranger Way

Ranger Way serves as a linear courtyard and connection within the area of greatest density on the site. A shared pedestrian and bicycle way is wide enough for emergency vehicular access.

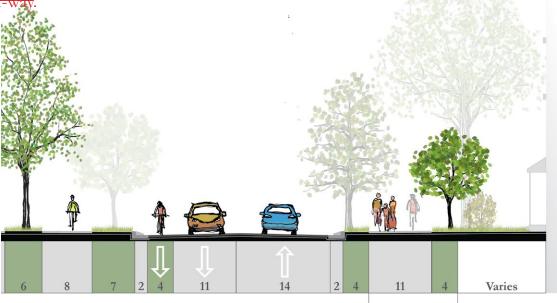




TOTAL RIGHT-OF WAY		32
BIKE	Shared	20
PEDESTRIAN		
BOULEVARD	Feet per side	4,8

# Mississippi River Boulevard Trail

A multi-use trail on the east side of Mississippi River Boulevard would allow safer and more enjoyable use of the Boulevard by giving people the option to move on that side. This plan does not propose extending the trail further south than a Hidden Falls connection. This plan shows the current alignment of Mississippi River Boulevard remaining as-is. However, in the event that a possibility to expand Hidden Falls Regional Park at the blufftop emerges, realignment of Mississippi River Boulevard at the southern end of the site should be considered to accommodate the park boundary change. , nor does it make recommendations for altering the exising Mississippi River Boulevard right-



**Existing ROW** 



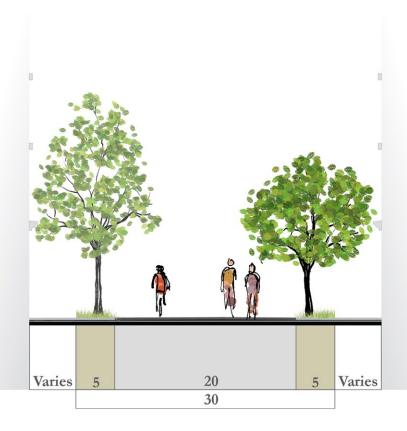
EXPANDED RIGHT-OF WAY		15
BIKE	Combined Path	11
PEDESTRIAN	Combined Path	
BOULEVARD		4



**Expanded ROW** 

# Galaxie Way (West of Stormwater)

Galaxie Way west of the stormwater corridor breaks up a potentially long block and allows more access options between the Mississippi River and the stormwater feature.





TOTAL RIGHT-OF WAY		30
BIKE	Shared	20
PEDESTRIAN		
BOULEVARD	Feet per side	5

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## 6.4 Stormwater Management System Plan and Standards

### Vision

Re-create the historic Hidden Falls Headwaters feature, naturalize the existing downstream creek, reconnect the future neighborhood to the river by means of an open-water flow path, and create a model for sustainable and resilient infrastructure development.

### Stormwater Goals



Incorporate a naturalized Hidden Falls and restored Hidden Falls Creek into the Ford Site



Treat stormwater as a resource, and not a waste stream



Move toward fulfilling the City's stated sustainability and public realm goals for the project



Increase community benefits while reducing big-picture impacts and conserving energy, water, and resources



Generate a higher sustainable return on investment by providing more big-picture community benefits per dollar invested

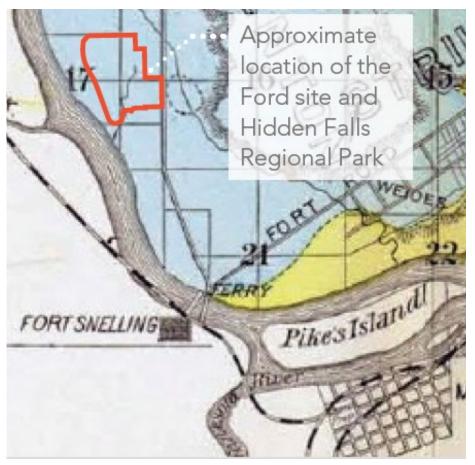




### Historical Perspective

The current condition of Hidden Falls Regional Park and Hidden Falls Creek represents the geological history along the Mississippi River, as well as recent activity associated with the Ford site. Prior to the development of the Ford Motor Company complex, a stream originated near what is now Cleveland Avenue and Ford Parkway—flowing southwest across the Ford site to Hidden Falls. That stream was buried during subsequent development, flowing through a culvert beneath Mississippi River Boulevard and daylighting at Hidden Falls.

The hydrology of Hidden Falls Creek is primarily generated as stormwater runoff from the surrounding watershed. The urbanization of the watershed, in conjunction with the nearly 100-foot elevation change from the falls to the river, has created a high-energy stream system.



Map of Ramsey County, 1867 (Winchell)





DRAFT (7/21/2017) - For review by the Saint Paul Planning Commission

## The Centralized Stormwater Concept

Runoff from the entire site will be directed to and managed in a primarily above-grade centralized green infrastructure corridor. The corridor would re-create the original headwaters feature. Downstream, Hidden Falls Creek would be restored and associated natural areas linked to the development. The community would benefit from green space and developers would not be required to manage stormwater on individual parcels.



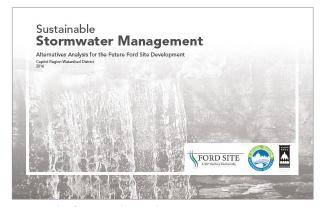












More information about stormwater design and performance can be found in the report, Sustainable Stormwater Management: Alternatives Analysis for the Future Ford Site Development.

### Restoration Potential

Redevelopment of the Ford site also offers the opportunity to protect and restore some of the area's natural resources: Hidden Falls Creek and Hidden Falls. The creek, which once meandered across the Ford site to Hidden Falls, was buried prior to the plant's construction and operation (1925). As a result, high stormwater runoff rates and volumes have eroded the area below the falls. A Ford Site stormwater management system shall emphasize strategies for collection, treatment, and release to achieve the following:

- Restoration of Hidden Falls Creek with a more naturalized flow regime.
- Reduction of stormwater runoff rates to their pre-settlement levelssubsequently reducing erosion, returning the surface water-groundwater connection, and improving resiliency.
- **Development of a natural corridor amenity**, linking the redeveloped area to Hidden Falls Creek and Hidden Falls.

### Design Recommendations:

- Provide visual and material consistency between the site and Hidden Falls
  Regional Park. Design should be consistent with renovation plans for Hidden Falls
  Regional Park.
- Use natural features and materials, such as limestone boulders and native vegetation, to improve aesthetics, reduce erosion mitigation, and allow for ease of maintenance.
- Support educational opportunities to discuss the historical and ecological significance of Hidden Falls Regional Park.

Stormwater management is just one part of the City's vision. A balanced blend of grey and green infrastructure will optimize use of developable land and ensure that residents and businesses benefit from a livable community, while impacts to the area's ecosystem are reduced.





# CHAPTER 7: PARKS AND OPEN SPACE





## Introduction

Parks and open spaces at the Ford site are an essential element of a healthy, attractive and vibrant community. They improve the experience of people at the site and enhance value for adjacent and nearby users. Parks and open spaces expected at the site are categorized as one of six (6) major types:

- Gateway Park
- Civic Square
- Neighborhood Park
- Pocket Parks
- Hidden Falls Headwater Park
- Recreational Fields

Each type is outlined below with a general description, location map, example images, and identifies key elements for the space. Design and performance standards for specific elements are provided in the Ford Site Open Space Guidelines Report, found on the City's Ford web pages.



## Open Space System



### PUBLIC OPEN SPACE

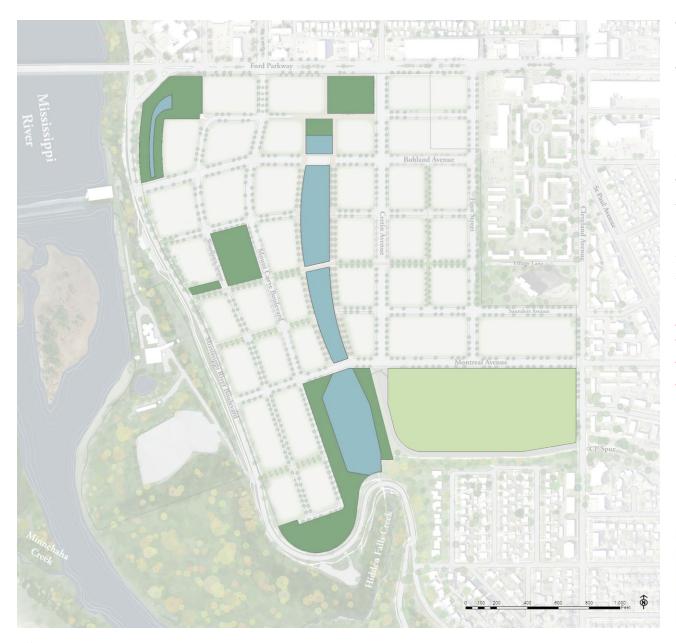
- 1 Gateway Park
- 2 Civic Square
- 3 Neighborhood Park
- 4 Pocket Park
- (5) Hidden Falls Headwaters Feature
- 6 Walking and Biking Paths

### NON-PUBLIC OPEN SPACE

(7) Recreational Fields



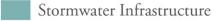
## Open Space Types



The open space system is intended to feel continuous in everyday experience. There are three distinct categories of open space types. The first is the traditional city park, which is acquired through parkland dedication and would become part of the city's park system. The second is the stormwater spine, which may feel like a park, but technically serves a utility function. A third category is a non-public recreation space, which is neither owned nor operated by the city, but is an allowed use in all zoning districts. It is included here to reflect the vision of the community to see a continuation of recreational fields on the site.

### **OPEN SPACE TYPES**





Non-Public Recreation





## Gateway Park

A gateway park at the northwest corner of the site will provide one of the first impressions of Saint Paul as people come over the Ford Parkway Bridge from Minneapolis, and will serve as a key visual entry into the site. The gateway park will be at the juncture of Mississippi River Boulevard and Ford Parkway, at the intersection of commerce and nature. Design of the park can serve as a transition between these two worlds, incorporating a mix of attractive hardscape and vegetation. The park should serve as a vibrant gathering place for the community and visitors to the area. Public art, design and amenities can provide a distinctive and unique character to the place, fitting to its role as a "gateway."

- Public art
- Water feature
- Public/private seating and tables for eating, games, other
- Picnic area
- Playground
- Dog park
- Community garden















## Civic Square

A civic square will serve as the focal point for community gathering throughout the day and year, for employees, residents, visitors and the Highland Community. The square will be located on the north end of the site near Ford Parkway, providing a link between the commerce and activity of Highland Village and the newly developed Ford site.

The public square will be lined with retail, service, residential and office uses, providing a critical mass of activity and people around the space. The square will be a pedestrian-only space, with vehicular access only for deliveries, cleaning, and emergency during restricted hours and from designated access points. Tenants choosing the office, residential and business frontages on the civic square, will be those that thrive in active, pedestrian environments, and seek a unique, place-based location that is rare to find in the region. All buildings lining the square will have vehicular access at the rear or in structured parking.

Potential elements for considered inclusion in the civic square are identified below. Particular emphasis will be placed on creating a flexible space that allows for a range of activities and community events throughout the year. The square is envisioned to be well lined with active commerce, particularly seasonal outdoor dining.

- Public art
- Water feature
- Flex stage and gathering area for performances, markets, etc.
- Public/private seating and tables for eating, games, other





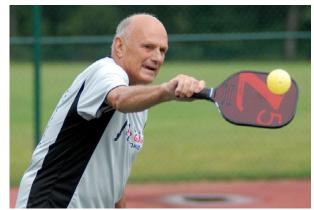
## Neighborhood Park

A neighborhood park towards the southern end of the site will provide a new community location for outdoor play and recreation for all ages. Key elements of the park will include a picnic area, a playground, and an open field for pick-up games and community gatherings. The park will be designed for all ages, providing a place to actively enjoy the outdoors for a variety of ability levels. The park will have a natural character and landscape design, with perhaps some formal garden or planting areas. If space and design allows, other desired elements to include in and near the neighborhood park are a designated off-leash dog area, a community garden, a flex athletic field, and an ice skating area. The neighborhood park will be closely linked to other recreation and parks both on and off the site with a series of walking and biking paths.

- Picnic area
- Playground
- Open field
- Community garden
- Dog park
- Ice skating
- Small sport facilities

















### Pocket Parks

Pocket parks are small, public open spaces, typically less than 1-acre, where people can relax, play or gather outdoors. They are tucked into and scattered throughout a neighborhood to serve a variety of needs. Functions can include small event space, play areas for children, spaces for relaxing or meeting friends, taking lunch breaks, and enjoying the outdoors. They can be a refuge from the bustle of surrounding urban life and offer opportunities for rest and relaxation. Pocket parks may be owned and maintained publicly or privately, as long as they are open for use to all members of the public. Pocket Parks are a useful tool to activate the public realm and utilize small undeveloped areas of land.

- Places to sit.
- Play activity for kids, adults and/or seniors
- Attractive landscaping
- Small space for public gathering or events
- Small formal gardens or community garden plots
- Dog suitable area for on-leash visitors





### Hidden Falls Headwater Feature

The Hidden Falls Headwater Feature at the southern end of the site will act as a smooth transition between the open space network of the Ford site and Hidden Falls Regional Park. The area will use natural landscaping and support passive uses by park goers. It will serve as a major access point for both human users and wildlife to move between the park and new development, and should be designed for the safety and accessibility of users.

Hidden Falls Regional Park is vulnerable to flash floods from heavy rainfall and water runoff from existing and future upstream development. The headwater feature should be engineered to channel water from a stormwater management system on the Ford Site and through Hidden Falls Park in such a way as to reduce erosion and flooding downstream.

- Bike and pedestrian trails
- Small picnic and rest areas, with vistas to Hidden Falls Park and the Mississippi River
- Safe access points between new Ford development and Hidden Falls Regional Park













### Recreational Fields

New recreational fields for private, non-profit recreational groups, can continue to serve the needs of youth and neighborhood sports organizations, such as Highland Little League. The fields would be best located in the southeast corner of the site to minimize any negative impacts of activity and lights on residential properties. The fields could be connected to the wider public park network through bike and pedestrian trails. In addition to the recreational fields, support facilities will be built on site to serve the athletes and visitors.

- Recreational fields
- Restrooms
- Concession building
- Storage building
- Play and picnic area with seating, tables, and shelter
- Some on-site surface parking and drop-off / pick-up area
- Bike racks









## 8.1 Introduction

In keeping with the visionary aspirations for the Ford Site Redevelopment established by the City of Saint Paul, by the public in planning meetings, and by the Ford Site Task Force, the public art for this new mixed-use area will be equally visionary and will advance the overall goals for this new urban village. This redevelopment presents a once-in-a-century opportunity to build from the ground up (and even from the underground up) a 122-acre development within an already developed urban context. For public art, this presents a rare opportunity to integrate public art into an entire neighborhood from the outset of its design, not only into one site or one building. It also offers the opportunity for public art approaches to underpin the public art planning process. Public art will be a critical element to make this new neighborhood a global model of progressive urban design.

A public art plan will employ the leading ideas and processes in the field of public art and urban design today. "Tactical urbanism," an approach that tests design ideas with the public before committing to projects and resources, will be adopted, as will ongoing community discussions set in neighborhood gathering spaces over at least a year-long period. This dynamic, durational approach will ensure that the public art plan will be relevant, utilized, and impactful. In addition to a written, illustrated public art plan, we will also include public presentations of the final plan to share its recommendations with developers, community organizations, residents, artists, and arts organizations.



## 8.2 Vision

Public art will be integrated systematically into this large site to advance good design of urban infrastructure, public places, green spaces, relationships to the Mississippi River and Hidden Falls, and to link new development into the Highland Park context. Public art will help to foster vibrant social life of the community, inviting people of all ages and backgrounds to share public spaces and enjoy time together.

Artists will play leading roles in shaping an overall plan for public art for this redevelopment, viewing the site in a systems-based, networked way for elements of place, infrastructure, mobility, mixed-use spaces, play spaces, food spaces, and more to work together. Some artists will design community engagement opportunities to involve the public in imagining what could be created. Some artists will be commissioned to create temporary public art prototypes that can be tested and experienced by the public before plans are refined for permanent features. Some artists will serve as consultants to advise those leading the public art plan, spurring new ideas and approaches.



Fish Stairs, Seoul, South Korea





### 8.3 Public Art Processes and Possibilities

Drawing from the City's goals for the Ford Site redevelopment and the public hopes expressed at public meetings and Pop Up meetings, this plan identifies 5 themes to guide approaches to the public art at the site:



The Clear Orb (proposed), Land Art Generator Initiative



Beckoning Cistern, Vine Street, Seattle

### 1. Futuristic/Future Imaginary

Underpinning the redevelopment of the Ford Site is the importance of manifesting advanced design ideas and aspirational values about how our neighborhoods are planned and designed, fostering new modes of everyday living. Futuristic design, materials, technologies, and imagery in public art will express and embody hopeful visions of the future, while also serving as models of new design approaches to other neighborhoods.

### 2. Sustainability

To support low impact development, public art in this development will be integrated into green infrastructure; reduce use of carbon-based energy sources; promote less reliance on car transportation; and promote everyday practices that use or reduce waste and use of chemicals. All will promote a more sustainable relationship with the extraordinary natural setting within which this development is sited.



#### 3. Health

Public art that promotes physical activity, meditative experiences, access to healthy foods, and enjoyment of outdoor green spaces, will promote greater human health. Public art located in engaging gathering spaces will bring neighbors together to see, interact with, and enjoy each other, promoting greater social health and cohesion.





Left: Inmotion: Memories of Invented Play, Boston Right: Vessel, Sculpture to be Climbed (proposed), New York

### 4. Heritage

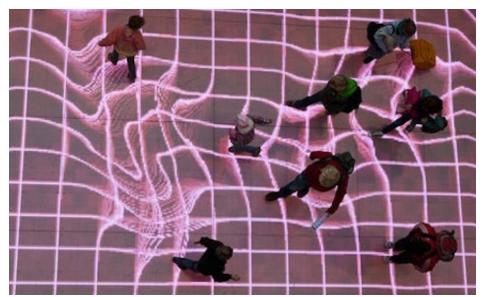
Public art will make visible the layered histories of the Ford Site neighborhood, from Native American stories about important places to the Ford Motor Company manufacturing plant history. The cultures of diverse populations that have settled the area in the past and in more recent times will inspire art approaches and projects.





Left: Heritage Park, Lake Union, Seattle Right: Mural of Cornish Mining Heritage, Devon, England





Onskebronn Interactive Plaza, Berlin

Interactive musical swings, Montreal, Canada

### 5. Playful Interactivity

To invite participation and enjoyment by residents and visitors, public art will take playful approaches and use interactivity rather than presenting only static sculptures to contemplate. Interactivity can be integrated into artworks, using old and new technologies, to involve people as co-creators in experiences. Playfulness promotes happiness and health and brings people together across generations and cultures.



## 8.4 Public Art Approaches

Myriad innovative ideas and approaches can grow out of this unique public art planning process. Rather than proscribe in advance the kinds of public art that could emerge at this early point, the process will build on ideas already generated in public meetings and then expand the voices and visions of the new urban village's future.

Here are some ideas that could inspire exciting directions:

### Rotating Outside Exhibit

Temporary Contemporary Curator and Programmer to activate space through outdoor exhibition/art work that is rotated annually and kept alive. Keep things fresh and invite artists to test ideas on an ongoing basis.



Lawrence Weiner Mural (Fourth on this wall), Boston

### Integrated System-Based Public Art

Public art that works within infrastructure to transform everyday city design and everyday city experiences. Projects that have already been tested and loved in the City of Saint Paul are Sidewalk Poetry and Artful Stop Sign Posts. What other systems could public art shift to make a whole new feel for this part of the city?



Sidewalk Poetry, Saint Paul







Northern Clay Center, Minneapolis, MN

### Residency/Studio/Gallery

A permanent art space for community workshops, shows, and residencies. Create a permanent space for continual engagement, a space that could host artists from across the world, show work all year round, and host community conversations.



Arco, Madrid

### Gateway Piece

Use public art to mark that people are entering a space of the future. Commission a major work of public art that could be Saint Paul's "Spoonbridge and Cherry" (by Claus Oldenburg and Coojse Von Bruggen in Minneapolis Sculpture Garden) and represent not only the Ford Site but also the whole city. It could be an artwork that lets people know they are entering a truly different place, a new urban village.



# CHAPTER 9: SUSTAINABILITY





### 9.1 Vision

The redeveloped Ford site will advance the key elements of sustainability -- economic, environmental and social. It will be a livable, mixed-use neighborhood that looks to the future with clean technologies and high quality design for energy, buildings and infrastructure. The site will support walking, biking and transit, and provide jobs, services, housing and activities that every generation can enjoy.

The Ford site should be a...

- Global model of sustainable, urban infill and carbon neutral design
- Neighborhood of regional significance and economic value
- Vibrant, fun place to live, work and play along the Mississippi valley
- Center of family-sustaining jobs
- Diverse blend of housing types and affordability levels
- Walkable, bikeable and transit oriented community
- Place for recreation, active lifestyles and leisure in a series of connected and distinctive parks, trails and open spaces
- Extension of the high quality shopping and services of Highland Village
- Demonstration site for the best technologies in infrastructure and buildings
   -- saving money, increasing efficiency, and reducing impacts on the environment



### 9.2 Roadmap to Sustainability for the Ford Site

### Background

The Roadmap to Sustainability for the Saint Paul Ford Site report completed in 2011, provided goals, performance standards and strategies to achieve a broad agenda of sustainability at the redeveloped Ford site. Many of the goals identified are advanced through the Zoning and Master Plan for the site, while others remain to be pursued through policy or other mechanisms. The goals, organized in eleven categories, should be advanced to the extent possible throughout site redevelopment. Environmental sustainability goals are:

### 1. Building Energy

- To maximize the use of renewable energy for buildings and infrastructure.
- To reduce operating energy use in all buildings and infrastructure.
- To maximize energy self-sufficiency.

### 2. Transportation and Public Realm Network

- To create a transportation infrastructure that balances modal choice between walking, biking, and vehicular movement.
- To reduce average vehicle miles driven by persons living, working and visiting the site.
- To increase average walking and biking miles per year for persons living or working on the site.
- To reduce energy use and Green House Gas (GHG) emissions related to high vehicle miles driven (VMD).
- To reduce adverse human health affects (such as asthma) related to air pollution.
- To maximize the diverse human benefits (such as childhood obesity reduction and lower family transportation costs) of safe and pleasurable pedestrian and multi-modal access to and from (on-site & off-site) transit stops, daily services, institutions, parks and public spaces.

#### 3. Materials

• To reduce embodied energy use, GHG emissions and other environmental impacts associated with building, infrastructure, and landscape materials.





#### 4. Water and Wastewater

- To reduce potable water consumption in all buildings and landscapes.
- To reduce wastewater leaving the site to treatment plants from all buildings and landscapes by increasing onsite wastewater reuse.

#### 5. Solid Waste

- To reduce solid waste from construction in all buildings and landscapes.
- To reduce solid waste from operation of all buildings and landscapes.

#### 6. Stormwater and Groundwater

- To minimize surface and ground water pollution.
- To minimize negative impacts of development on the hydrological cycle by treating stormwater as a resource and recharging groundwater through infiltration as local soils and subsurface conditions allow.
- To not exceed natural erosion and sedimentation levels in streams and lakes.
- To protect plant, invertebrate, and animal life in lakes and streams.
- To utilize stormwater runoff as a resource rather than as a waste product.
- To pre-treat all water flowing to Hidden Falls and maintain a more constant flow volume.

#### 7. Soil

- To protect and restore soil structure, stability, and biological health to optimize plant health and species richness and optimize water infiltration and filtration.
- To reduce soil loss and minimize disturbance of existing quality soil.
- To maximize on-site reuse of existing soils.
- To address impacted soil conditions on site.





### 8. Vegetation and Habitat

- To maximize biodiversity of the site and provide maximum possible contribution to local landscape ecology.
- To reduce destruction and removal of existing vegetation.
- To increase vegetation on site with new plantings.
- To provide wildlife habitat.
- To maximize ecological services on site and for the surrounding area.

### 9. Recreation and Public Space

- To improve personal health through increased physical activity, by providing on site facilities for a variety of active and passive exercise and recreational choices such as recreational walking and biking, informal play, or participation in organized sport activities.
- To encourage the development of (and connections to) biking and walking trails within, to, from and through the site.
- To encourage provision of and/or access to a comprehensive set of public gathering spaces for a full range of civic and community events.
- To provide space for community gardens, local agriculture, and the sale of locally-grown food.

### 10. Night Sky Radiation

- To reduce light emitted from site to the sky at night.
- To protect the environments of predator and prey.

#### 11. Urban Heat Island

- To reduce urban heat island effects on site by reducing the heat absorption of materials used in buildings, landscaping and infrastructure.
- To increase vegetative cover to help keep the site and buildings cool in the summer.
- To reduce the need for air conditioning and irrigation in the summer.





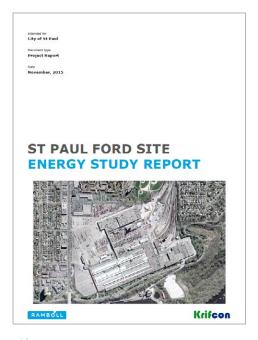
### 9.3 Sitewide Energy System

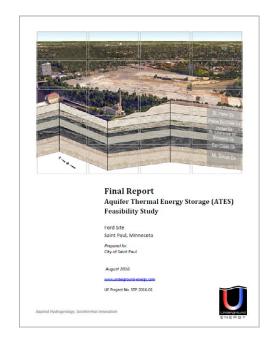
This project will build on Saint Paul's demonstrated success of initiatives to improve energy efficiency, reduce fossil fuel use and expand renewable energy generation, but will take it to the next level by striving for a net-zero community.

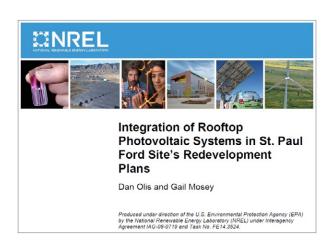
The foundation of a sustainable Ford site redevelopment is a site-wide, integrated energy system that incorporates renewable energy sources and design efficiencies to reduce demand, reduce carbon emissions, and reuse energy.

The site will be redeveloped from scratch starting with installation of new utilities, streets, sewers and water. This provides an unprecedented opportunity to design and install a comprehensive and integrated energy system using the best, cutting edge technologies and systems appropriate to site conditions.

A series of site-specific energy studies are examining these options and identifying a path to implementation. Local utilities, in partnership with the city, are leading this effort and will provide a recommended energy system plan to the future site developer(s).











### 9.4 Affordable Housing

#### Overview and Intent

Housing affordability is an important need across the region, the city, and within the Highland Park neighborhood. The Ford site provides a vital opportunity to provide affordable housing for the community and the city. Public and private entities will pursue implementation of affordable housing goals for the redeveloped site through policy making, funding strategies, and developer selection.

Housing Affordability Goals for the Ford site:

- 10% of housing units should be affordable to households earning 60% of Average Median Income
- 10% of housing units should be affordable to households earning 50% of Average Median Income
- Affordable units should be a mix of housing types, including townhomes, rental, ownership and senior
- Provide some affordable units within mixed-income buildings -- a blend of market-rate and affordable units
- Locate affordable units throughout the site; do not cluster or concentrate them in one area



Sibley Court, Saint Paul



Frogtown Square, Saint Paul



East Lake Rowhouses, Seattle



### 9.5 City of Saint Paul Sustainable Building Policy

#### Overview and Intent

The City of Saint Paul and the Saint Paul Housing and Redevelopment Authority have sustainable development policies for public and privately developed project receiving more than \$200,000 in public financing.

Saint Paul's Sustainable Building Policy is intended to help reduce environmental degradation and improve healthy living. Buildings use one-third of our total energy, two-thirds of our electricity, one-eighth of our water, and transform land that provides valuable ecological services. Improving the environmental effects and healthy living requires that buildings be constructed, renovated and operated in a sustainable manner. The policy works hand in hand with both national and local sustainable building rating systems, with an emphasis on environmental concerns expressed by Saint Paul citizens and businesses.





Information about Saint Paul's Sustainable Building Policy can be found on the City of Saint Paul website at www.stpaul.gov.

The Penfield mixed-used project was built in accordance with Saint Paul's Sustainable Building Policy.







#### ALTERNATIVE FOR DISCUSSION: RECREATIONAL FIELDS NOT SHOWN

### F1 River Residential District

The River Residential District provides for high quality, large home structures with two to six dwelling units each and rear carriage units with an additional one to two housing units in a combined garage structure. The district is characterized by deep setbacks from Mississippi River Boulevard, consistent with the historic form of residential homes along the corridor.

General Character	High quality design and residential form that mirrors
	look of Mississippi River Boulevard
Land Uses	Residential mix of multi-unit homes and carriage
	houses
Heights	
Minimum	20 feet
Maximum	48 feet
FAR	0.25 - 1.5













#### ALTERNATIVE FOR DISCUSSION: RECREATIONAL FIELDS NOT SHOWN

### Open Space System



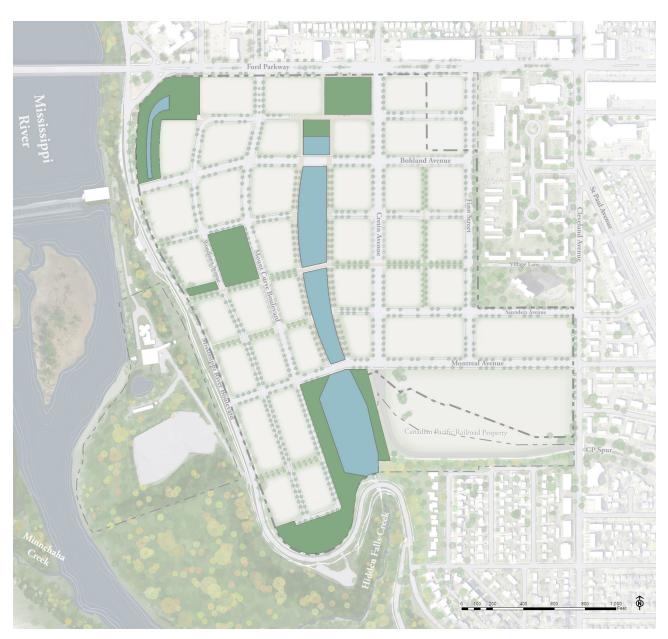
### PUBLIC OPEN SPACE

- 1 Gateway Park
- 2 Civic Square
- 3 Neighborhood Park
- 4 Pocket Park
- (5) Hidden Falls Headwaters Feature
- 6 Walking and Biking Paths



#### ALTERNATIVE FOR DISCUSSION: RECREATIONAL FIELDS NOT SHOWN

### Open Space Types



The open space system is intended to feel continuous in everyday experience. There are three distinct categories of open space types. The first is the traditional city park, which is acquired through parkland dedication and would become part of the city's park system. The second is the stormwater spine, which may feel like a park, but technically serves a utility function. A third category is a non-public recreation space, which is neither owned nor operated by the city, but an allowed use in all zoning districts. It is included here to reflect the vision of the community to see a continuation of recreational fields on the site.

### **OPEN SPACE TYPES**



City Park

